



Scenic Pacifica

Incorporated Nov. 22, 1957

PLANNING COMMISSION Agenda

DATE: February 6, 2017
LOCATION: Council Chambers, 2212 Beach Boulevard
TIME: 7:00 PM
ROLL CALL:
SALUTE TO FLAG:

ADMINISTRATIVE BUSINESS:

Approval of Order of Agenda

Approval of Minutes: January 17, 2017

Designation of Liaison to City Council Meeting: None required

Oral Communications:

This portion of the agenda is available to the public to address the Planning Commission on any issue within the subject matter jurisdiction of the Commission that is not on the agenda. The time allowed for any speaker will be three minutes.

CONSENT ITEMS: None

CONTINUED PUBLIC HEARINGS:

- PSD-757-06** **SITE DEVELOPMENT PERMIT PSD-757-06, USE PERMIT UP-965-06, AND TENTATIVE SUBDIVISION**
UP-965-06 **MAP (CONDOMINIUM) SUB-211-06**, filed by Shaohong "Simon" Weng; Pinkstone LLC, to extend the
SUB-211-06 expiration date of permits for the construction of nine condominiums located at 1567 Beach Boulevard
(APN 016-011-190). Recommended Action: Continue to February 6, 2017.

NEW PUBLIC HEARINGS:

- CDP-375-16** **COASTAL DEVELOPMENT PERMIT CDP-375-16**, filed by applicant Jo Ann Cullom of California Department of Transportation District 4, to replace the existing pedestrian overcrossing between Francisco Blvd. at San Jose Ave. and Eureka Square Shopping Center with a new pedestrian overcrossing. The new crossing incorporates longer ramps to meet Americans with Disabilities Act's ramp slope requirements. Recommended CEQA status: Class 2 Categorical Exemption, Section 15302. Recommended Action: Approve, as conditioned.
- UP-080-16** **USE PERMIT UP-080-16 AND SITE DEVELOPMENT PERMIT PSD-816-16**, filed by the City of Pacifica, to
PSD-816-16 construct a 2.1-million-gallon capacity equalization (EQ) basin, a 10-foot tall motor control center building, ventilation and odor-control system, and a cleaning system within the EQ basin at 540 Crespi Drive in Pacifica. The Project would also include construction of two diversion structures to passively divert excess flows from the existing Linda Mar and Arguello sanitary sewer lines and transport the flow via a conveyance pipeline to the EQ basin during storm events and an effluent conveyance pipeline routing flows to the existing Crespi Drive sanitary sewer line and Linda Mar Blvd Pump Station. Recommended California Environmental Quality Act (CEQA) status: Adopt a Final Mitigated Negative Declaration/Initial Study. Recommended Action: Approve, as conditioned.

CONSIDERATION ITEMS:

- Annual Report to the City Council.
Proposed Action: Discuss Annual Report.

COMMUNICATIONS:

Commission Communications:

Staff Communications:

ADJOURNMENT

Anyone aggrieved by the action of the Planning Commission has 10 calendar days to appeal the decision in writing to the City Council. If any of the above actions are challenged in court, issues which may be raised are limited to those raised at the public hearing or in written correspondence delivered to the City at, or prior to, the public hearing. Judicial review of any City administrative decision may be had only if a petition is filed with the court not later than the 90th day following the date upon which the decision becomes final. Judicial review of environmental determinations may be subject to a shorter time period for litigation, in certain cases 30 days following the date of final decision.

The City of Pacifica will provide special assistance for persons with disabilities upon 24 hours advance notice to the City Manager's office at (650) 738-7301, including requests for sign language assistance, written material printed in a larger font, or audio recordings of written material. All meeting rooms are accessible to persons with disabilities.

NOTE: Off-street parking is allowed by permit for attendance at official public meetings. Vehicles parked without permits are subject to citation. You should obtain a permit from the rack in the lobby and place it on the dashboard of your vehicle in such a manner as is visible to law enforcement personnel.



Scenic Pacifica
Incorporated Nov. 22, 1957

PLANNING COMMISSION Staff Report

DATE: February 6, 2017

FILE: PSD-757-06

UP-965-06

ITEM: 1

SUB-211-06

SUBJECT: Request to Extend Expiration Date of Site Development Permit PSD-757-06, Use Permit UP-965-06, and Tentative Subdivision Map (Condominium) SUB-211-06 for the Construction of Nine Condominiums

PROJECT LOCATION: 1567 Beach Boulevard (APN 016-011-190) – Sharp Park

APPLICANT Shaohong “Simon” Weng

AND OWNER: Pinkstone LLC

P.O. Box 505

Brisbane, CA 94005

RECOMMENDED ACTION: Continue to February 21, 2017

PREPARED BY: Tina Wehrmeister, Planning Director

DISCUSSION:

At the December 19, 2016 meeting, the Planning Commission continued this item to February 6, 2017 in order to allow the applicant to respond to staff’s recommendation. The Planning Commission also requested that the applicant submit materials by January 10, 2017 in order to provide staff adequate time to prepare an analysis. The applicant submitted materials on January 17, 2017, a week after the deadline. Staff is therefore requesting a continuance to February 21, 2017.



Scenic Pacifica
Incorporated Nov. 22, 1957

PLANNING COMMISSION Staff Report

DATE: February 6, 2017

FILE: CDP-375-16

ITEM: 2

PUBLIC NOTICE: Notice of Public Hearing was published in Pacifica Tribune on January 25, 2017, and mailed to 278 surrounding property owners and occupants.

APPLICANT Jo Ann Cullom, Chief
Office of Biological Sciences and Permits
California Department of Transportation
111 Grand Avenue
Oakland, CA 94623

PROJECT LOCATION: State Route (SR) 1 between 2040 Francisco Blvd and 1617-1655 Oceana Blvd (b/w APNs 016-191-110 and 016-220-150)

PROJECT DESCRIPTION: Replace the existing 278-foot long, 10-foot wide, and minimum 17-foot high pedestrian overcrossing (Bridge No. 35-0240) between Francisco Boulevard and Oceana Boulevard with a new 416-foot, 10-foot wide, and minimum 18.5-foot high pedestrian overcrossing. The new crossing incorporates longer ramps to meet Americans with Disabilities Act's ramp slope requirements.

SITE DESIGNATIONS: General Plan: Not Applicable
Zoning: CZ (Coastal Zone Combining)

RECOMMENDED CEQA STATUS: Class 2 Categorical Exemption, Section 15302

ADDITIONAL REQUIRED APPROVALS: None. Subject to appeal to the City Council.

RECOMMENDED ACTION: Approve as conditioned.

PREPARED BY: Bonny O'Connor, Assistant Planner

1. PROJECT DESCRIPTION

California Department of Transportation (Caltrans) is proposing to replace the San Jose Avenue pedestrian overcrossing (POC) on State Route (SR) 1, located between Francisco Boulevard and Oceana Boulevard, within the City of Pacifica (Bridge No. 35-0240). The existing overcrossing, which was built in 1968, has a documented history of deterioration. A seismic retrofit project was completed in 1994 to prolong its service life. In addition, Caltrans identified the POC as not complying with the requirements of the Americans with Disabilities Act (ADA) and recommended replacement in 2002.

The purpose of the project is to maintain safe pedestrian access to the coast, and maintain community connectivity between the West and East Sharp Park neighborhoods. The proposed overcrossing would be built to current design standards and would utilize material that is less susceptible to corrosion from the marine ambient environment. The proposed overcrossing would meet ADA standards by incorporating a ramp with a maximum 8.33 percent slope, as well as enhance the visual aesthetics of the SR 1 corridor.

Pedestrian Overcrossing

The proposed project involves replacing the existing 278-foot long, 10-foot wide, and minimum 17-foot high overcrossing with a new 416-foot long, 10-foot wide, and minimum 18.5-foot high overcrossing. The proposed crossing incorporates longer ramps to meet ADA's ramp slope requirements, and would have 255 feet of ramp compared to the 176 feet of ramp of the existing overcrossing. The east (Oceana Boulevard) entrance to the proposed overcrossing would be constructed approximately 82 feet north of the existing overcrossing.

The bridge would incorporate a coastal design (Attachment D). Design features including custom black light poles, black fencing with blue wave design weaved in the fencing, as well as a blue wave design imprinted near the base of the pillars of the overcrossing and along the edge of the overcrossing slab.

The east entrance of the existing overcrossing is at grade with Oceana Boulevard. The main span is composed of 2.5-foot thick, cast-in-place/reinforced concrete (CIP/RC) box girders, with a reinforced concrete column located in the median. The existing ramp is composed of 2.5-foot thick CIP/RC slab and cell components, with two reinforced concrete column supports. The proposed overcrossing would be constructed in three stages.

- **Stage 1** – Construction of the main span and slab ramp: The proposed 161-foot long main span would consist of a 3.0-foot thick CIP/RC box girder, with a reinforced concrete column located in the median. The east entrance would be at grade with Oceana Boulevard, and would be supported by a subgrade abutment. The proposed slab ramp would consist of 1.75-foot thick CIP/RC slab, which would be supported by three reinforced concrete columns. It would extend north from the west end of the main span for approximately 70 feet, then turn for approximately 10 feet, and extend south for approximately 80 feet, where it would meet the approach ramp.
- **Stage 2** – Demolition of the existing POC. Demolition of the existing bridge would include testing for asbestos-containing materials and any lead-based paint. Condition of Approval No. 8 would require the applicant to abate and dispose of hazardous materials in accordance with federal, state, and local regulations.

- **Stage 3** – Construction of the approach ramp: The proposed approach ramp would consist of 1.0-foot thick CIP/RC slab with a closed cell, and extend approximately 95 feet to the south, and finally touch down at the intersection of San Jose Avenue and Francisco Boulevard near the existing touchdown point.

The stages of construction may occur concurrently. The existing overcrossing would be in service during Stage 1 construction. A shuttle service would be used to transport citizens across the freeway during Stage 2 and Stage 3 construction. The applicants have not established a shuttle schedule at this time.

Construction activities, including staging, laydown and vehicles parking would occur within paved or unvegetated road shoulders.

Crosswalks and Sidewalks

On the Francisco Boulevard side of the POC, the existing crosswalks crossing Francisco Boulevard and San Jose Avenue would be repainted. The existing curb ramps located on the southwest and east side of Francisco Boulevard would be upgraded to ADA compliant curb ramps. Crosswalk signage with flashing beacon would be installed to warn motorists traveling northbound and southbound on Francisco Boulevard that pedestrians are present.

On the Oceana Boulevard side of the proposed POC a new crosswalk would be installed approximately 20 feet north of the new proposed POC east entrance. ADA compliant curb ramps would be installed on both sides of the new crosswalk. Crosswalk signage with flashing beacon would be installed to warn motorists traveling northbound and southbound on Oceana Boulevard that pedestrians are present (Attachment C, Sheet E-2 and E-5).

Infrastructure Relocation

One SamTrans bus stop sign pole on the southbound side of Oceana Boulevard would be relocated since it conflicts with crosswalk relocation plans. The bridge light electrical control box adjacent to the Francisco Boulevard entrance, and the light pole adjacent to the Oceana Boulevard entrance may need to be relocated if they conflict with the final project plans. The City of Pacifica would be notified once plans are finalized. The existing light poles on the overcrossing would be replaced with new non-standard (custom) light poles (Attachment C, Sheet E-4).

Vegetation Trimming and Landscape Restoration

Landscape vegetation within the Caltrans right-of-way would be trimmed or removed during the course of construction. All areas disturbed during project activities would be restored to preconstruction conditions. Disturbed landscaped areas would be stabilized and revegetated after construction is complete. No heritage tree outside of the Caltrans right-of-way would be affected.

Traffic Control

K-rails would be installed and occasional temporary lane closures would be required throughout the duration of the project. Individual lanes may be closed at any time during the construction process. Lane closure charts provided in the standard specifications detail how many through lanes are required by

Caltrans to be left open by the contractor. The chart details lane requirements hourly, seven days a week. Final lane closure schedules would be developed as a part of the construction contract. A complete closure may be necessary for safe falsework installation. Road closures to place falsework would most likely occur at night, and reopen in the morning. Full closure of SR-1 would be minimized as much as possible.

Construction Schedule and Measures

Construction is scheduled to start in January 2018 and be completed by November 2018. Project construction is estimated for 150 working days over one construction season.

The US Fish and Wildlife Service (USFWS) prepared a Biological Assessment (BA) to evaluate the potential effects of the Project on species that are regulated by the USFWS. The BA identified two special-status species with the potential to occur in the project area: the San Francisco garter snake (*Thamnophis sirtalis tetrataenia*, SFGS), and California red-legged frog (*Rana draytonii*, CRLF). The BA concludes that no temporary or permanent effects or losses are expected to occur to either of these species or their associated habitat. However, because the presence of SFGS and CRLF in the project area cannot be completely ruled out, Condition of Approval No. 4 would require the applicant to implement standard best management practices and specific avoidance and minimization measures detailed in the BA to avoid adverse effects on listed species and their habitats. These actions would include avoiding construction activities during and after rain events, performing preconstruction surveys, and conducting preconstruction worker environmental awareness training.

2. GENERAL PLAN, ZONING, AND SURROUNDING LAND USE

The project site is not designated by the General Plan as the entire project would occur within Caltrans and City of Pacifica right-of-way. The project is located in the non-appeal zone of the Coastal Zone. The Eureka Square Shopping Center is located east of the project. A multiunit apartment building, restaurant, and city parking lot is located to the west of the project. SR-1 is located north and south of the project.

3. MUNICIPAL CODE

The project requires issuance of a Coastal Development Permit (CDP) under Section 9-4.4304(k) of the Pacifica Municipal Code (PMC). Issuance of a CDP requires the Planning Commission to make two findings, as follows:

- a) The proposed development is in conformity with the City's certified Local Coastal Program.
- b) Where the Coastal Development Permit is issued for a development between the nearest public road and the shoreline, the development is in conformity with the public recreation policies of Chapter 3 of the California Coastal Act.

4. REQUIRED FINDINGS

In order to approve the subject Coastal Development Permit (CDP-375-16), the Planning Commission must make the two findings required by PMC Section 9-4.4304(k). The following discussion supports the Commission's findings in this regard.

A. *The proposed development is in conformity with the City's certified Local Coastal Program.*

Discussion: The City's certified Local Coastal Program includes a Local Coastal Land Use Plan (LCLUP) that contains policies to further the City's coastal planning activities. The proposed project is consistent with many of these policies, as discussed below.

Coastal Act Policy 1. *Maximum access shall be conspicuously posted and recreational opportunities shall be provided for all the people consistent with public safety needs and the need to protect public rights, rights of property owners, and natural resource areas from overuse.*

Discussion: The project would improve the accessibility and public safety of pedestrians traveling into and out of the coastal zone. The proposed POC would be built to current ADA standards, which would include a reduced slope, as well as, the installation of ADA sidewalk ramps. The ADA compliance of the proposed POC would increase the range of pedestrians that would be able to use the POC.

The proposed POC would replace the existing overcrossing that has experienced deterioration in the form of concrete clumps dropping on the highway below and rusting fencing and structural support wire with coastal climate appropriate materials built and used to current engineering design standard.

Coastal Act Policy 24. *The scenic and visual qualities of coastal areas shall be considered and protected as a resource of public importance. Permitted development shall be sited and designed to protect views to and along the ocean and scenic coastal areas, to minimize the alteration of natural landforms, to be visually compatible with the character of surrounding areas, and, where feasible; to restore and enhance visual quality in visually degraded areas. New development in highly scenic areas such as those designated in the California Coastline Preservation and Recreation Plan, prepared by the Department of Parks and Recreation and by local government, shall be subordinate to the character of its setting.*

Discussion: The existing POC has extensive visual signs of deterioration. The 1968 POC has significant rust, discoloration, missing clumps of concrete, and exposed wiring throughout the POC. The proposed POC would provide a significant visual improvement to the area by removing the deteriorating structure, replacing it with a new structure and refreshing the landscaping in the area. The proposed POC would incorporate a coastal design (Attachment D). Design features including custom black light poles, black fencing with blue wave design weaved in the fencing, as well as a blue wave design imprinted near the base of the pillars of the overcrossing and along the edge of the overcrossing slab.

Coastal Act Policy 25. *The location and amount of new development should maintain and enhance public access to the coast by:*

a) *Facilitating the provision or extension of transit service;*

- b) *Providing commercial facilities within or adjoining residential development, or in other areas that will minimize the use of coastal access roads;*
- c) *Providing non-automobile circulation within the development;*
- d) *Providing adequate parking facilities or providing substitute means of serving the development with public transportation;*
- e) *Assuring the potential for public transit for high intensity uses such as high-rise office buildings; and*
- f) *Assuring that the recreational needs of new residents will not overload nearby coastal recreation areas by correlating the amount of development with local park acquisition and development plans with, the provision of on-site recreational facilities to serve the new development.*

Discussion: Consistent with subsection b), the project would provide a pedestrian short-cut between a residential district and a commercial shopping area which would encourage walking and minimize the use of coastal roads.

Consistent with subsection c), the project would provide non-automobile circulation within the development. As discussed above, the project would improve the accessibility and public safety of pedestrians traveling into and out of the coastal zone. The proposed POC would be built to current ADA standards, which would include a reduced slope, as well as, the installation of ADA sidewalk ramps. The ADA compliance of the proposed POC would increase the range of pedestrians that would be able to use the POC.

The project would not impact public transit service or parking and therefore would not conflict with subsection a),d), e), and f).

- B. *Where the Coastal Development Permit is issued for a development between the nearest public road and the shoreline, the development is in conformity with the public recreation policies of Chapter 3 of the California Coastal Act.*

Discussion: The development would not be located between the nearest public road and the shoreline, therefore this finding does not apply.

5. CEQA RECOMMENDATION

Staff analysis of the proposed project supports a Planning Commission finding that it qualifies for a categorical exemption from the California Environmental Quality Act (CEQA). The project qualifies as a Class 2 exemption under CEQA Guidelines Section 15302. CEQA Guidelines Section 15302 states, in part:

15302. Replacement or Reconstruction

Class 2 consists of replacement or reconstruction of existing structures and facilities where the new structure will be located on the same site as the structure replaced and will have substantially the same purpose and capacity as the structure replaced [...].

The project would replace the existing SR-1 POC with a new POC that would provide the same purpose and capacity and would be located in the same Caltrans site. Additionally, none of the exceptions to the exemption in Section 15300.2 of the CEQA Guidelines apply, as described below.

- Sec. 15300.2(b): There is no evidence in the record that cumulative projects of the same type would occur within the same place to create a significant cumulative impact.
- Sec. 15300.2(c): There is no evidence that the activity would have a significant effect on the environment due to unusual circumstances.
- Sec. 15300.2(d): The project would occur within an eligible State Scenic highway; therefore, the construction and operation of the project would not significantly damage scenic resources within an officially designated state scenic highway. The project would replace a deteriorating POC with a new POC. The new POC includes a coastal themed design and the use of coastal climate appropriate materials, which would significantly improve the aesthetics along SR-1. The applicant has stated that any landscaping and vegetation affected by construction would be restored to pre-construction conditions.
- Sec. 15300.2(e and f): The project does not involve a current or former hazardous waste site, and, does not affect any historical resources. Therefore, the provisions of subsections (e) and (f) are not applicable to this project.

Because the project is consistent with the requirements for a Class 2 exemption and none of the exceptions to the exemption in Section 15300.2 apply, there is substantial evidence in the record to support a finding that the project is categorically exempt from CEQA.

6. CONCLUSION

In staff's opinion, as conditioned, the project is consistent with the General Plan, Local Coastal Land Use Plan, and the Pacifica Municipal Code. The project would replace a deteriorating POC with a new POC that meets current ADA standards. The coastal themed detailed in the design and the use of coastal appropriate materials would significantly improve the aesthetics along SR-1. The project is consistent with the requirements for a Class 2 exemption and none of the exceptions to the exemption in Section 15300.2 apply. Therefore, staff recommends approval of the CDP.

7. COMMISSION ACTION

MOTION FOR APPROVAL:

Move that the Planning Commission finds the project is exempt from the California Environmental Quality Act; **APPROVE** Coastal Development Permit CDP-375-16, by adopting the resolution included as Attachment A to the staff report, including conditions of approval in Exhibit A to the resolution; and, incorporate all maps and testimony into the record by reference.

Attachments:

- A. Draft Resolution and Conditions of Approval
- B. Land Use and Zoning Exhibit
- C. Selected sheets of Project Plans
- D. Visual Rendering
- E. Comment Letter Received

RESOLUTION NO. _____

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PACIFICA APPROVING COASTAL DEVELOPMENT PERMIT CDP-375-16, SUBJECT TO CONDITIONS, FOR REPLACEMENT OF SAN JOSE AVENUE PEDESTRIAN OVERCROSSING (BRIDGE NO. 35-0240) ON STATE ROUTE 1, LOCATED BETWEEN FRANCISCO BOULEVARD AND OCEANA BOULEVARD, WITHIN THE CITY OF PACIFICA AND FINDING THE PROJECT EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA).

Initiated by: Jo Ann Cullom, California Department of Transportation (“Applicant”).

WHEREAS, an application has been submitted to replace the existing 278-foot long, 10-foot wide, and minimum 17-foot high pedestrian overcrossing (Bridge No. 35-0240) between Francisco Boulevard and Oceana Boulevard with a new 416-foot, 10-foot wide, and minimum 18.5-foot high pedestrian overcrossing; and

WHEREAS, the project requires approval of a Coastal Development Permit because the project involves development within the Coastal Zone; and, the project does not qualify as a category of exempted or excluded development; and

WHEREAS, the Planning Commission of the City of Pacifica did hold a duly noticed public hearing on February 6, 2017, at which time it considered all oral and documentary evidence presented, and incorporated all testimony and documents into the record by reference.

NOW, THEREFORE BE IT RESOLVED by the Planning Commission of the City of Pacifica as follows:

1. The above recitals are true and correct and material to this Resolution.
2. In making its findings, the Planning Commission relied upon and hereby incorporates by reference all correspondence, staff reports, and other related materials.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Pacifica does hereby make the following findings pertaining to Coastal Development Permit CDP-375-16 for development within the Coastal Zone:

Required Finding: *The proposed development is in conformity with the City’s certified Local Coastal Program.*

Discussion: The City’s certified Local Coastal Program includes a Local Coastal Land Use Plan (LCLUP) that contains policies to further the City’s coastal planning activities. The proposed project is consistent with many of these policies, as discussed below.

***Coastal Act Policy 1.** Maximum access shall be conspicuously posted and recreational opportunities shall be provided for all the people consistent with public safety needs and the need to protect public rights, rights of property owners, and natural resource areas from overuse.*

Discussion: The project would improve the accessibility and public safety of pedestrians traveling into and out of the coastal zone. The proposed POC would be built to current ADA standards, which would include a reduced slope, as well as, the installation of ADA sidewalk ramps. The ADA compliance of the proposed POC would increase the range of pedestrians that would be able to use the POC.

The proposed POC would replace the existing overcrossing that has experienced deterioration in the form of concrete clumps dropping on the highway below and rusting fencing and structural support wire with coastal climate appropriate materials built and used to current engineering design standard.

***Coastal Act Policy 24.** The scenic and visual qualities of coastal areas shall be considered and protected as a resource of public importance. Permitted development shall be sited and designed to protect views to and along the ocean and scenic coastal areas, to minimize the alteration of natural landforms, to be visually compatible with the character of surrounding areas, and, where feasible; to restore and enhance visual quality in visually degraded areas. New development in highly scenic areas such as those designated in the California Coastline Preservation and Recreation Plan, prepared by the Department of Parks and Recreation and by local government, shall be subordinate to the character of its setting.*

Discussion: The existing POC has extensive visual signs of deterioration. The 1968 POC has significant rust, discoloration, missing clumps of concrete, and exposed wiring throughout the POC. The proposed POC would provide a significant visual improvement to the area by removing the deteriorating structure, replacing it with a new structure and refreshing the landscaping in the area. The proposed POC would incorporate a coastal design (Attachment D). Design features including custom black light poles, black fencing with blue wave design weaved in the fencing, as well as a blue wave design imprinted near the base of the pillars of the overcrossing and along the edge of the overcrossing slab.

***Coastal Act Policy 25.** The location and amount of new development should maintain and enhance public access to the coast by:*

- a) Facilitating the provision or extension of transit service;*
- b) Providing commercial facilities within or adjoining residential development, or in other areas that will minimize the use of coastal access roads;*
- c) Providing non-automobile circulation within the development;*
- d) Providing adequate parking facilities or providing substitute means of serving the development with public transportation;*

- e) *Assuring the potential for public transit for high intensity uses such as high-rise office buildings; and*
- f) *Assuring that the recreational needs of new residents will not overload nearby coastal recreation areas by correlating the amount of development with local park acquisition and development plans with, the provision of on-site recreational facilities to serve the new development.*

Discussion: Consistent with subsection b), the project would provide a pedestrian short-cut between a residential district and a commercial shopping area which would encourage walking and minimize the use of coastal roads.

Consistent with subsection c), the project would provide non-automobile circulation within the development. As discussed above, the project would improve the accessibility and public safety of pedestrians traveling into and out of the coastal zone. The proposed POC would be built to current ADA standards, which would include a reduced slope, as well as, the installation of ADA sidewalk ramps. The ADA compliance of the proposed POC would increase the range of pedestrians that would be able to use the POC.

The project would not impact public transit service or parking and therefore would not conflict with subsection a),d), e), and f).

Required Finding: *Where the Coastal Development Permit is issued for any development between the nearest public road and the shoreline, the development is in conformity with the public recreation policies of Chapter 3 of the California Coastal Act.*

Discussion: The development would not be located between the nearest public road and the shoreline, therefore this finding does not apply.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Pacifica does hereby make the following findings pertaining to the project:

That the project is exempt from the CEQA as a Class 2 exemption provided in Section 15302 of the CEQA Guidelines.

15302. Replacement or Reconstruction

Class 2 consists of replacement or reconstruction of existing structures and facilities where the new structure will be located on the same site as the structure replaced and will have substantially the same purpose and capacity as the structure replaced [...].

The project would replace the existing SR-1 POC with a new POC that would provide the same purpose and capacity and would be located in the same Caltrans site. Additionally,

none of the exceptions to the exemption in Section 15300.2 of the CEQA Guidelines apply, as described below.

- Sec. 15300.2(b): There is no evidence in the record that cumulative projects of the same type would occur within the same place to create a significant cumulative impact.
- Sec. 15300.2(c): There is no evidence that the activity would have a significant effect on the environment due to unusual circumstances.
- Sec. 15300.2(d): The project would occur within an eligible State Scenic highway; therefore, the construction and operation of the project would not significantly damage scenic resources within an officially designated state scenic highway. The project would replace a deteriorating POC with a new POC. The new POC includes a coastal themed design and the use of coastal climate appropriate materials, which would significantly improve the aesthetics along SR-1. The applicant has stated that any landscaping and vegetation affected by construction would be restored to pre-construction conditions.
- Sec. 15300.2(e and f): The project does not involve a current or former hazardous waste site, and, does not affect any historical resources. Therefore, the provisions of subsections (e) and (f) are not applicable to this project.

Because the project is consistent with the requirements for a Class 2 exemption and none of the exceptions to the exemption in Section 15300.2 apply, there is substantial evidence in the record to support a finding that the project is categorically exempt from CEQA.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Pacifica approves Coastal Development Permit (CDP-375-16) for replacement of the existing 278-foot long, 10-foot wide, and minimum 17-foot high pedestrian overcrossing (Bridge No. 35-0240) between Francisco Boulevard and Oceana Boulevard with a new 416-foot, 10-foot wide, and minimum 18.5-foot high pedestrian overcrossing, subject to conditions of approval included as Exhibit A to this resolution.

* * * * *

Passed and adopted at a regular meeting of the Planning Commission of the City of Pacifica, California, held on the 6th day of February 2017.

AYES, Commissioners:

NOES, Commissioners:

ABSENT, Commissioners:

ABSTAIN, Commissioners:

ATTEST:

Tina Wehrmeister, Planning Director

Josh Gordon, Chair

APPROVED AS TO FORM:

Michelle Kenyon, City Attorney

Exhibit A

Conditions of Approval: Coastal Development Permit (CDP-375-16) for replacement of the existing 278-foot long, 10-foot wide, and minimum 17-foot high pedestrian overcrossing (Bridge No. 35-0240) between Francisco Boulevard and Oceana Boulevard with a new 416-foot, 10-foot wide, and minimum 18.5-foot high pedestrian overcrossing

Planning Commission Meeting of February 6, 2017

Planning Division of the Planning Department

1. Development shall be substantially in accord with the plans entitled "Project Plans for Construction on State Highway in San Mateo County in Pacifica at San Jose Avenue," received by the City of Pacifica on January 25, 2017 and with the architectural rendering (Attachment D of the February 6, 2017 staff report) received by the City of Pacifica on October 11, 2016 as part of the original application, except as modified by the following conditions.
2. Applicant shall submit a shuttle schedule, route, and a description of the shuttle vehicle the City of Pacifica prior to start of construction.
3. Applicant shall lead coordination between the Applicant, SamTrans, and the City of Pacifica for the relocation of the SamTrans bus stop located on the southbound side of Oceana Boulevard.
4. Applicant shall implement Avoidance and Minimization Measures as detailed in the May 2015 Biological Assessment for the Highway 1 Pedestrian Bridge Structure Replacement Project, Caltrans District 4, San Mateo County, Highway 1, PM R-44.00/44.04.
5. That the approval or approvals is/are valid for a period of one year from the date of final determination. If the use or uses approved is/are not established within such period of time, the approval(s) shall expire unless Applicant submits a written request for an extension and applicable fee prior to the expiration date, and the Planning Director or Planning Commission approves the extension request as provided below. The Planning Director may administratively grant a single, one year extension provided, in the Planning Director's sole discretion, the circumstances considered during the initial project approval have not materially changed. Otherwise, the Planning Commission shall consider a request for a single, one year extension.
6. Applicant shall maintain its site in a fashion that does not constitute a public nuisance and that does not violate any provision of the Pacifica Municipal Code.
7. Prior to start of construction, Applicant shall clearly indicate compliance with all conditions of approval on the plans and/or provide written explanations to the Planning Director's satisfaction.

8. If hazardous materials are found during testing, the Applicant shall abate and dispose of hazardous materials in accordance with federal, state, and local regulations.
9. The applicant shall indemnify, defend and hold harmless the City, its Council, Planning Commission, advisory boards, officers, employees, consultants and agents (hereinafter "City") from any claim, action or proceeding (hereinafter "Proceeding") brought against the City to attack, set aside, void or annul the City's actions regarding any development or land use permit, application, license, denial, approval or authorization, including, but not limited to, variances, use permits, developments plans, specific plans, general plan amendments, zoning amendments, approvals and certifications pursuant to the California Environmental Quality Act, and/or any mitigation monitoring program, or brought against the City due to actions or omissions in any way connected to the applicant's project, but excluding any approvals governed by California Government Code Section 66474.9. This indemnification shall include, but not be limited to, damages, fees and/or costs awarded against the City, if any, and costs of suit, attorney fees and other costs, liabilities and expenses incurred in connection with such proceeding whether incurred by the applicant, City, and/or parties initiating or bringing such Proceeding. If the applicant is required to defend the City as set forth above, the City shall retain the right to select the counsel who shall defend the City.
10. Prior to the start of construction, Applicant shall submit a final landscape plan for approval by the Planning Director. The landscape plan shall show each type, size, and location of plant materials, as well as the irrigation system. Landscaping materials included on the plan shall be coastal compatible, drought tolerant and shall be predominantly native, and shall include an appropriate mix of trees, shrubs, and other plantings to soften the expanded structure. In addition, the landscaping shall be maintained as shown on the landscape plan and shall be designed to incorporate efficient irrigation to reduce runoff, promote surface filtration, and minimize the use of fertilizers, herbicides, and pesticides. Landscaping on the site shall be adequately maintained in a healthful condition and replaced when necessary as determined by the Planning Director.

Building Division of the Planning Department

11. Slopes five (5) percent or greater shall require handrails on both sides of the pedestrian overcrossing.

Engineering Division of Public Works Department

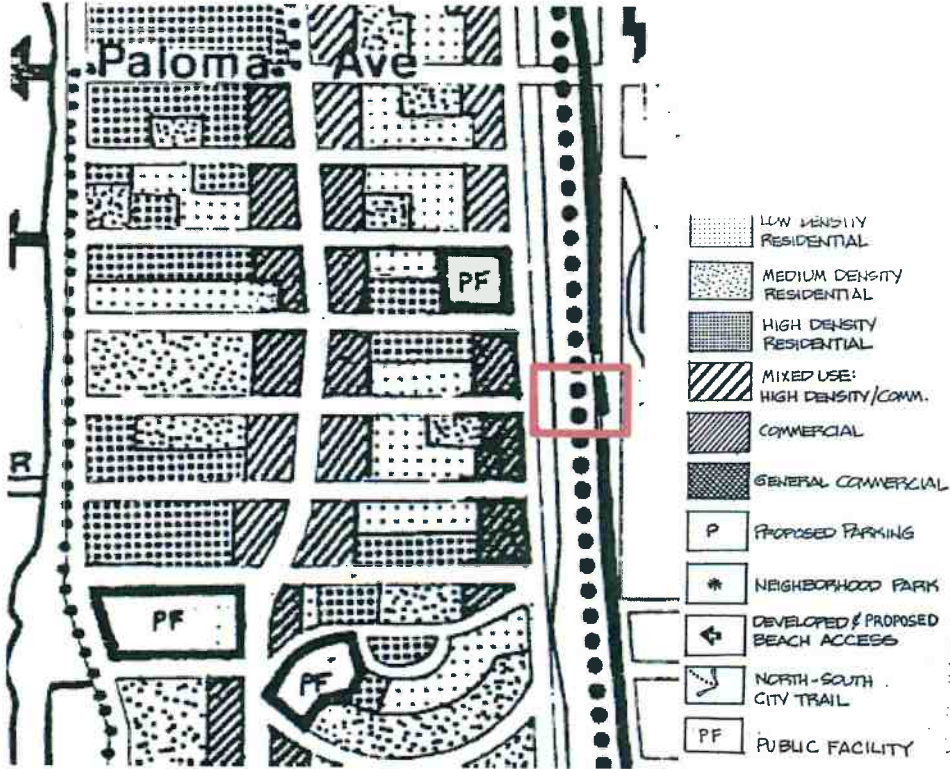
12. Construction within the Caltrans right-of-way shall be covered under the Caltrans Stormwater Management Plan. Construction within the City of Pacifica right-of-way shall be in conformance with the San Mateo Countywide Storm Water Pollution Prevention Program. Best Management Practices shall be implemented.
13. Roadways shall be maintained clear of construction materials and debris, especially mud and dirt tracked, onto Oceana Boulevard and Francisco Boulevard. Dust control and daily road cleanup will be strictly enforced.

14. Existing curb, sidewalk or other street improvements adjacent to the property frontage that is damaged or displaced shall be repaired or replaced as deemed by the City Engineer even if damage or displacement occurred prior to any work performed for this project.
15. Applicant shall upgrade all three curb ramps at the intersection of Francisco Boulevard and San Jose Avenue to current standards.
16. Applicant shall install a permanent pedestrian barricade at the eastern edge of the sidewalk on the eastern side of the pedestrian overcrossing, similar to the existing one, to discourage pedestrians from crossing Oceana Blvd. at an undesignated location.
17. Applicant shall provide final details for the construction along Oceana Boulevard.
18. A traffic control plan shall be submitted for review by the City Engineer. Lane closures shall be requested 72 hours in advance of schedule and coordinated with Pacifica Police and Fire Departments. Through traffic shall be maintained at all times along Oceana Boulevard and Francisco Boulevard.
19. A City of Pacifica Encroachment Permit shall be obtained for all work undertaken in the public right-of-way. All work shall be done in accordance with City Standards, Standard Specifications for Public Works Construction (Green Book) or Caltrans Standard Specifications, Pacifica Municipal Code, Administrative Policies and to the satisfaction of the City Engineer or his designee and shall be completed prior issuance of the Certificate of Occupancy. Permit fees shall be determined per the current adopted fee schedule at the time of permit issuance.

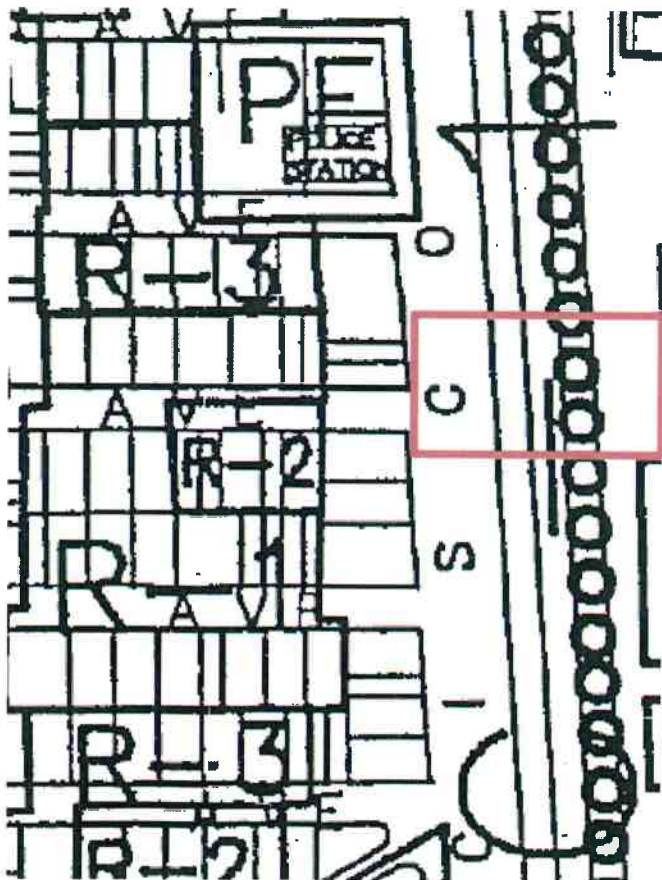
END

Attachment B. Land Use and Zoning Exhibit

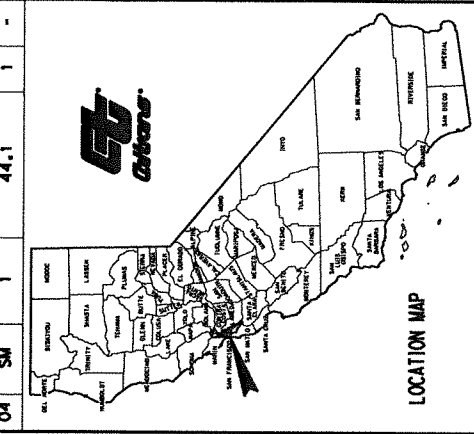
General Plan Land Use



Zoning District



DIST	COUNTY	ROUTE	TOTAL SHEETS	SHEET NO.
04	SM	1	44-1	1



STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION
PROJECT PLANS FOR CONSTRUCTION ON
STATE HIGHWAY
IN SAN MATEO COUNTY
IN PACIFICA
AT SAN JOSE AVENUE

TO BE SUPPLEMENTED BY STANDARD PLANS DATED 2015

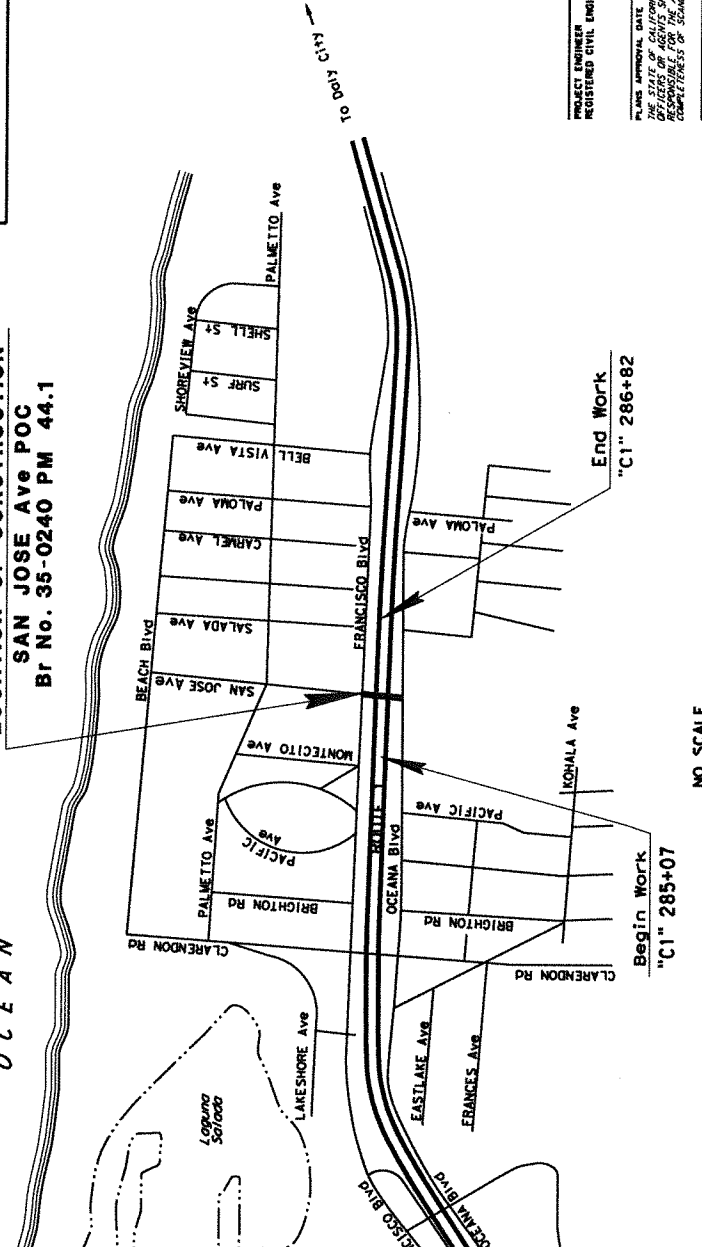


INDEX OF PLANS

SHEET No.	DESCRIPTION
1	TITLE SHEET AND LOCATION MAP
2 - 4	TYPICAL CROSS SECTIONS
5	LAYOUT
6	PROFILE
7 - 8	CONSTRUCTION DETAILS
9	WATER POLLUTION CONTROL QUANTITIES
10 - 12	DRAINAGE PLAN, PROFILE AND QUANTITIES
13	UTILITY PLAN
14 - 16	CONSTRUCTION AREA SIGNS
17 - 21	STAGE CONSTRUCTION AND TRAFFIC HANDLING PLAN AND QUANTITIES
22 - 23	PAVEMENT DELINEATION PLAN AND QUANTITIES
24 - 25	SIGN PLAN AND QUANTITIES
26	SUMMARY OF QUANTITIES
27 - 28	IRRIGATION PLAN
29	PLANTING PLAN
30 - 32	EROSION CONTROL LEGEND, QUANTITIES AND PLANS
33 - 38	ELECTRICAL PLAN AND DETAILS
39 - 45	REVISED STANDARD PLANS
46 - 50	STRUCTURE PLANS
	SAN JOSE Ave POC

PACIFIC OCEAN
LOCATION OF CONSTRUCTION
SAN JOSE Ave POC
Br No. 35-0240 PM 44.1

THE STANDARD PLANS LIST APPLICABLE TO THIS CONTRACT IS INCLUDED IN THE NOTICE TO BIDDERS AND SPECIAL PROVISIONS BOOK.



LIMITS OF WORK ON LOCAL STREETS

STREET NAME	LINE FROM	TO
FRANCISCO BLVD	"FL3" 285+00	285+25
OCEANA BLVD	"FR3" 284+75	286+75

THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE "NOTICE TO BIDDERS."

PROJECT ENGINEER: _____ DATE: _____
 REGISTERED CIVIL ENGINEER

PLANS APPROVAL DATE: _____
 OFFICERS OF AGENCIES SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SOURCE DATA OF THIS PLAN SHEET.

CONTRACT No.	04-4G8504
PROJECT ID	0413000052
PROJECT NUMBER & PHASE	04130000321

RELATIVE BORDER SCALE 0 1 2 3
 1" IS 15' IN INCHES
 USER NAME: 03 WELLS
 DOW FILE # 3 REQUEST

DESIGN MANAGER	AMIR H. SMAYTAN
PROJECT MANAGER	A

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION		DESIGN		FUNCTIONAL SUPERVISOR		AMR H SANATKAR		CHECKED BY		TANZEEBA KISHWAR		DATE REVISED		11-00-16	
CALCULATED BY		DESIGNED BY		TANZEEBA KISHWAR		REVISOR		REVISOR		TK		DATE REVISED		11-00-16	

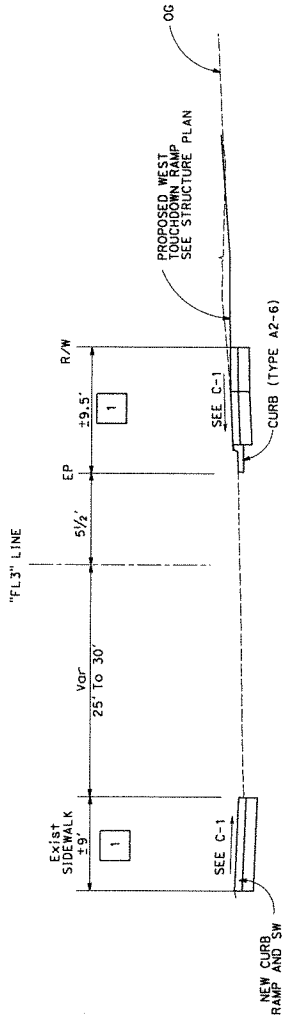
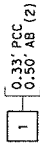
NOTES:

- DIMENSIONS OF THE PAVEMENT STRUCTURES (STRUCTURAL SECTIONS) ARE TO TOLERANCES SPECIFIED IN THE STANDARD SPECIFICATIONS.
- FOR EXACT LOCATION OF FENCE, SIDEWALK, CURB REPLACEMENT SEE LAYOUT SHEETS.

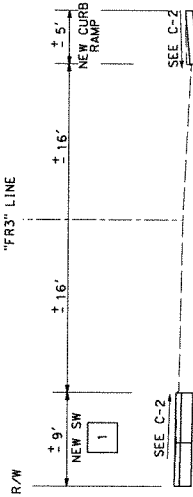
ABBREVIATIONS:

- PCC PORTLAND CEMENT CONCRETE
- AB AGGREGATE BASE
- SW SIDE WALK

PAVEMENT STRUCTURAL SECTIONS:



"FL3" 285+00 TO 285+20
FRANCISCO Blvd



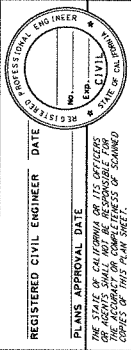
"FR3" 285+76 TO 285+86
OCEANA Blvd

TYPICAL CROSS SECTIONS
NO SCALE

X-1

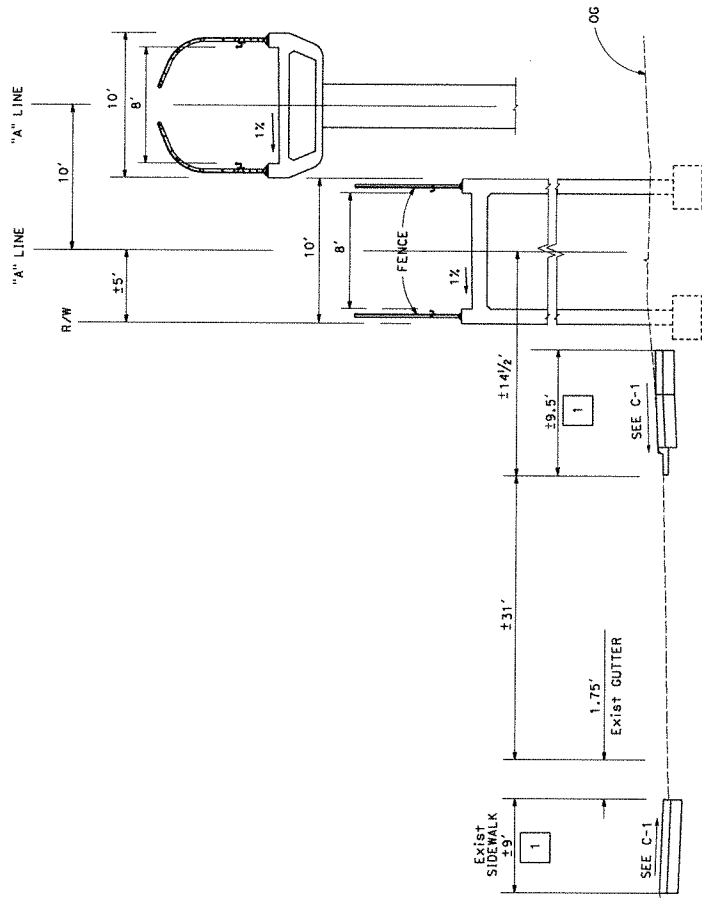
DIST	COUNTY	ROUTE	SHEET NO.	TOTAL SHEETS
04	SM	1	44.1	2

REGISTERED CIVIL ENGINEER DATE _____
PLANS APPROVAL DATE _____
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENCIES SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION CONTAINED HEREIN UNLESS IT IS SHOWN OTHERWISE ON THIS PLAN SHEET.



DIS+1	COUNTY	ROUTE	POST MILE TOTAL PROJECT	SHEET NO. OF SHEETS
04	SM	1	44.1	3

REGISTERED CIVIL ENGINEER DATE _____
 PROFESSIONAL ENGINEER No. _____
 CIVIL No. _____
 STATE OF CALIFORNIA
 THE STATE OF CALIFORNIA ON ITS OFFICERS
 OF THE BOARD OF PROFESSIONAL ENGINEERS
 DOES HEREBY CERTIFY THAT THE ABOVE IS A TRUE
 COPY OF THIS PLAN SHEET.



**RAMP ACCESS SECTION
 SAN JOSE Ave Ped OC**

TYPICAL CROSS SECTIONS
 NO SCALE

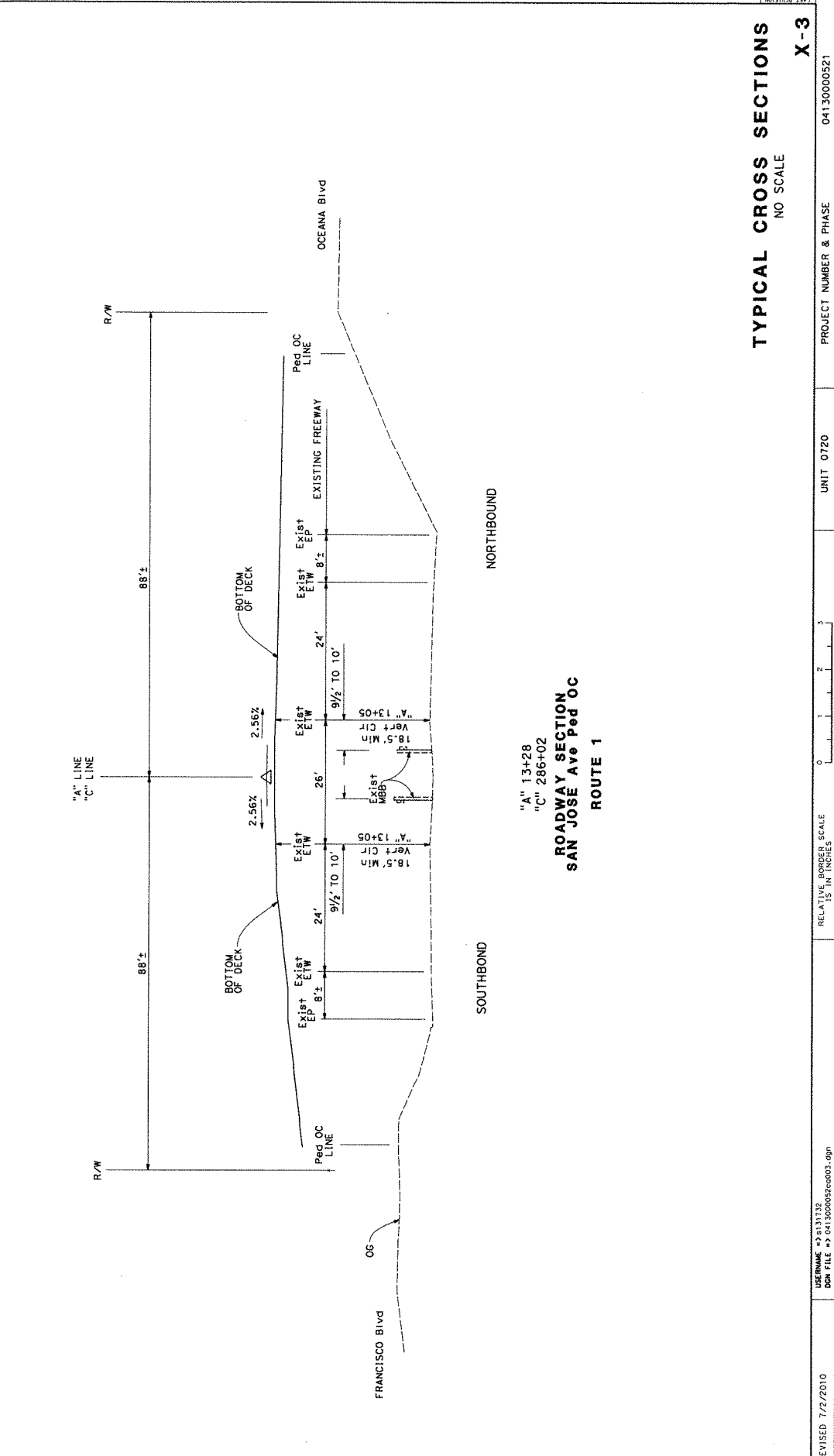
X-2

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	DESIGN	AMIR H SAMATKAR	CHECKED BY	TANZEBA KISHWAR	DATE REVISED	11-00-16
FUNCTIONAL SUPERVISOR			DESIGNED BY	TANZEBA KISHWAR	REVISOR	TK

DATE	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET TOTAL NO. SHEETS
04	SM	1	44.1	4

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR CONSEQUENCES OF THIS PLAN SHEET.



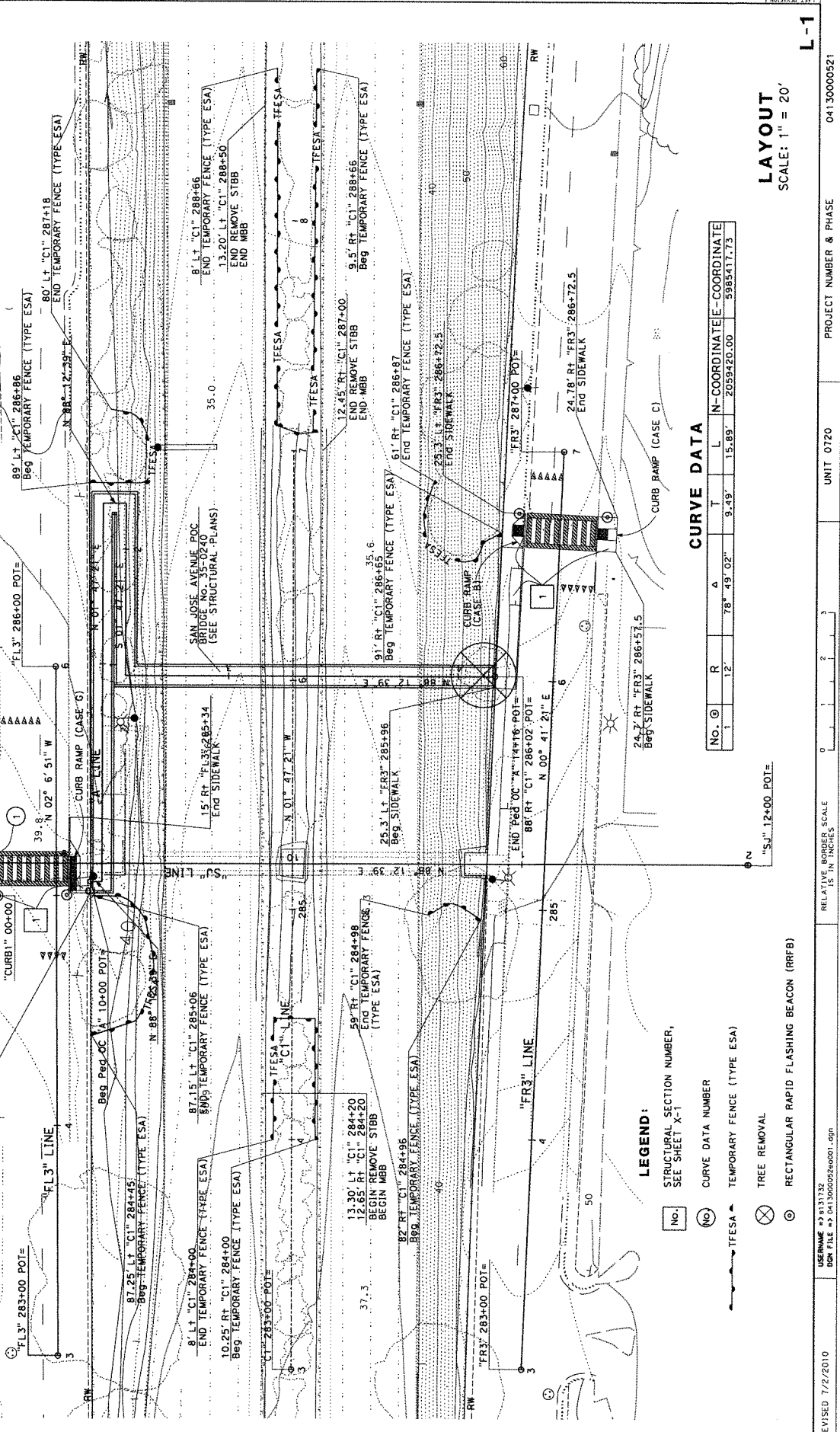
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	FUNCTIONAL SUPERVISOR	AMIR H SANATKAR	CHECKED BY	TANZEBA KISHMAR	DATE REVISED	11-00-16
DESIGN	DESIGNED BY	TANZEBA KISHMAR	REVISOR	TX		

TYPICAL CROSS SECTIONS
NO SCALE
X-3

DATE REVISION	DATE PLOTTED	DATE	11-17-16	09-05-2016	09-05-2016
USER NAME	DKM FILE	PROJECT NUMBER & PHASE	0413000521	0413000521	0413000521

DESIGNED BY	TANZEBBA KISHWAR	REGISTERED CIVIL ENGINEER	DATE	4.1	SHEET TOTAL SHEETS	1	A
CHECKED BY	TANZEBBA KISHWAR	PLANS APPROVAL DATA					
FUNCTIONAL SUPERVISOR	AMIR H SAAKATKAR	THE STATE OF CALIFORNIA OR ITS OFFICERS					
DESIGN		OR AN AUTHORIZED ENGINEER RESPONSIBLE FOR					
		COPIES OF THIS PLAN SHEET.					

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



CURVE DATA

No.	θ	R	Δ	T	L	N-COORDINATE	E-COORDINATE
1	12	78	49	02	9.49	2059420.00	5985477.73

- LEGEND:**
- NO. [Symbol] STRUCTURAL SECTION NUMBER, SEE SHEET X-1
 - NO. [Symbol] CURVE DATA NUMBER
 - TFESA [Symbol] TEMPORARY FENCE (TYPE ESA)
 - ⊗ [Symbol] TREE REMOVAL
 - ⊙ [Symbol] RECTANGULAR RAPID FLASHING BEACON (RRFB)

DATE PLOTTED => 06-DEC-2018 11-17-16

REGISTERED CIVIL ENGINEER

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS
OR AGENCIES SHALL NOT BE RESPONSIBLE FOR
ANY ERRORS OR OMISSIONS OR FOR THE
CONSEQUENCES OF THIS PLAN SHEET.

REGISTERED CIVIL ENGINEER: DATE: PROJECT NO.: SHEET NO.: TOTAL SHEETS: 44.1 6 A

PROJECT: COUNTY: ROUTE: SHEET: TOTAL SHEETS: 04 SM 1 6 A

REGISTERED CIVIL ENGINEER: DATE: PROJECT NO.: SHEET NO.: TOTAL SHEETS: 44.1 6 A

REGISTERED CIVIL ENGINEER: DATE: PROJECT NO.: SHEET NO.: TOTAL SHEETS: 44.1 6 A

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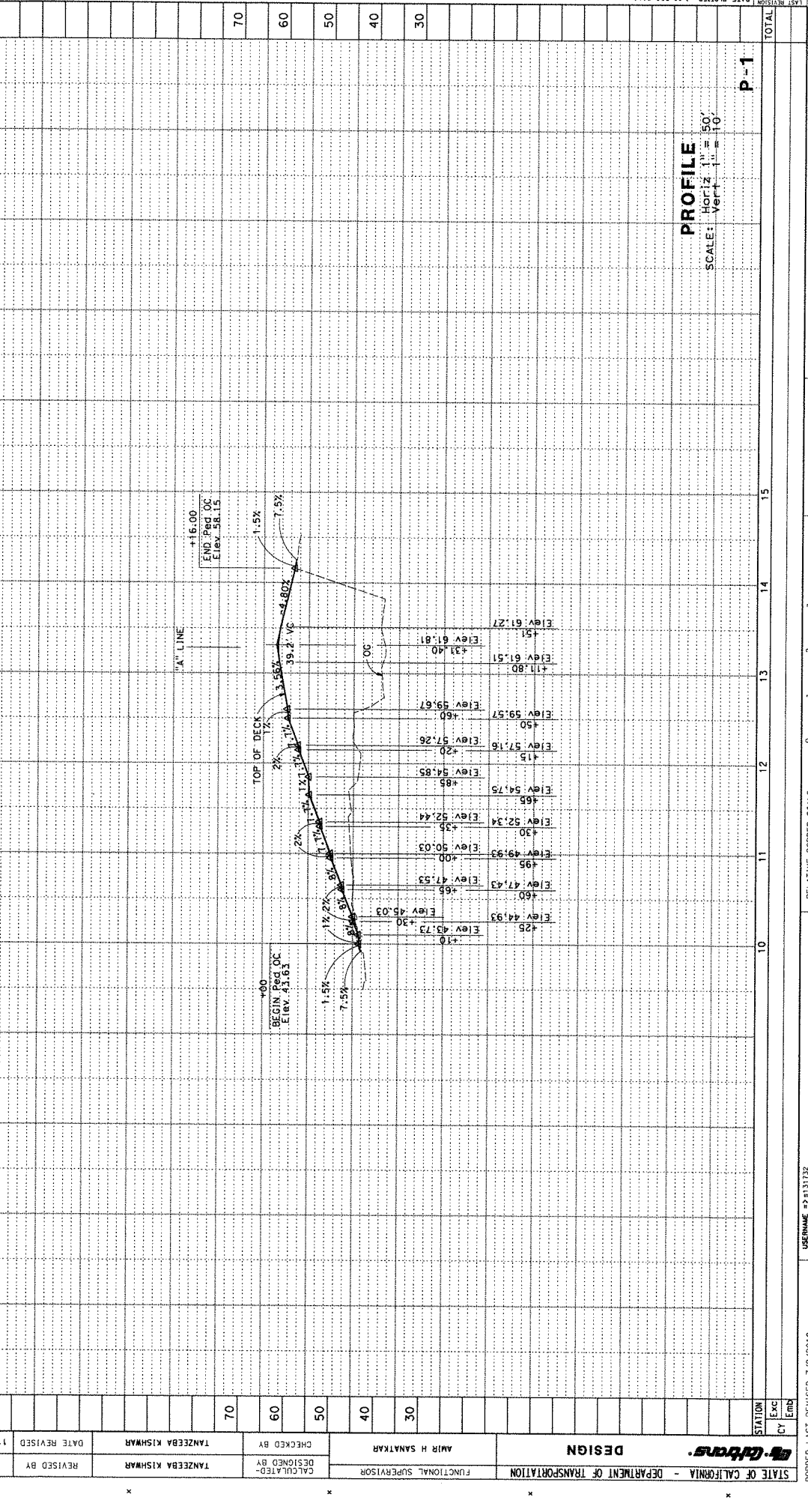
REGISTERED CIVIL ENGINEER: DATE: PROJECT NO.: SHEET NO.: TOTAL SHEETS: 44.1 6 A

REGISTERED CIVIL ENGINEER: DATE: PROJECT NO.: SHEET NO.: TOTAL SHEETS: 44.1 6 A

REGISTERED CIVIL ENGINEER: DATE: PROJECT NO.: SHEET NO.: TOTAL SHEETS: 44.1 6 A

REGISTERED CIVIL ENGINEER: DATE: PROJECT NO.: SHEET NO.: TOTAL SHEETS: 44.1 6 A

REGISTERED CIVIL ENGINEER: DATE: PROJECT NO.: SHEET NO.: TOTAL SHEETS: 44.1 6 A



PROFILE
SCALE: Horiz 1" = 50', Vert 1" = 10'

P-1

STATION	Exc	Emb	CY
70			
60			
50			
40			
30			
TOTAL			

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	FUNCTIONAL SUPERVISOR	AMR H SAAKAR	CHECKED BY	TANZEBBA KISHWAR	DATE REVISION	11-00-16
DESIGNED BY	TANZEBBA KISHWAR	REVISION BY	K			
DESIGNED BY	TANZEBBA KISHWAR	DATE REVISION	11-00-16			

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

FUNCTIONAL SUPERVISOR: AMR H SAAKAR

CHECKED BY: TANZEBBA KISHWAR

DATE REVISION: 11-00-16

DESIGNED BY: TANZEBBA KISHWAR

REVISION BY: K

PROJECT NUMBER & PHASE: UNIT 0720

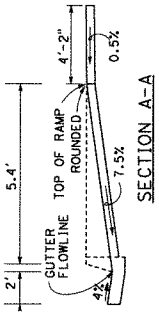
PROJECT NUMBER & PHASE: 04130000521

RELATIVE BORDER SCALE: 1" = 10'

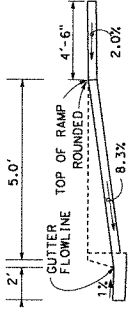
DATE PLOTTED: 06-DEC-2018

TIME PLOTTED: 11:17

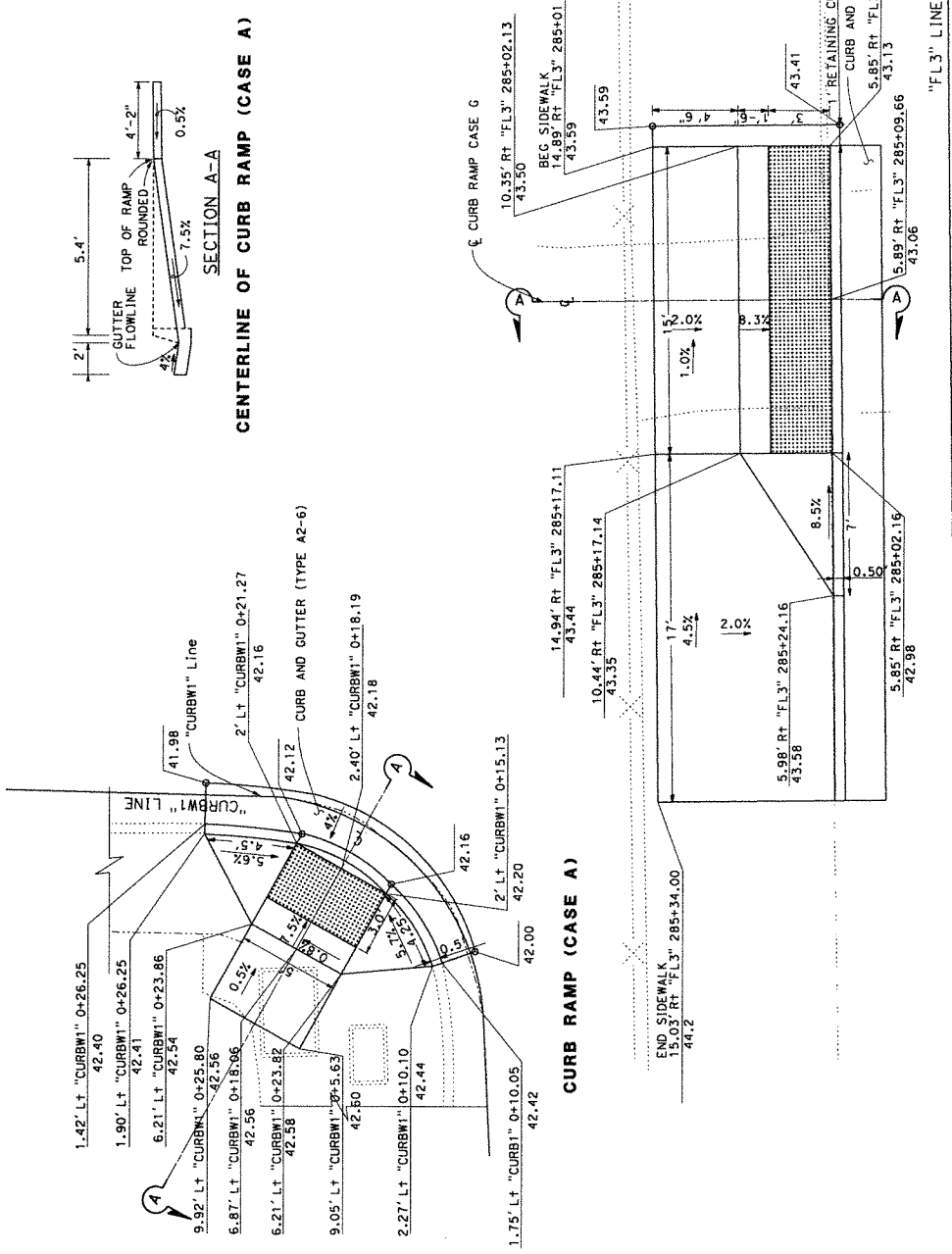
DATE	COUNTY	ROUTE	POST MILES	SHEET TOTAL
04	SM	1	44.1	7
REGISTERED CIVIL ENGINEER			DATE	NO. OF SHEETS
PLANS APPROVAL DATE				7
RESISTANCE				A
THE STATE OF CALIFORNIA OR ITS OFFICERS SHALL NOT BE HELD RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF THE COPIES OF THIS PLAN SHEET.				



CENTERLINE OF CURB RAMP (CASE A)



CENTERLINE OF CURB RAMP (CASE G)



CURB RAMP (CASE G)

FRANCISCO BLVD
CURB RAMP AND SIDE WALK

CONSTRUCTION DETAILS
NO SCALE

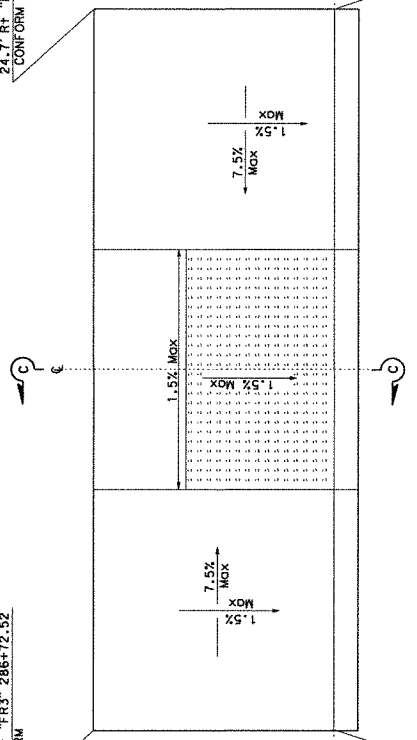
C-1

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	DESIGN	AMIR H SANATKAR	CHECKED BY	TANZEDA KISHWAR	REVISD BY	TK
FUNCTIONAL SUPERVISOR		AMIR H SANATKAR	DESIGNED BY	AMIR H SANATKAR	DATE REVISED	12-05-16
BORDER LAST REVISED 7/2/2010	USERNAME: a2131732 DGN FILE: 0413000052a001.dgn	RELATIVE BORDER SCALE 15 IN INCHES	UNIT 0720	PROJECT NUMBER & PHASE 04130000521		

POST MILES TOTAL PROJECT	44.1	SHEET NO.	8	TOTAL SHEETS	8
ROUTE	1	DATE		REGISTERED CIVIL ENGINEER	
COUNTY	SM	DATE		REGISTERED CIVIL ENGINEER	
DIST	04	DATE		REGISTERED CIVIL ENGINEER	

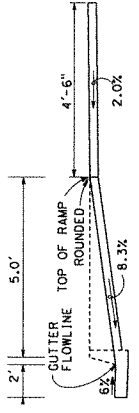
END SIDEWALK
24.7' RT "FR3" 286+57.50
CONFORM

86.0 SIDEWALK
24.7' RT "FR3" 286+72.52
CONFORM



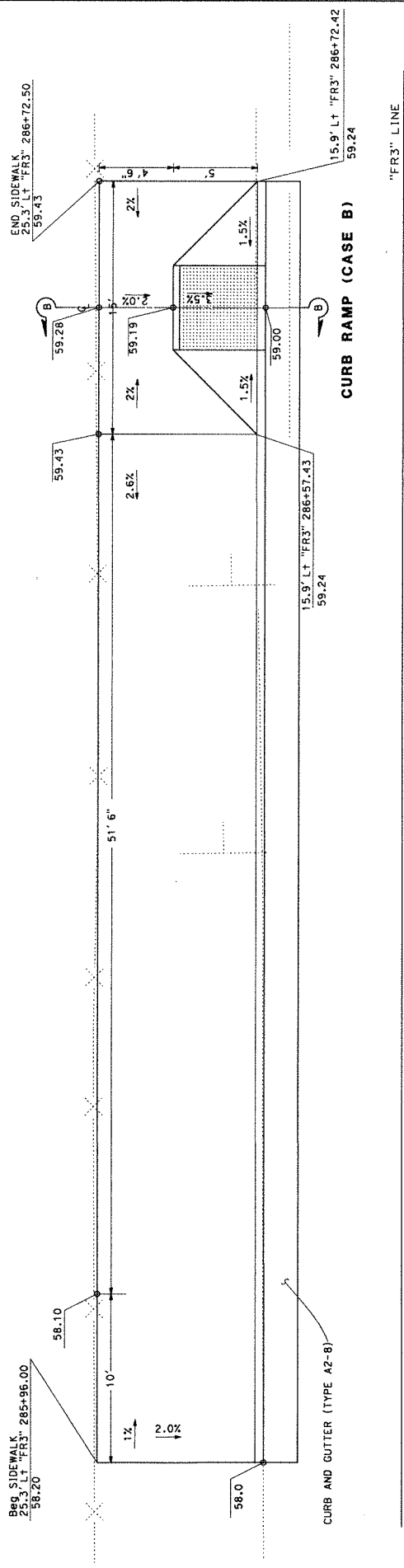
CENTERLINE OF CURB RAMP (CASE C)
SECTION C-C

CURB RAMP (CASE C)



CENTERLINE OF CURB RAMP (CASE B)
SECTION B-B

"FR3" LINE



CURB RAMP (CASE B)

"FR3" LINE

OCEANA Blvd
CURB RAMP AND SIDE WALK

CONSTRUCTION DETAILS
NO SCALE

FOR NOTES, ABBREVIATIONS
AND LEGEND, SEE SHEET C-1

DATE PLOTTED = 06-DEC-2016
TIME PLOTTED = 15:17

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	DESIGN	AMR H SANAKAR	CHECKED BY	TANZEEBA KISHMAR	DATE REVISED	11-00-16	REVISOR	TK
FUNCTIONAL SUPERVISOR			DESIGNED BY	TANZEEBA KISHMAR				
			CALCULATED BY	TANZEEBA KISHMAR				

WATER POLLUTION CONTROL QUANTITIES

WQ-1

TEMPORARY WATER POLLUTION CONTROL QUANTITIES

STATION	SIDE	TEMPORARY SILT FENCE	TEMPORARY FIBER ROLL	TEMPORARY DRAINAGE INLET PROTECTION	TEMPORARY HYDRAULIC MULCH (BONDED FIBER MATRIX)	TEMPORARY COVER
		LF	LF	EA	SQYD	SQYD
"C" 285+00	Med	120	120	1	570	
"C" 285+00 TO 286+20	RT	200	400		880	
"C" 285+80 TO 286+20	Med	320	520	1	1450	150
TOTAL						150

DIR#	COUNTY	ROUTE	SHEET NUMBER	TOTAL PROJECT SHEETS
04	SM	1	44.1	9
REGISTERED CIVIL ENGINEER DATE				
PLANS APPROVAL DATE				

DATE	11-00-16
REVISION	DL
DESIGNED BY	DIXON LAU
CHECKED BY	TANZEBKA KISHWAR
DATE	11-00-16
PROJECT	POST MILES TOTAL PROJECT 44.1
COUNTY	SM
ROUTE	1
SHEET NO.	8
TOTAL SHEETS	8

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

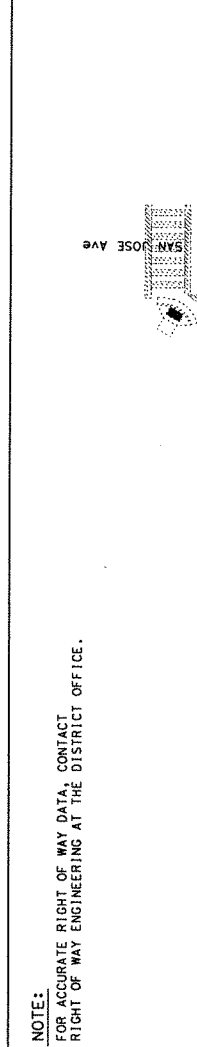
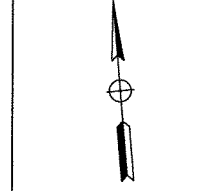
THE STATE OF CALIFORNIA OFFICE OF THE REGISTERED PROFESSIONAL ENGINEERS

REGISTERED CIVIL ENGINEER DATE

REGISTERED PROFESSIONAL ENGINEER

REGISTERED CIVIL ENGINEER DATE

REGISTERED PROFESSIONAL ENGINEER



NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

- NOTES:**
1. PROJECT VERTICAL DATUM IS NAVD88.
 2. UNLESS OTHERWISE NOTED, STATION AND OFFSET TIES TO EXISTING STRUCTURES ARE AS SHOWN ON DRAINAGE PROFILE SHEET DP-1.
 3. ALL TIES ARE TO THE CENTER OF DRAINAGE INLET OR MANHOLES UNLESS OTHERWISE NOTED.
 4. THE LOCATIONS OF EXISTING DRAINAGE FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE. WHERE A PROPOSED DRAINAGE FACILITY IS TO BE LOCATED AT AN EXISTING DRAINAGE FACILITY, FIELD VERIFY THE LOCATION AND INVERT ELEVATION OF THE EXISTING FACILITY. NOTIFY THE ENGINEER OF ANY DISCREPANCIES FOR ANY PORTION OF THE PROPOSED DRAINAGE SYSTEM.
 5. CONSTRUCT DRAINAGE SYSTEM (1) IN STAGE 1 PHASE 1.
 6. FLOWLINES FOR UNIT 9 AND 9' 18" RCP ARE TO CONFORM TO EXISTING 18" RCP.

ABBREVIATIONS:

RCP REINFORCED CONCRETE PIPE

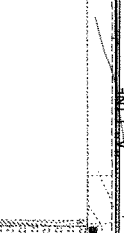
GCP CONCRETE PIPE INLET WITH GRATE

LEGEND:

SD NEW STORM DRAIN

X-X-X-X REMOVE EXISTING PIPE

DRAINAGE INLET: STORM DRAIN



APPROVED FOR DRAINAGE WORK ONLY

PROJECT NUMBER & PHASE 04130000521

UNIT 0720

DATE PLOTTED 05-DEC-2016

TIME PLOTTED 15:17

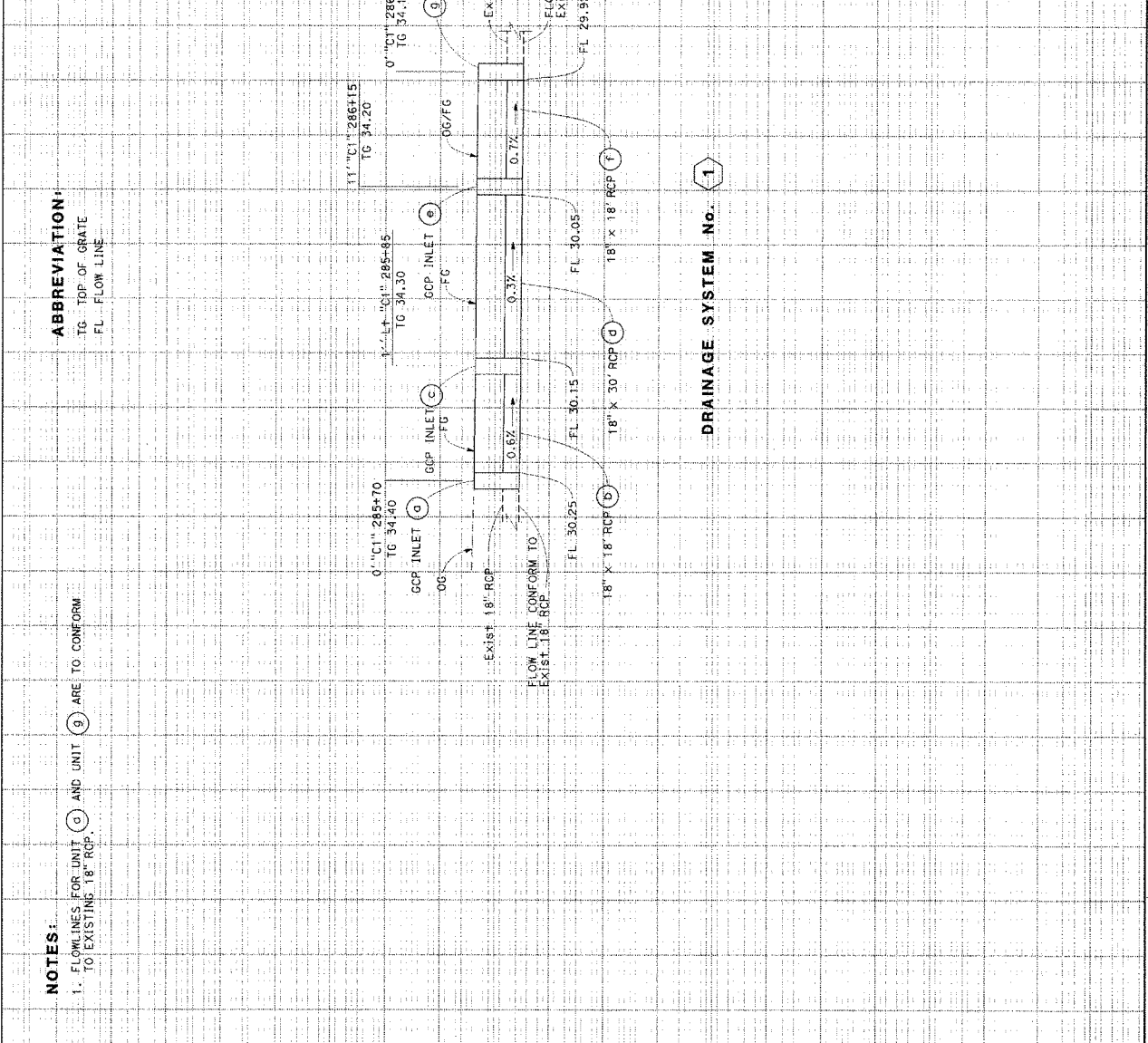
11-17-16

04	SM	1	44.1	9	A
----	----	---	------	---	---

DRAINAGE PROFILES

SCALE: Horiz 1" = 10'
 Vert 1" = 10'
DP - 1

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 HYDRAULICS
 PROJECT NUMBER & PHASE
 UNIT 0720
 PROJECT NUMBER & PHASE
 04130000521



40	35	30	25	20	15	10
----	----	----	----	----	----	----

REVISIONS:
 DATE REVISD BY
 11-00-16 DL
 DIXON LAU
 TANZEEBA KISHMAR

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
 HYDRAULICS
 F. J. LAJTHA, REGISTERED CIVIL ENGINEER

NOTES:

- FOR MINIMUM ALLOWABLE CLASS OF RCP, SEE STD PLAN 079.
- ALL GCP INLETS, USE FRAME AND GRATE (TYPE R36).

DRAINAGE QUANTITIES

DRAINAGE SHEET NO.	DRAINAGE SYSTEM NO.	DRAINAGE UNIT	REMOVE CULVERT	18" RCP	36" GCP INLET	MISCELLANEOUS IRON AND STEEL	FRAME AND GRATE TYPE R36	HEIGHT OF COVER (N)	HEIGHT OF INLET (N)	PIPE JOINT CLASSIFICATION	DESCRIPTION	STATION	DRAINAGE SYSTEM NO.	DRAINAGE UNIT
D-1	1	0	LF	4.2	236	1	4.2	2.5	4.2	S	GCP INLET	0" "C1" 285+70	1	0
		b	18	4.2	236	1	4.2	2.5	4.2	S	18" RCP	0" "C1" 285+70 TO 11' LT "C" 285+85		b
		c	30	4.2	236	1	4.2	2.5	4.2	S	GCP INLET	11' LT "C1" 285+85		c
		e	18	4.2	236	1	4.2	2.5	4.2	S	18" RCP	11' LT "C1" 285+85 TO 11' LT "C1" 286+15		e
		f	18	4.2	236	1	4.2	2.5	4.2	S	GCP INLET	11' "C1" 286+15		f
		g	18	4.2	236	1	4.2	2.5	4.2	S	18" RCP	11' LT "C1" 286+15 TO 0" "C" 286+30		g
		h	60	4.2	236	1	4.2	2.5	4.2	S	GCP INLET	0" "C1" 286+30		h
		i	60	4.2	236	1	4.2	2.5	4.2	S	REMOVE EXISTING 18" RCP	0" "C1" 285+70 TO 0" "C1" 286+30		i
SHEET TOTAL			60	66	16.8	949	4	7.5	16.8					

(N) NOT A SEPARATE PAY ITEM, FOR INFORMATION ONLY

DIST COUNTY ROUTE DIST VALUE SHEET TOTAL PROJECT SHEET TOTAL SHEETS
 04 SM 1 44.1 10 A

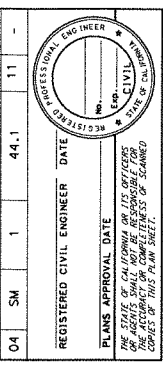
REGISTERED CIVIL ENGINEER DATE
 PROFESSIONAL ENGINEER
 CIVIL
 STATE OF CALIFORNIA

PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS
 OR AGENTS SHALL NOT BE RESPONSIBLE FOR
 THE ACCURACY OR COMPLETENESS OF ANY
 COPIES OF THIS PLAN SHEET.

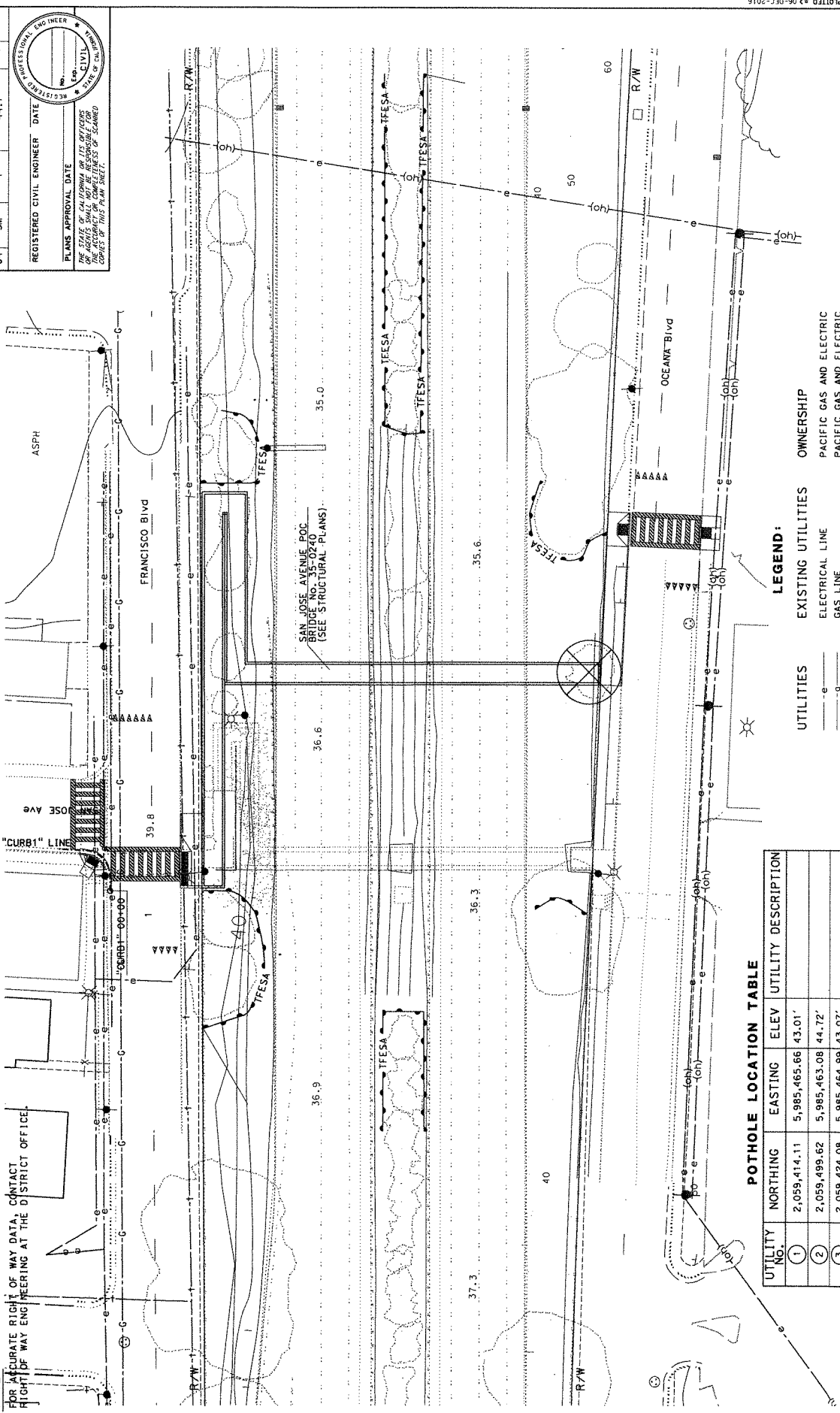
DRAINAGE QUANTITIES

D0-1

DATE	COUNTY	ROUTE	POST MILES	SHEET TOTAL
04	SM	1	44.1	11
REGISTERED CIVIL ENGINEER DATE				
PLANS APPROVAL DATE				
THE STATE OF CALIFORNIA OR ITS OFFICERS THE REGISTERED CIVIL ENGINEER THE REGISTERED CIVIL ENGINEER'S COPIES OF THIS PLAN SHEET				



NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



POTHOLE LOCATION TABLE

UTILITY No.	NORTHING	EASTING	ELEV	UTILITY DESCRIPTION
1	2,059,414.11	5,985,465.66	43.01'	
2	2,059,499.62	5,985,463.08	44.72'	
3	2,059,424.08	5,985,464.99	43.07'	
4	2,059,511.87	5,985,427.75	43.72'	

LEGEND:

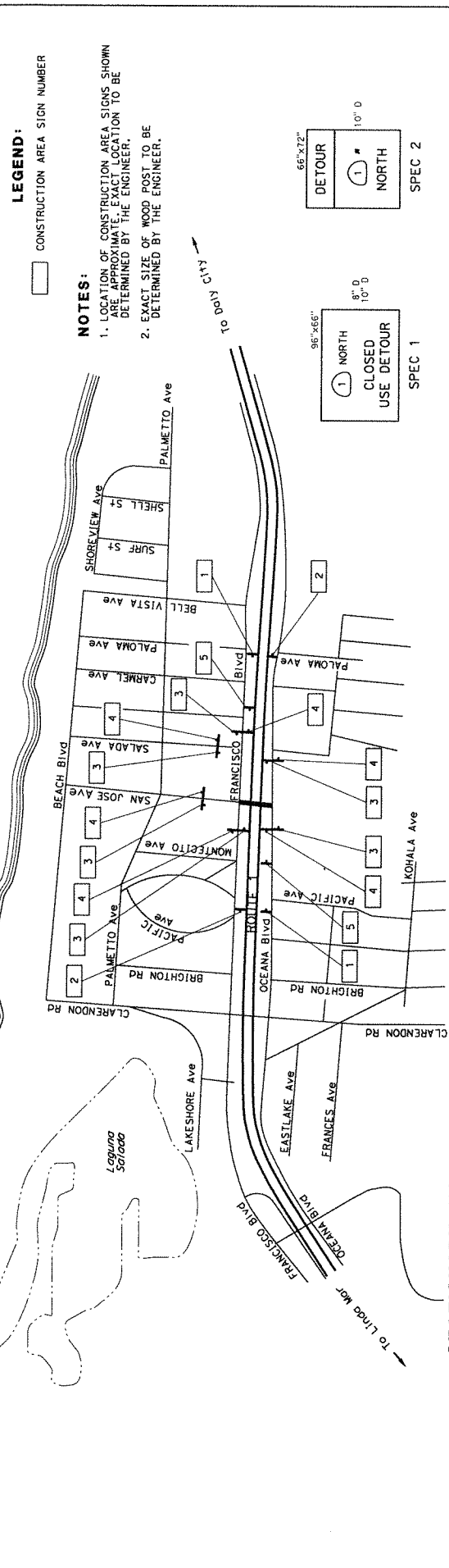
UTILITIES	EXISTING UTILITIES	OWNERSHIP
— e —	ELECTRICAL LINE	PACIFIC GAS AND ELECTRIC
— g —	GAS LINE	PACIFIC GAS AND ELECTRIC
— (oh) —	ELECTRICAL LINE	PACIFIC GAS AND ELECTRIC

UTILITY PLAN
SCALE: 1" = 50'

U-1

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	SM	1	44.1	10	A

REGISTERED CIVIL ENGINEER DATE
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS SHALL NOT BE HELD RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ANY INFORMATION OR DATA SHOWN ON THESE PLANS UNLESS IT IS SPECIFICALLY NOTED OTHERWISE.
 REGISTERED PROFESSIONAL ENGINEER
 CIVIL
 STATE OF CALIFORNIA



STATIONARY MOUNTED CONSTRUCTION AREA SIGNS

SIGN NO.	MUTCD CODE	MESSAGE	PANEL SIZE	NUMBER OF POSTS AND SIZE	NUMBER OF SIGNS
1	W20-1	ROAD WORK AHEAD	48" x 48"	(ONE) - 6" x 6"	2
2	G20-2	END ROAD WORK	48" x 24"	(ONE) - 4" x 6"	2
3	W20-1	ROAD WORK AHEAD	36" x 36"	(ONE) - 4" x 6"	6
4	G20-2	END ROAD WORK	36" x 18"	(ONE) - 4" x 4"	6
5	W1-5		48" x 48"	(ONE) - 6" x 6"	2
6	SPEC 1	1 NORTH-CLOSED-USE DETOUR	96" x 66"	(TWO) - 6" x 8"	1
7	SPEC 2	DETOUR-1 NORTH	66" x 72"	(TWO) - 6" x 6"	1
8	G28-2 (CA)	1 NORTH	48" x 18"	(ONE) - 4" x 6"	4
	M3-1	DETOUR	30" x 15"		
	M4-8	DETOUR	24" x 25"		
9	G28-2 (CA)	1 NORTH	24" x 25"	(ONE) - 4" x 6"	1
	M3-1	DETOUR	30" x 15"		
	M6-2	DETOUR	21" x 15"		

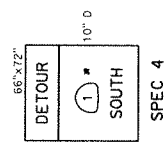
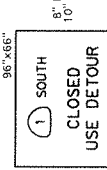
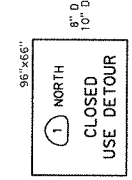
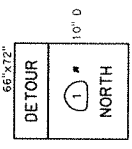
STATIONARY MOUNTED CONSTRUCTION AREA SIGNS

SIGN NO.	MUTCD CODE	MESSAGE	PANEL SIZE	NUMBER OF POSTS AND SIZE	NUMBER OF SIGNS
10	SPEC 3	1 SOUTH-CLOSED-USE DETOUR	96" x 66"	(TWO) - 6" x 8"	1
11	SPEC 4	DETOUR-1 SOUTH	66" x 72"	(TWO) - 6" x 6"	1
12	G28-2 (CA)	1 SOUTH	48" x 18"	(ONE) - 4" x 6"	5
	M3-3	DETOUR	24" x 25"		
	M4-8	DETOUR	30" x 15"		
13	G28-2 (CA)	1 SOUTH	30" x 15"	(ONE) - 4" x 6"	1
	M3-3	DETOUR	24" x 25"		
	M6-2	DETOUR	21" x 15"		
14	SC6-4 (CA)	RAMP CLOSED (Date) [Time]	48" x 60"	(ONE) - 6" x 6"	2

LEGEND:
 CONSTRUCTION AREA SIGN NUMBER

NOTES:

1. LOCATION OF CONSTRUCTION AREA SIGNS SHOWN ARE APPROXIMATE. EXACT LOCATION TO BE DETERMINED BY THE ENGINEER.
2. EXACT SIZE OF WOOD POST TO BE DETERMINED BY THE ENGINEER.



CONSTRUCTION AREA SIGNS
 NO SCALE
CS-1

DIRT COUNTY	ROUTE	POST MILES	SHEET TOTAL	NO. SHEETS	DATE	REGISTERED CIVIL ENGINEER	DATE	REGISTERED PROFESSIONAL ENGINEER
04	SM	1	44.1	X	A			

PLANS APPROVAL DATE _____

REGISTERED CIVIL ENGINEER _____

DATE _____

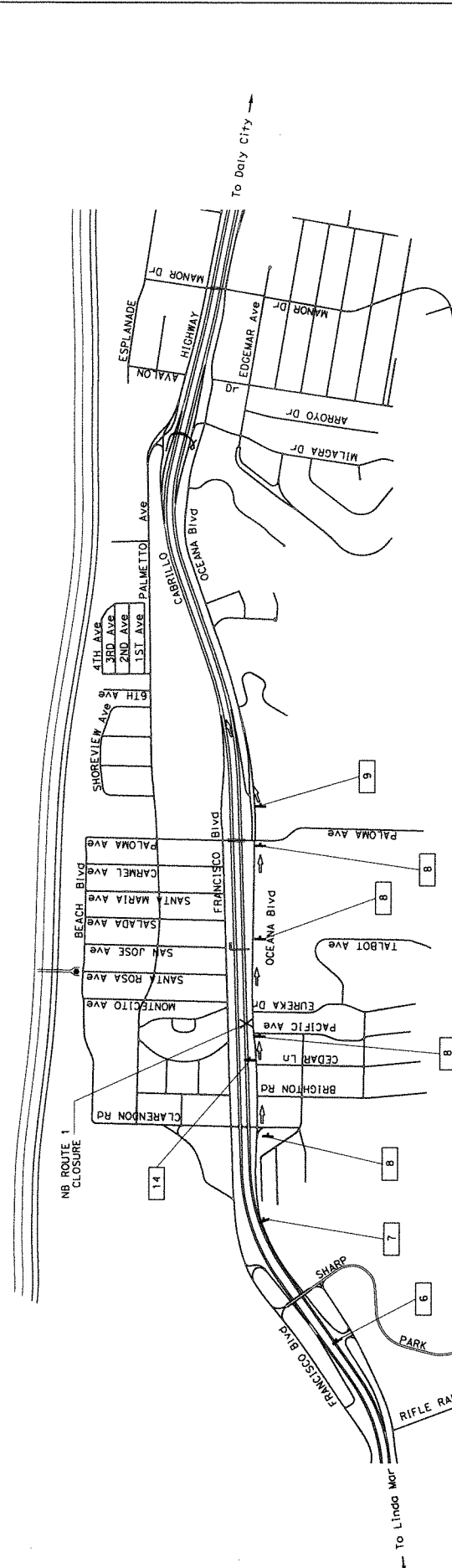
REGISTERED PROFESSIONAL ENGINEER _____

NO. _____

CIVIL _____

STATE OF CALIFORNIA

THE STATE OF CALIFORNIA OFFICE OF THE REGISTERED PROFESSIONAL ENGINEERS HAS REVIEWED THIS PLAN SHEET FOR THE ACCURACY OF THE INFORMATION THEREON AND THE COMPLETENESS OF THE SCANNED COPIES OF THIS PLAN SHEET.



DETOUR PLAN No. 1
NB ROUTE 1 COMPLETE FREEWAY CLOSURE
 FROM NB 001
 VIA: NB ROUTE 001;
 OFF-RAMP TO CLARENDON Rd/OCEANA BIVG;
 NB OCEANA BIVG;
 ON-RAMP TO NB ROUTE 1

CONSTRUCTION AREA SIGNS
 NO SCALE

APPROVED FOR CONSTRUCTION AREA SIGN WORK ONLY

PROJECT NUMBER & PHASE	UNIT	0720
RELATIVE BORDER SCALE	0 1 2 3	
RELATIVE BORDER SCALE	15' IN INCHES	

DATE PLOTTED → 06-DEC-2016

TIME PLOTTED → 15:18

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	DESIGN	CHECKED BY	RAJESH OBERI	DATE REVISD	
FUNCTIONAL SUPERVISOR	RAJESH OBERI	CHECKED BY	RAJESH OBERI	DATE REVISD	
DESIGNED BY	ULICES VEGA	CHECKED BY	ULICES VEGA	DATE REVISD	
REVISD BY		CHECKED BY		DATE REVISD	

BORDER LAST REVISED 7/2/2010

USERNAME → 8131732

PKY FILE → 041300005210002.dgn

PROJECT NUMBER & PHASE

UNIT 0720

04130000521

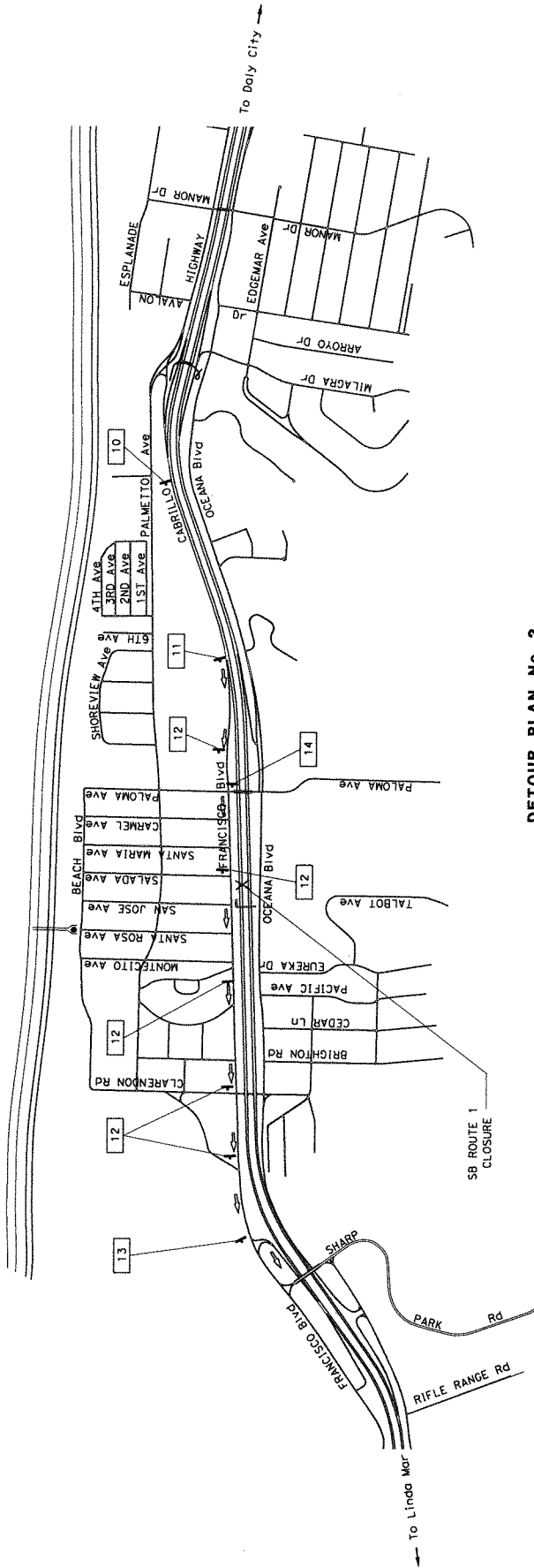
CS-2

DIR#	COUNTY	ROUTE	ASST. PROJECT SHEET NO.	TOTAL PROJECT SHEETS
04	SM	1	44.1	X

REGISTERED CIVIL ENGINEER DATE _____
 PROFESSIONAL ENGINEER
 No. _____
 STATE OF CALIFORNIA
 CIVIL
 THE STATE OF CALIFORNIA OFFICE OF THE REGISTERED PROFESSIONAL ENGINEERS AND SURVEYORS SHALL BE RESPONSIBLE FOR THE ACCURACY OF THIS PLAN SHEET.



PACIFIC OCEAN



DETOUR PLAN No. 2
 SB ROUTE 1 COMPLETE FREEWAY CLOSURE
 FROM SB 001
 FROM SB ROUTE 001
 VIA: SB ROUTE 001
 OFF-RAMP TO PALOMA AVE/FRANCISCO BLVD;
 SB FRANCISCO BLVD;
 ON-RAMP TO SB ROUTE 1

SB ROUTE 1 CLOSURE

CONSTRUCTION AREA SIGNS
 NO SCALE

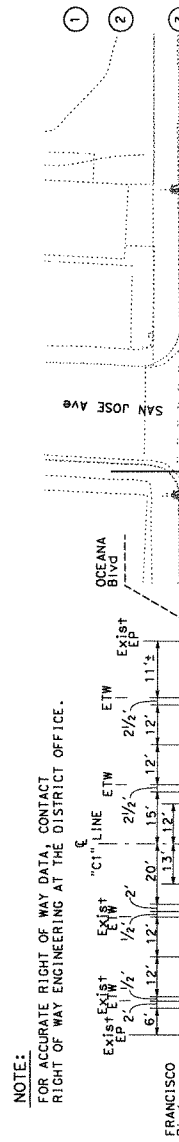
CS-3

APPROVED FOR CONSTRUCTION AREA SIGN WORK ONLY

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	DESIGN	RAJESH OBERI	CHECKED BY	RAJESH OBERI	DATE REVISED
FUNCTIONAL SUPERVISOR	RAJESH OBERI	CHECKED BY	RAJESH OBERI	DATE REVISED	
DESIGNED BY	LILICES VEGA	REVISD BY			

DATE REVISION	11-10-16	REVISION	TK
DATE REVISION	11-00-16	REVISION	

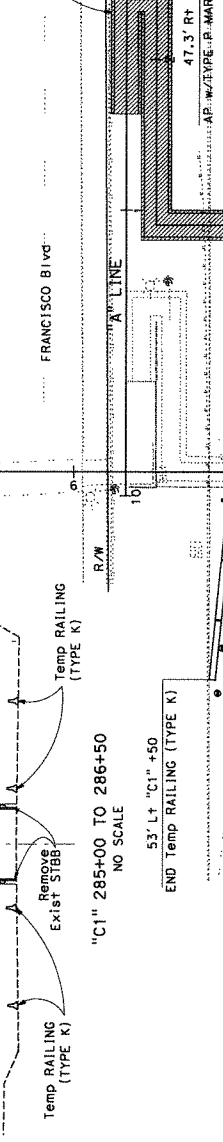
NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



CALCULATED/DESIGNED BY	TANZEBBA KISHWAR
CHECKED BY	TANZEBBA KISHWAR
FUNCTIONAL SUPERVISOR	AMR H SANATKAR

DATE REVISION	11-10-16	REVISION	TK
DATE REVISION	11-00-16	REVISION	

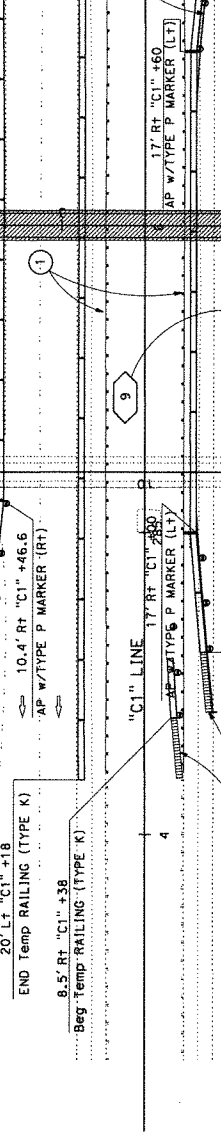
CONSTRUCTION:
1 REMOVE PORTION OF STBB ALONG MEDIAN FROM "C1" 284+18 TO 288+74.
2 CONSTRUCT POC BRIDGE STRUCTURE (CIP/R/C SLAB AND CIP/R/C BOX GIRDER PORTION). FOR DETAILS SEE STRUCTURAL PLANS.
3 CONSTRUCT SIDEWALK AND CURB RAMP ALONG OCEANA BLVD.



REGISTERED CIVIL ENGINEER	DATE	PROFESSIONAL NO.	SHEET NO.	TOTAL SHEETS
			44.1	12

TRAFFIC:
A FOR NB ROUTE 1 RE-STRIPE TRAFFIC LANES FROM "C1" 284+40 TO 287+00
B FOR OCEANA BLVD RE-STRIPE CENTERLINE FROM "FR3" 285+35 TO 286+88

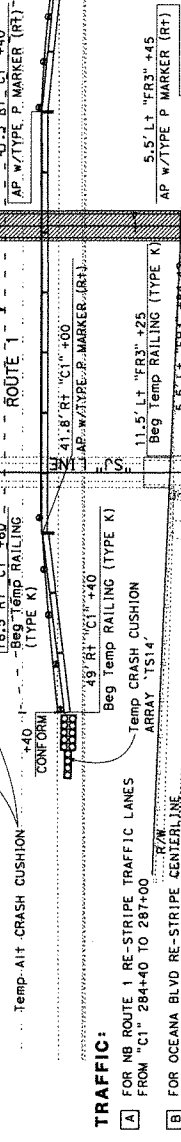
NOTES:
1. UNLESS OTHERWISE NOTED, ALL TRAFFIC LANES MUST BE 11'-0" MIN. WIDE.
2. ALL STATIONING AND OFFSETS OF TEMPORARY RAILING (TYPE K) CORRESPOND TO THE NEAREST FACE OF TEMPORARY RAILING FROM THE ALIGNMENT LINE.
3. PRIOR TO OPENING ROADWAY TO TRAFFIC, ALL EXISTING OR TEMPORARY PAVEMENT DELINEATION AND SIGNS IN CONFLICT WITH TRAFFIC HANDLING PLAN MUST BE REMOVED OR COVERED.
4. A TYPE P MARKER PANEL SHALL BE INSTALLED AT THE ANGLE POINT OF EACH APPROACH TAPER OF TEMPORARY RAILING (TYPE K).
5. CHANNELIZERS (SURFACE MOUNTED) SHOWN ARE SPACED 20' C-C MAX.
6. ALL TEMP RAILING AND CRASH CUSHION FROM PREVIOUS PHASE SHOULD BE REMOVED UNLESS OTHERWISE NOTED.
7. ALL TEMPORARY TRAFFIC STRIPES ARE TO BE 2-COAT PAINTED LINES.



DESIGN	DEPARTMENT OF TRANSPORTATION
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STAGE CONSTRUCTION AND TRAFFIC HANDLING PLAN
STAGE 1
NO SCALE

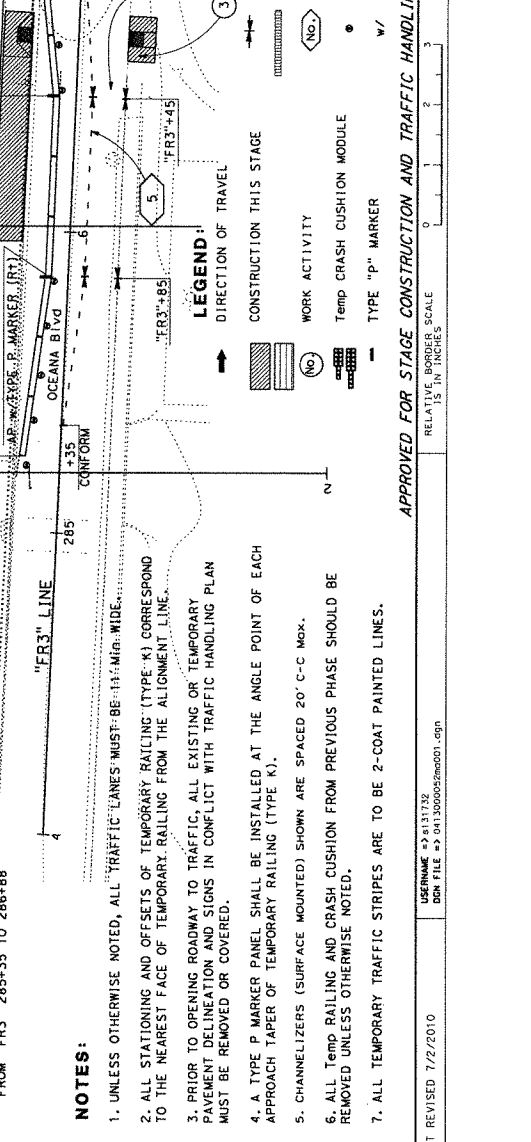
LEGEND:
DIRECTION OF TRAVEL
CONSTRUCTION THIS STAGE
WORK ACTIVITY
LIMIT OF DESIGNATED TRAFFIC STRIPE
PAVEMENT DELINEATION DETAIL
CHANNELIZER (SURFACE MOUNTED)



APPROVED FOR STAGE CONSTRUCTION AND TRAFFIC HANDLING WORK ONLY	UNIT 0720	PROJECT NUMBER & PHASE	04130000521
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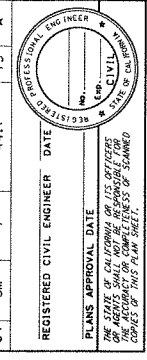
STATE OF CALIFORNIA	DEPARTMENT OF TRANSPORTATION
---------------------	------------------------------

APPROVED FOR STAGE CONSTRUCTION AND TRAFFIC HANDLING WORK ONLY



DATE REVISION	7/2/2010	USER NAME	04130000521.dgn
BORDER LAST REVISED	7/2/2010	DIR FILE	04130000521.dgn

Dist	County	Route	Post Miles	Total	Sheet	Total
04	SM	1	44.1	13	A	



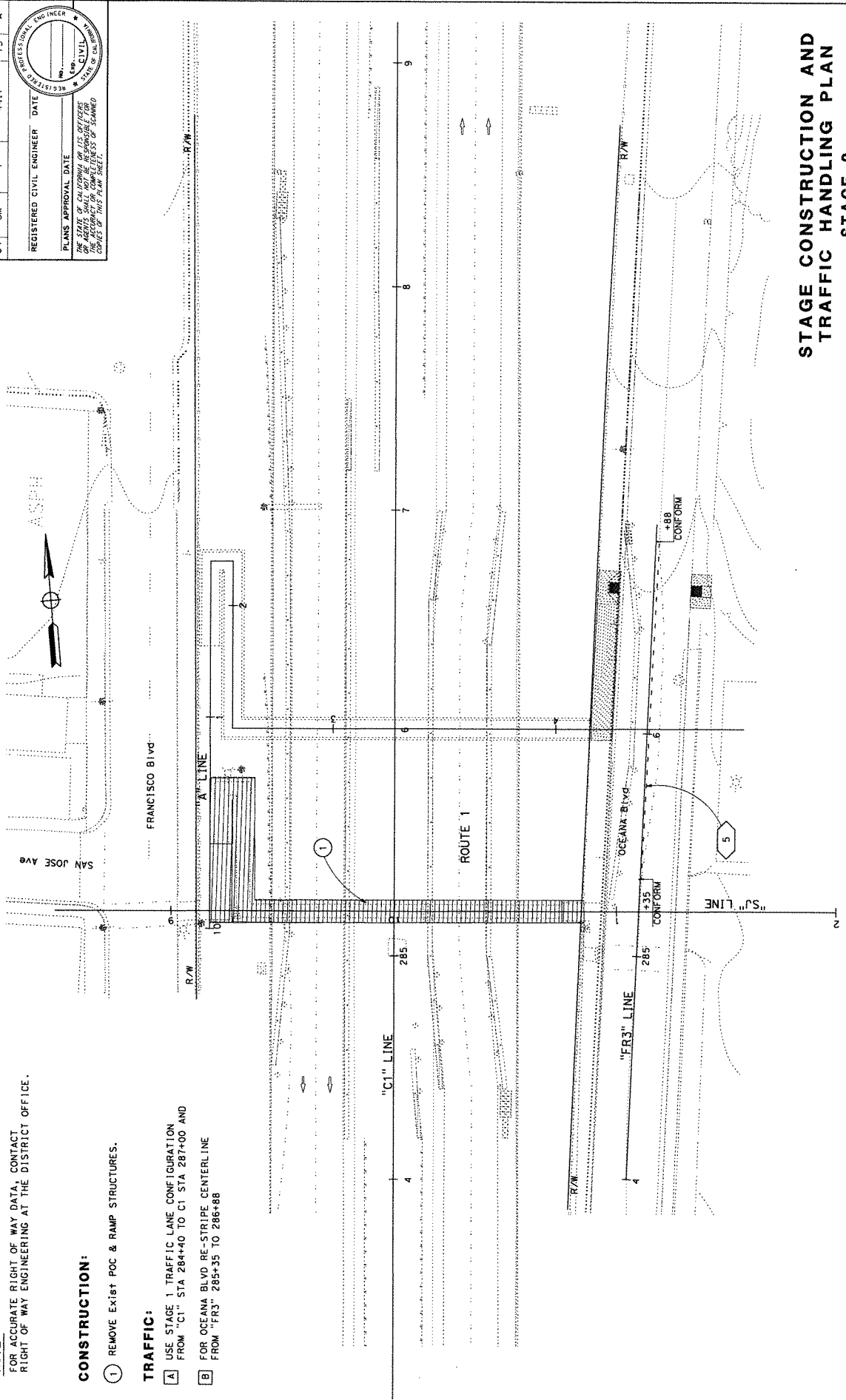
NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

CONSTRUCTION:

- 1 REMOVE Exist POC & RAMP STRUCTURES.

TRAFFIC:

- A USE STAGE 1 TRAFFIC LANE CONFIGURATION FROM "C1" STA 284+40 TO C1 STA 287+00 AND
- B FOR OCEANA BLVD RE-STRIPE CENTERLINE FROM "FR3" 285+35 TO 286+88



FOR NOTES, ABBREVIATIONS
AND LEGEND, SEE SHEET SC-1

**STAGE CONSTRUCTION AND
TRAFFIC HANDLING PLAN
STAGE 2**
NO. SCALE

APPROVED FOR STAGE CONSTRUCTION AND TRAFFIC HANDLING WORK ONLY

DATE PLOTTED: 08-05-2016
TIME PLOTTED: 15:18

PROJECT NUMBER & PHASE
04130000521

UNIT: 0120

RELATIVE BORDER SCALE
IS IN INCHES

USERNAME: s131732
ODM FILE: 041300052mod02.dgn

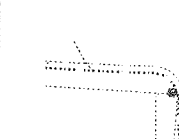
BORDER LAST REVISED: 7/2/2010

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION
DESIGN
FUNCTIONAL SUPERVISOR
AMIR H SAMATKAR
CHECKED BY
TANZEBDA KISHWAR
DESIGNED BY
TANZEBDA KISHWAR
DATE REVISID
11-00-16
REVISID BY
TK

SC-2

Dist	County	ROUTE	SHEET NO.	TOTAL SHEETS
04	SM	1	44.06	14
REGISTERED CIVIL ENGINEER		DATE	REVISED	
PLANS APPROVAL DATE		PROFESSIONAL ENGINEER		

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ANY INFORMATION CONTAINED HEREIN UNLESS SPECIFICALLY STATED OTHERWISE.



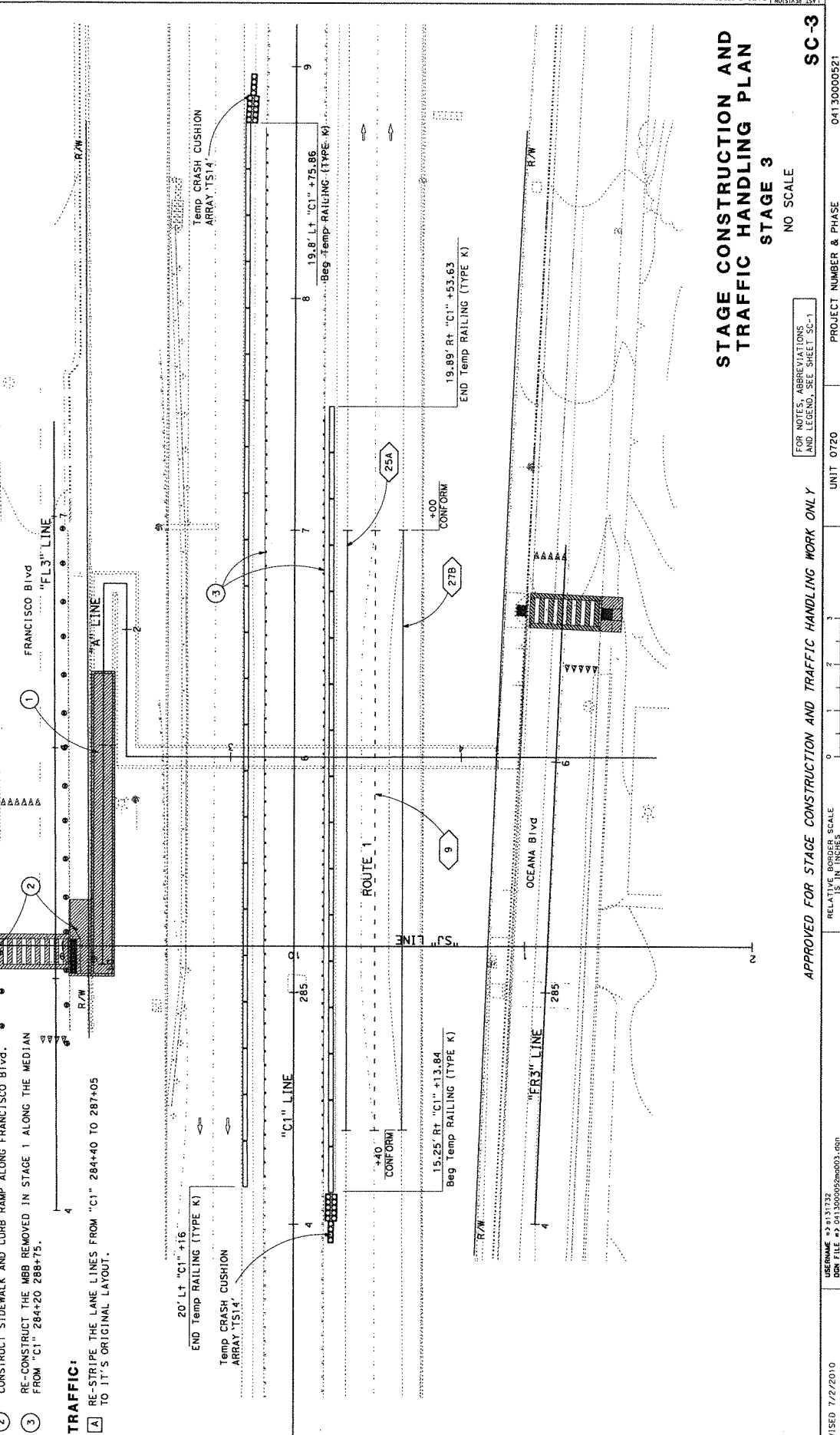
NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

CONSTRUCTION:

- 1 CONSTRUCT REMAINING PORTION OF POC BRIDGE STRUCTURE (CIP/RC CELL). FOR DETAILS SEE STRUCTURAL PLANS.
- 2 CONSTRUCT SIDEWALK AND CURB RAMP ALONG FRANCISCO BLVD.
- 3 RE-CONSTRUCT THE MBB REMOVED IN STAGE 1 ALONG THE MEDIAN FROM "C1" 284+20 TO 288+75.

TRAFFIC:

- A RE-STRIPE THE LANE LINES FROM "C1" 284+40 TO 287+05 TO IT'S ORIGINAL LAYOUT.



STAGE CONSTRUCTION AND TRAFFIC HANDLING PLAN
STAGE 3
NO SCALE

FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET SC-1

APPROVED FOR STAGE CONSTRUCTION AND TRAFFIC HANDLING WORK ONLY

RELATIVE NUMBER SCALE
1" = 15' IN INCHES

UNIT 0720 PROJECT NUMBER & PHASE
04130000521

DATE PLOTTED 05-06-2016 15:18
LAST REVISION 11-10-16

SC-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	DESIGN	FUNCTIONAL SUPERVISOR	AMIR H SANATKAR	CHECKED BY	TANZEBA KISHWAR	DATE REVISION	11-00-16
		DESIGNED BY	TANZEBA KISHWAR	REVISION BY	TK		

DIST	COUNTY	ROUTE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
04	SM	1	44.1	-	-

REGISTERED CIVIL ENGINEER DATE _____

PLANS APPROVAL DATE _____

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS ON THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER

NO. _____

CIVIL

STATE OF CALIFORNIA

TRAFFIC STRIPES, PAVEMENT MARKING AND PAVEMENT MARKERS

SHEET No.	STATION	DETAIL No. OR PAVEMENT MARKING	REMOVE PAVEMENT MARKER		REMOVE THERMOPLASTIC TRAFFIC STRIPE		TEMPORARY PAVEMENT MARKER		TEMPORARY TRAFFIC STRIPE (PAINT)			
			TYPE A	TYPE G	TYPE H	EA	* 4" YELLOW	4" WHITE	4" WHITE BROKEN	4" YELLOW	4" WHITE	4" WHITE BROKEN
	"C1" 284+40 TO 287+00	27B	EA			LF	EA					LF
	"C1" 284+40 TO 287+00	9	14			260	14					260
SC-1	"C1" 284+40 TO 287+00	25A		21								260
	"FR3" 285+35 TO 286+88	5										96
	"FR3" 285+35 TO 286+88	36C	80	32								96
SC-2	"FR3" 285+35 TO 286+88	5										96
	SUBTOTAL		80	46	21	356	14	25	452	260	452	98
	TOTAL		147			714	35					810

* YELLOW TRAFFIC STRIPE (HAZARDOUS WASTE)

STAGE CONSTRUCTION QUANTITIES

SCQ-1

PROJECT NUMBER & PHASE

04130000521

UNIT 0720

RELATIVE BORDER SCALE

0 1 2 3

15 IN INCHES

USER NAME → B131732

DDN FILE → 04130000521mc001.dgn

BORDER LAST REVISED 7/2/2010

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

DESIGN

AMIR H SANATKAR

FUNCTIONAL SUPERVISOR

AMIR H SANATKAR

CHECKED BY

TANZEBA KISHWAR

DESIGNED BY

AMIR H SANATKAR

DATE REVISED

11-00-16

REVISOR

TK

DIR#	COUNTY	ROUTE	POST MILE TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	SM	001	44.1		A

REGISTERED CIVIL ENGINEER DATE _____
 PROFESSIONAL ENGINEER No. _____
 THE STATE OF CALIFORNIA ON ITS OFFICIAL SEAL
 OR AGENTS SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS OR FOR THE CONSEQUENCES OF THIS PLAN SHEET.

CHANNELIZER (SURFACE-MOUNTED)

STAGE	SIDE	STATION	EA
1	ROUTE 1 NB	AS SHOWN ON SC-1	19
1	ROUTE 1 SB	AS SHOWN ON SC-1	14
1	OCEANA Blvd SB	AS SHOWN ON SC-1	09
3	FRANCISCO Blvd NB	AS SHOWN ON SC-3	15
3	FRANCISCO Blvd SB	AS SHOWN ON SC-3	05
TOTAL			62

TEMPORARY RAILING (TYPE K)

STATION	STAGE	SIDE	TEMPORARY RAILING (TYPE K)
"CI" 284+38 TO 287+00	1	NB	520
"CI" 284+18 TO 288+58	1	SB	820
"FR3" 285+25 TO 286+85	1	SB	160
"CI" 284+15.25 TO 287+53.63	3	NB	340
"CI" 284+16 TO 288+75.86	3	SB	460
TOTAL			2300

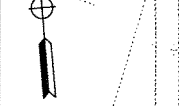
TEMPORARY ALTERNATIVE CRASH CUSHION

SHEET NO.	STATION	STAGE	SIDE	TEMPORARY CRASH CUSHION MODULE	OBJECT MARKER (TYPE P)	TEMPORARY ALTERNATIVE CRASH CUSHION
SC-1	"CI" 284+00 TO 289+00	1	NB	14	EA	2
SC-1	"CI" 284+00 TO 289+00	1	SB	14	4	2
SC-1	"FR3" 285+25 TO 286+85	1	SB	11	2	2
SC-3	"CI" 284+13.84 TO 287+53.63	3	NB	14		
SC-3	"CI" 284+16 TO 288+75.86	3	SB	14		
TOTAL				67	B	4

STAGE CONSTRUCTION QUANTITIES
SCQ-2

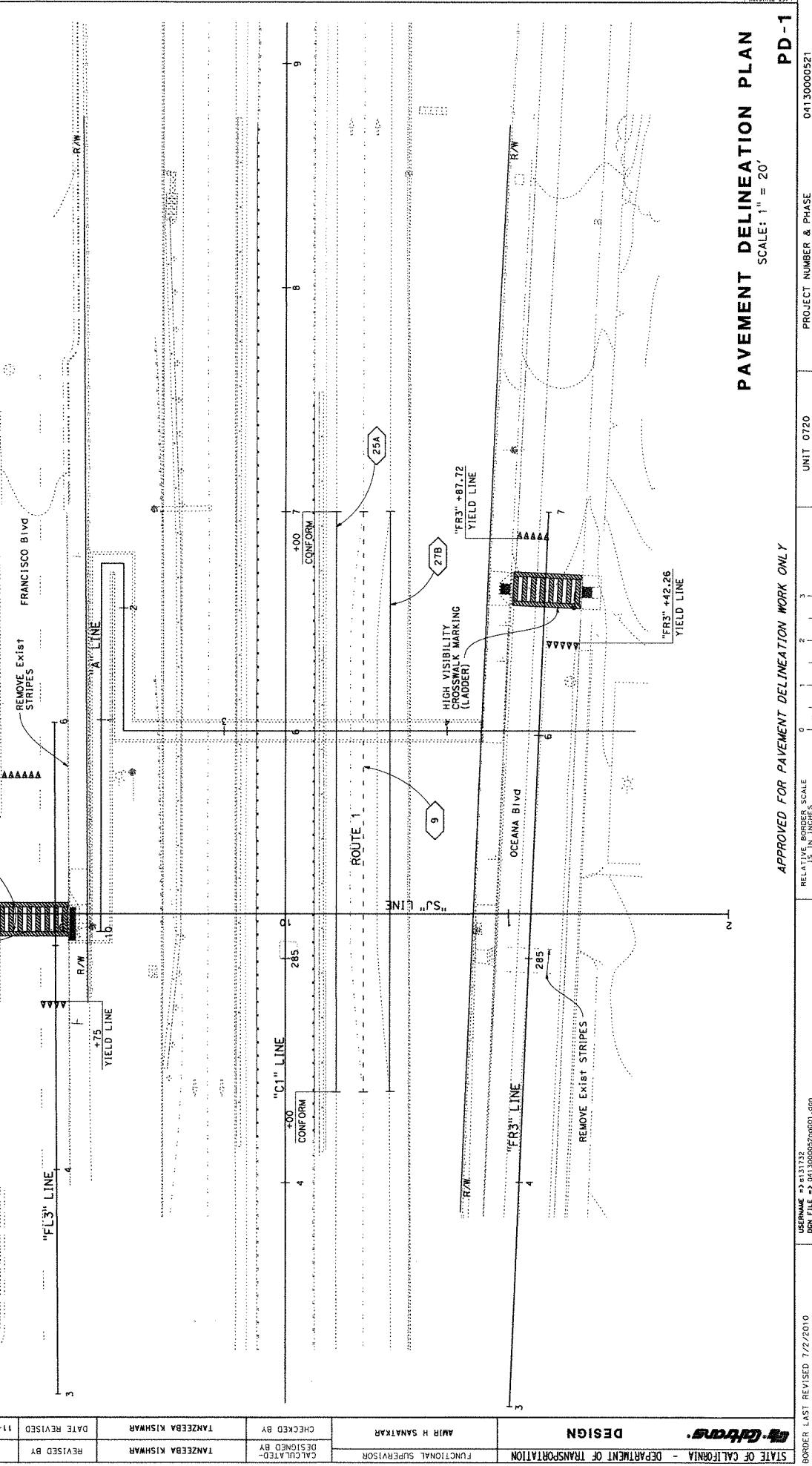
Dist	County	Route	Sheet No.	Total Sheets
04	SM	1	44.1	XX A

REGISTERED CIVIL ENGINEER
 DATE
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICES
 OR AGENTS SHALL NOT BE RESPONSIBLE FOR
 THE ACCURACY OF THIS PLAN SHEET.



NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

DATE REVISION	11-00-16
REVISION BY	TK



PAVEMENT DELINEATION PLAN
 SCALE: 1" = 20'

APPROVED FOR PAVEMENT DELINEATION WORK ONLY

RELATIVE BORDER SCALE
 15" IN FICHES

11-10-16 LINE PLOTTED => 09-DEC-2016 9:15:18

DIR#	COUNTY	ROUTE	PROJECT MILES	SHEET NO.	TOTAL SHEETS
04	SM	001	44.1	XX	XX

REGISTERED CIVIL ENGINEER DATE _____
 PLANS APPROVAL DATE _____
 THE STATE OF CALIFORNIA OFFICE OF THE REGISTERED PROFESSIONAL ENGINEERS
 THE ACCURACY OF THIS PLAN SHEET IS THE RESPONSIBILITY OF THE ENGINEER

TRAFFIC STRIPES, PAVEMENT MARKINGS AND PAVEMENT MARKERS

SHEET NO.	STATION	DETAIL NO. OR PAVEMENT MARKING	REMOVE PAVEMENT MARKER		REMOVE THERMOPLASTIC TRAFFIC STRIPE			REMOVE THERMOPLASTIC PAVEMENT MARKING			PAINT TRAFFIC STRIPE (2-COAT)			THERMOPLASTIC THERMOPLASTIC AND PAVEMENT MARKING (ENHANCED MET NIGHT VISIBILITY)
			TYPE G	TYPE H	* 4" YELLOW	4" WHITE	4" WHITE BROKEN	SOFT	SOFT	SOFT	4" YELLOW	4" WHITE	4" WHITE BROKEN	
PD-1	"C1" 284+40.00 TO 287+00.00	27B	EA		LF		SOFT							
	"C1" 284+40.00 TO 287+00.00	9	14		260		98					260	98	
	"C1" 284+40.00 TO 287+00.00	25A	21		260							260		
	"FR3" 286+40.00 TO 286+91.00	YIELD												30
	"FR3" 286+40.00 TO 286+91.00	CROSSWALK												298
	"FL3" 284+71.00 TO 285+80.00	YIELD												30
"FL3" 284+71.00 TO 285+80.00	CROSSWALK												579	
"FR3" 284+93.00 TO 285+05.00	CROSSWALK													
"FL3" 285+00.00 TO 285+95.00	CROSSWALK													
	SUBTOTAL		14	21	260	260	98	180	14	21	260	260	98	937
	TOTAL		35		618	618	180	35	618		618		937	

* YELLOW TRAFFIC STRIPE (HAZARDOUS WASTE)

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	DESIGN	AMIR H SANATKAR	FUNCTIONAL SUPERVISOR
TANZEEBA KISHWAR	DESIGNED BY	AMIR H SANATKAR	CHECKED BY
DATE REVISED	REVISOR	DATE REVISED	REVISOR

PAVEMENT DELINEATION QUANTITIES PDQ-1

PROJECT NUMBER & PHASE 04130000521

UNIT 0720

RELATIVE BORDER SCALE 15" = 1' INCHES

USERNAME: s131732
DIRN FILE: 0413000052pdq01.dgn

BORDER LAST REVISED 7/2/2010

DIS*	COUNTY	ROUTE	POST MILE TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	SM	1	44.1	33	139

REGISTERED CIVIL ENGINEER DATE _____





PLANS APPROVAL DATE _____

THE STATE OF CALIFORNIA OR ITS OFFICERS
 THE SIGNATURE OF THE REGISTERED CIVIL ENGINEER
 THE SIGNATURE OF THE REGISTERED CIVIL ENGINEER
 COPIES OF THIS PLAN SHEET.

NOTES:

1. EXACT LOCATION AND POSITION OF SIGNS TO BE DETERMINED BY THE ENGINEER.
2. SIGN POST LENGTH ARE APPROXIMATE EXACT SIZE AND LENGTH TO BE DETERMINED BY THE ENGINEER.
3. ALL EXISTING SIGNS NOT SHOWN FOR REMOVAL SHALL REMAIN IN PLACE.
4. FOR CONSTRUCTION AREA SIGNS SEE SHEET CS-1, CS-2 AND CS-3.

LEGEND:

- (NO)  ROADSIDE SIGN NUMBER
-  OVERHEAD SIGN LETTER
-  CONSTRUCTION AREA SIGN LETTER
-  REMOVE ROADSIDE SIGN

DESIGNED BY	ULICES VECA	REVISD BY	
CHECKED BY	RAJESH OBEROI	DATE REVISED	1-14-16
FUNCTIONAL SUPERVISOR	RAJESH OBEROI		
TRAFFIC			
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION			

SIGN PLAN
NO SCALE

APPROVED FOR SIGN WORK ONLY



BORDER LAST REVISED 7/2/2010

USERNAME: 278131732
 DGN FILE: 0413000050a001.dgn

PROJECT NUMBER & PHASE

UNIT 0720

04130000521

S-1

DIS#	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	SM	1	44.1		

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

REGISTERED PROFESSIONAL ENGINEER

THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION
 THE ACCURACY OR COMPLETENESS OF ANY INFORMATION CONTAINED
 HEREIN IS THE RESPONSIBILITY OF THE USER.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	FUNCTIONAL SUPERVISOR	DESIGNED BY	REUSED BY
TRAFFIC	RAJESH OBEROI	CHECKED BY	RAJESH OBEROI
		DATE REVISED	

SIGN QUANTITIES

SQ-1

PROJECT NUMBER & PHASE

UNIT 0720

RELATIVE BORDER SCALE
1" = 15' IN INCHES

0 1 2 3

USER NAME => B131732
 DGN FILE => 04130000520001.dgn

BORDER LAST REVISED: 7/27/2010

ROADWAY QUANTITIES

SHEET No.	STATION	CLASS 2 AGGREGATE BASE	CY
L-1	SIDEWALK, CURB AND GUTTER	25	25
TOTAL			25

EARTHWORK SUMMARY

SHEET No.	ROADWAY EXCAVATION	CY
"L1"		45
TOTAL		45

MINOR CONCRETE (CURB, SIDEWALK AND CURB RAMP)

STATION	SIDE	TYPE	CY
"CURB1" 10+00 TO 286+25	Lt	SIDEWALK	.25
"CURB1" 10+00 TO 286+25	Lt	CURB AND GUTTER TYPE A(2-6)	.37
"CURB1" 10+00 TO 286+25	Lt	CURB RAMP A MODIFIED	1
"FL3" 285+01.15 TO 286+34.15	Rt	SIDEWALK	2.5
"FL3" 285+01.15 TO 286+34.15	Rt	CURB AND GUTTER TYPE A(2-6)	4
"FL3" 285+01.15 TO 286+34.15	Rt	CURB RAMP G MODIFIED & RETAINING CURB	1.16
"FR3" 285+96.00 TO 286+72.42	Lt	SIDEWALK	8
"FR3" 285+96.00 TO 286+72.42	Lt	CURB AND GUTTER TYPE A(2-6)	2.5
"FR3" 285+96.00 TO 286+72.42	Lt	CURB RAMP B MODIFIED	1
"FR3" 286+57.52 TO 286+72.57	Rt	SIDEWALK	2
"FR3" 286+57.52 TO 286+72.57	Rt	CURB AND GUTTER TYPE A(2-6)	0.5
"FR3" 286+57.52 TO 286+72.57	Rt	CURB RAMP C MODIFIED	2.5
TOTAL			22.16

REMOVE CONCRETE (CURB, GUTTER AND SIDEWALK)

STATION	SIDE	CY
"FR3" 284+93 TO 285+05	Lt	2.5
"FL3" 285+06 TO 285+12	Rt	1.93
"CURB1" 10+00 TO 286+25	Lt	1.31
"FR3" 286+57.52 TO 286+72.57	Rt	1.93
TOTAL		7.67

TREATED WOOD WASTE

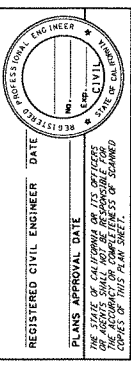
SHEET No.	STATION	DIRECTION	WOOD WEIGHT LB
L-1	FROM REMOVAL STBB	Lt	6452
L-1	FROM REMOVAL STBB	Rt	6452
TOTAL			12904

PRE/POST CONSTRUCTION SURVEYS

SHEET No.	STATION	SIDE	CURB RAMP TYPE	NUMBER OF CONSTRUCTION SURVEYS EACH
L-1	"CURB1" 00+06.69 TO 00+19.27	Lt	CURB RAMP A MODIFIED	1
L-1	"FR3" 286+57.43 TO 286+72.42	Lt	CURB RAMP B MODIFIED	1
TOTAL				2

PROJECT MILE TOTAL PROJECT NO. SHEETS XX
 04 SW 001 44.1

REGISTERED CIVIL ENGINEER DATE
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICERS
 THE AUTHORITY FOR THE COMPLETION OF THESE
 COPIES OF THIS PLAN SHEET.



REMOVE SINGLE METAL BEAM BARRIER (WOOD POST)

SHEET No.	STATION	SIDE	LF
L-1	"C1" 284+20 TO 288+50	Lt	430
L-1	"C1" 284+20 TO 288+50	Rt	430
TOTAL			860

SINGLE THRIE BEAM BARRIER (WOOD POST)

SHEET No.	STATION	SIDE	LF
L-1	"C1" 284+20 TO 288+50	Lt	430
L-1	"C1" 284+20 TO 288+50	Rt	430
TOTAL			860

TEMPORARY FENCE (TYPE ESA)

ITEM	UNIT	STATION	QUANTITY
TEMPORARY FENCE (TYPE ESA)	LF	AS SHOWN ON L-1	715

CHAIN LINK FENCE (TYPE CL-6)

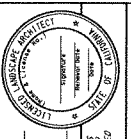
SHEET No.	STATION	SIDE	LF
L-1	"FR3" 285+75 TO 286+25	Lt	50
L-1	"FL3" 284+95 TO 286+85	Rt	190
TOTAL			240

REMOVE FENCE

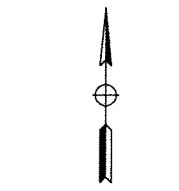
SHEET No.	STATION	SIDE	LF
L-1	"FR3" 285+75 TO 286+25	Lt	50
L-1	"FL3" 284+95 TO 286+85	Rt	190
TOTAL			240

SUMMARY OF QUANTITIES
Q-1

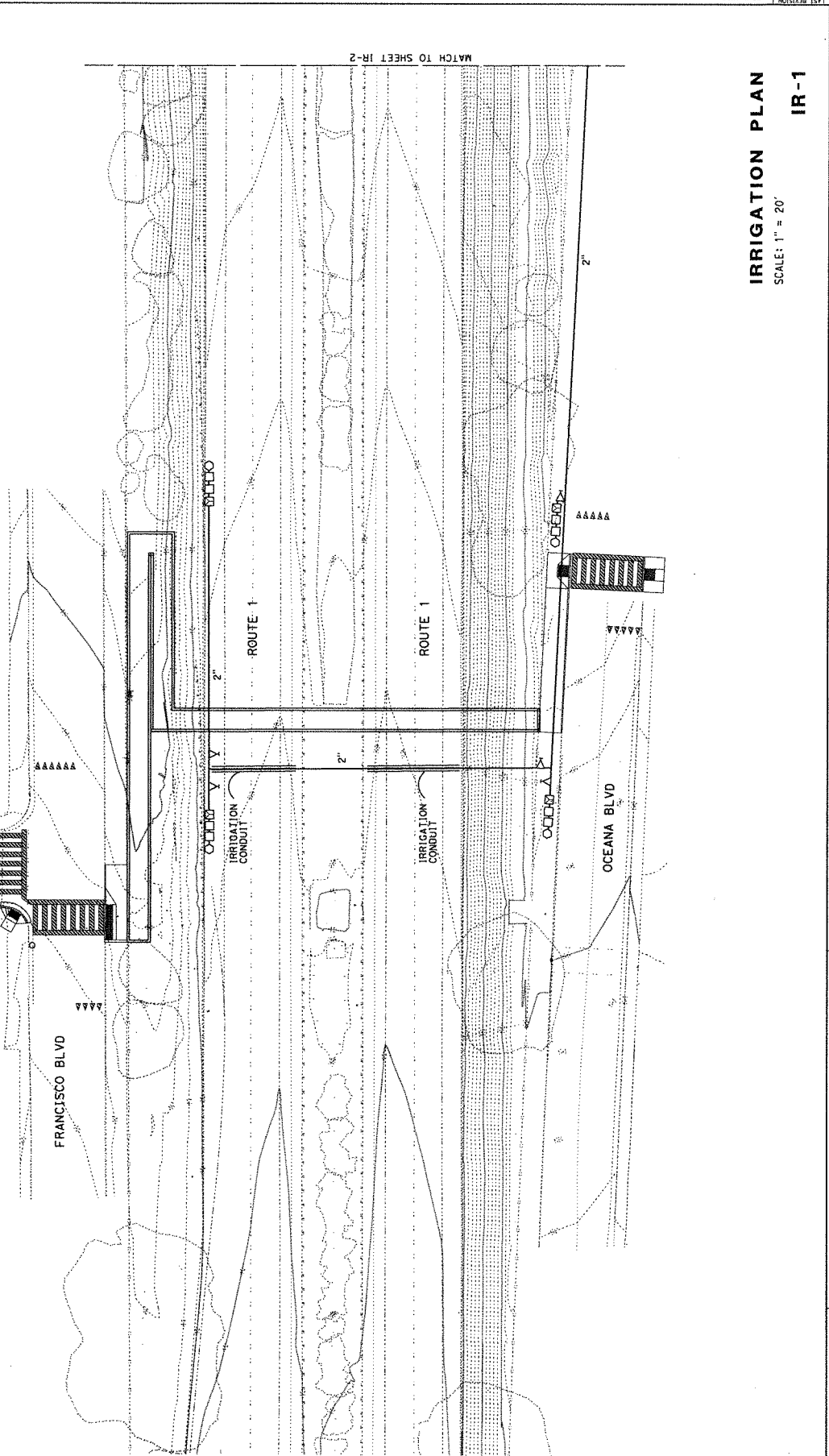
Dist	County	Route	Sheet Miles	Sheet Project	Sheet No.	Sheet Total
04	SM	1	R441.0, 44.20	A	A	A



LICENSED LANDSCAPE ARCHITECT
 PLANS APPROVAL DATE
 THE STATE OF CALIFORNIA OR ITS OFFICES
 DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE
 COPIES OF THIS PLAN SHEET.



NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



IRRIGATION PLAN
 IR-1
 SCALE: 1" = 20'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	SENIOR LANDSCAPE ARCHITECT	DESIGNED BY	CONNIE YIP	REVISD BY	A
Ed. Lyons LANDSCAPE ARCHITECTURE	KIMBERLY WHITE	CHECKED BY	A	DATE REVISED	A
BORDER LAST REVISED 7/2/2010	ISSUANCE 09-15-2009	009 FILE # 08280111-001	RELATIVE BORDER SCALE IS IN INCHES	UNIT 0789	PROJECT NUMBER & PHASE 041.3000052

DIST	COUNTY	ROUTE	PROJECT TOTAL SHEETS	SHEET NUMBER
04	SM	1	R44-0, 44-20	A

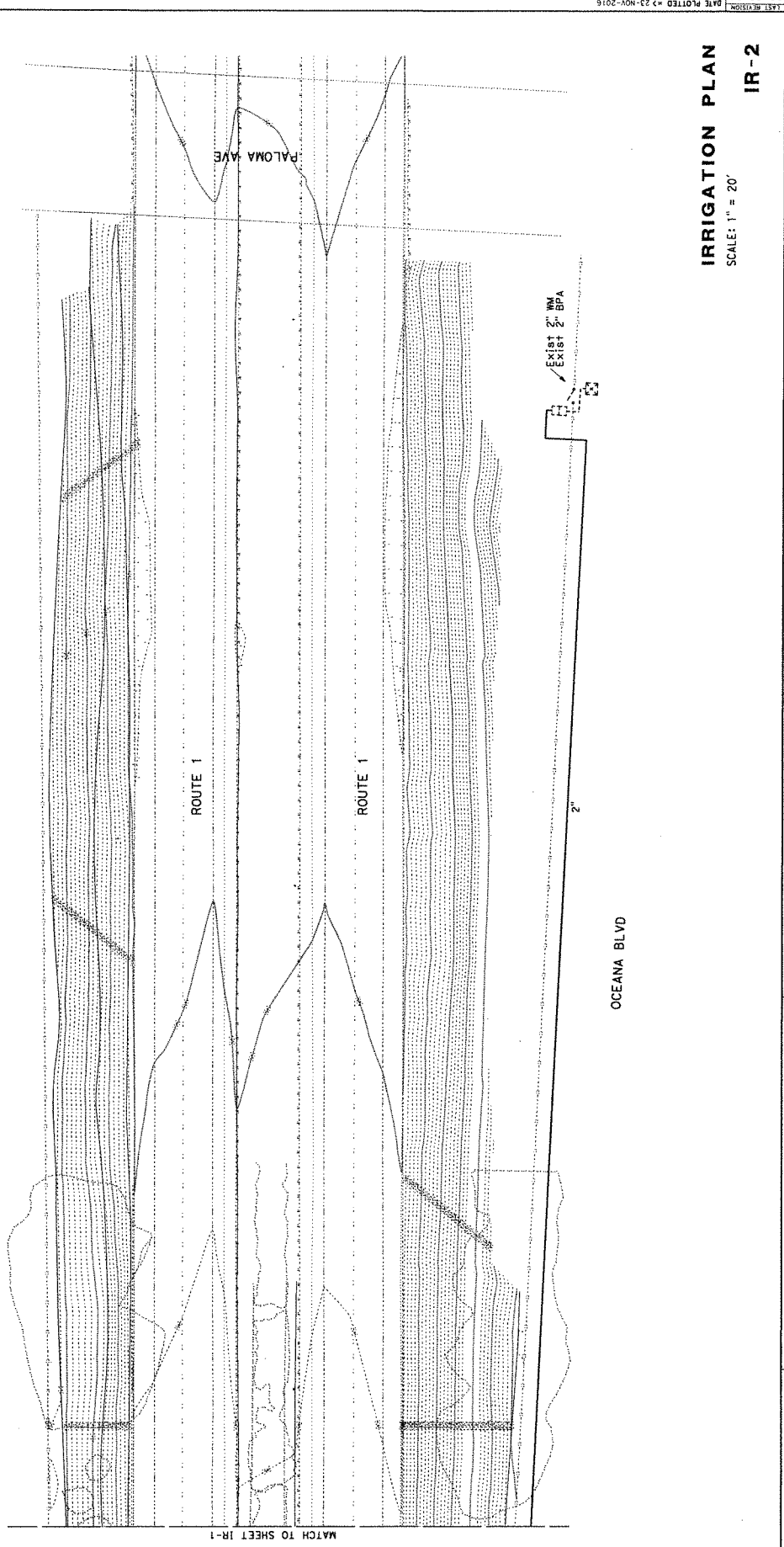
LICENSED LANDSCAPE ARCHITECT

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS
 DO NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF ANY
 COPIES OF THIS PLAN SHEET.



NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.



IRRIGATION PLAN
IR-2

SCALE: 1" = 20'

DATE PLOTTED: 23-NOV-2016 15:02
 PROJECT NUMBER & PHASE: UNIT 0789
 RELATIVE BORDER SCALE: 1/8" IN FIGURES
 0 1 2 3
 BORDER LAST REVISED 7/2/2010
 USER NAME: J. J. JONES
 DWM FILE: J. JONES.P2.199
 0413000052

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	SENIOR LANDSCAPE ARCHITECT	CALCULATED BY	CONNIE YIP	REVISOR	A
Kimberly White	CHECKED BY	DATE REVISED			A

NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

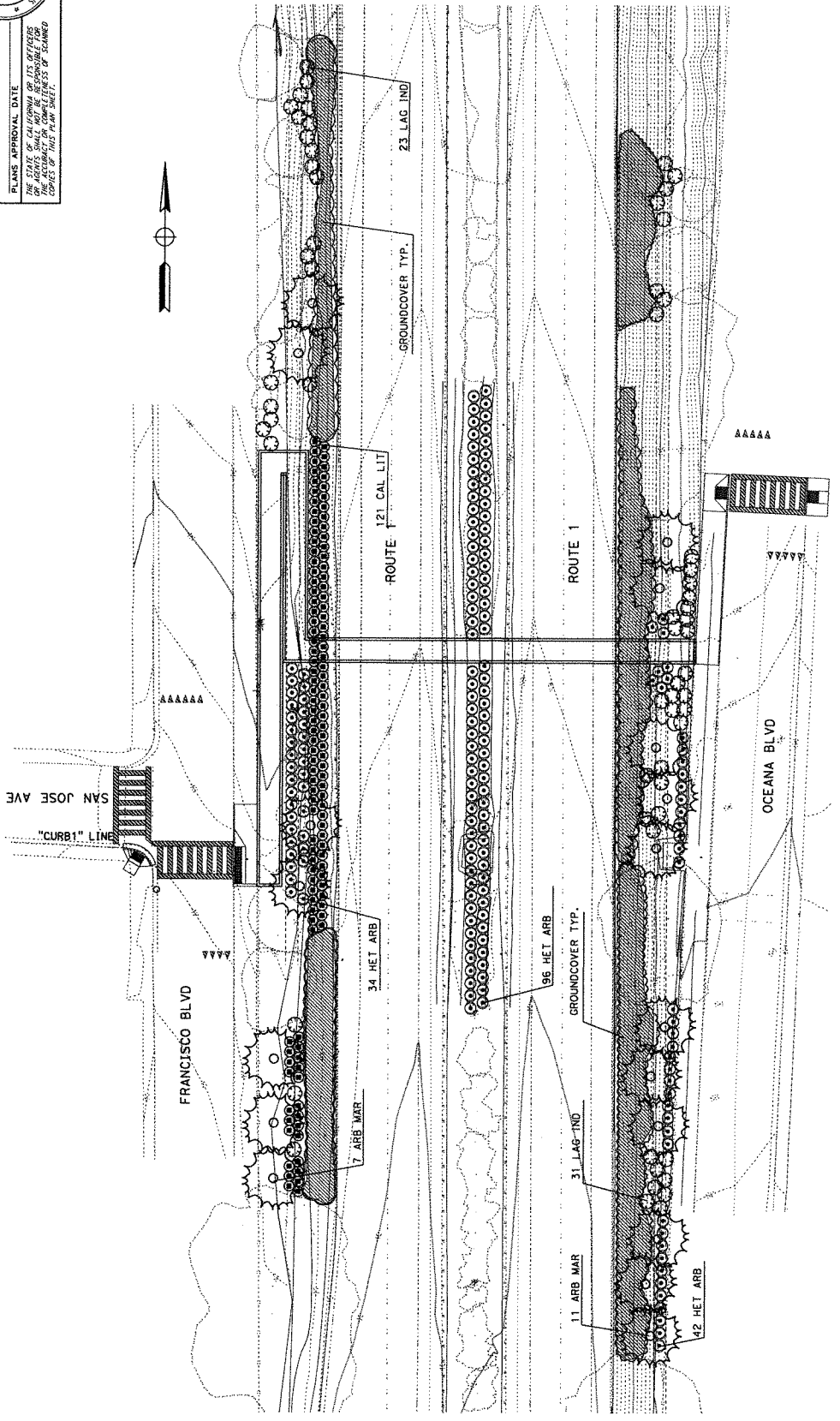
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	SENIOR LANDSCAPE ARCHITECT	DESIGNED BY	CONNIE YIP	REVISOR	A
Ed. Gibbons LANDSCAPE ARCHITECTURE	KIMBERLY WHITE	CHECKED BY	A	DATE REVISED	A
BORDER LAST REVISED 7/2/2010					

DIST#	COUNTY	ROUTE	FIRST SHEET	TOTAL SHEETS	SHEET NO.	SHEET TITLE
04	SM	1	R441-0	44.20	A	A

LICENSED LANDSCAPE ARCHITECT

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS
THE ASSISTANT COUNTY ENGINEERS OF PLANNING
CONSENT TO THIS PLAN SHEET.



PLANTING PLAN

SCALE: 1" = 20'

PP-1

DATE: 04/04 COUNTY: SM ROUTE: 1 TOTAL PROJECT: 4.1 SHEET NO.: A A A

LICENSED LANDSCAPE ARCHITECT

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 DIVISION OF LANDSCAPE ARCHITECTURE
 DIVISION OF PLANNING AND DESIGN

LEGEND:

EROSION CONTROL (TYPE 1)

EROSION CONTROL (TYPE 2)

FR FIBER ROLL

SEED MIX

SEED	BOTANICAL NAME (COMMON NAME)	PERCENT GERMINATION (MINIMUM)	POUNDS PURE LIVE SEED PER ACRE (SLOPE MEASUREMENT)
MIX 1	ACHILLEA MILLEFOLIUM (WHITE YARROW)	49	1
	DESCHAMPSIA CESPIOSA (TUFTED HAIRGRASS)	47	7
	ESCHSCHOLZIA CALIFORNICA (CALIFORNIA POPPY)	47	3
	GAZANIA SPLENDENS (MIXED COLOR GAZANIA)	48	14
	HETEROMELES ARBUTIFOLIA (TOYON CHRISTMAS BERRY)	30	4
	HOBBEUM BRACHYANTHERUM (MEADOW BARLEY)	40	9
	LAYIA PLATYGLOSSA (COMMON TIDY TIPS)	40	3
	NASELLA PULCHRA (PURPLE NEEDLEGRASS)	40	10
	TRIFOLIUM WILDENOVII (TOMCAT CLOVER)	40	4
	VULPIA MICROSTACHYS (THREE WEEKS FESCUE)	45	5

EROSION CONTROL TYPE 2

SEQUENCE	ITEM	DESCRIPTION	MATERIAL TYPE	APPLICATION RATE
STEP 1	ROLLED EROSION CONTROL PRODUCT	NETTING	TYPE A	-
STEP 2	FIBER ROLLS	FIBER ROLL	8" TO 10" DIA. AND 1.1 LB/FT	-
STEP 3	COMPOST	COMPOST	FINE	68 CY/ACRE
STEP 4	HYDROSEED	SEED	SEED MIX 1	60 LB/ACRE
		FIBER	WOOD	500 LB/ACRE
STEP 5	HYDROMULCH	FIBER	WOOD	1,500 LB/ACRE
		TACKLIFIER	GUAR	125 LB/ACRE

EROSION CONTROL TYPE 1

SEQUENCE	ITEM	DESCRIPTION	MATERIAL TYPE	APPLICATION RATE
STEP 1	FIBER ROLLS	FIBER ROLL	8" TO 10" DIA. AND 1.1 LB/FT	-
STEP 2	COMPOST	COMPOST	FINE	68 CY/ACRE
STEP 3	HYDROSEED	SEED	SEED MIX 1	60 LB/ACRE
		FIBER	WOOD	500 LB/ACRE
STEP 4	HYDROMULCH	FIBER	WOOD	1,500 LB/ACRE
		TACKLIFIER	GUAR	125 LB/ACRE

EROSION CONTROL LEGEND

ECL-1

PROJECT NUMBER & PHASE

UNIT 0792

APPROVED FOR EROSION CONTROL WORK ONLY

RELATIVE BORDER SCALE IS IN INCHES

DATE PLOTTED: 7-2-2010

DATE PLOTTED: 7-2-2010

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

WATER QUALITY

SENIOR LANDSCAPE ARCHITECT

SALLY BANG

CHECKED BY: SALLY BANG

DESIGNED BY: ANGELA KMAN

REVISOR: ANGELA KMAN

DATE REVISED: _____

Dist#	COUNTY	ROUTE	TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	SM	1	4.1	A	A

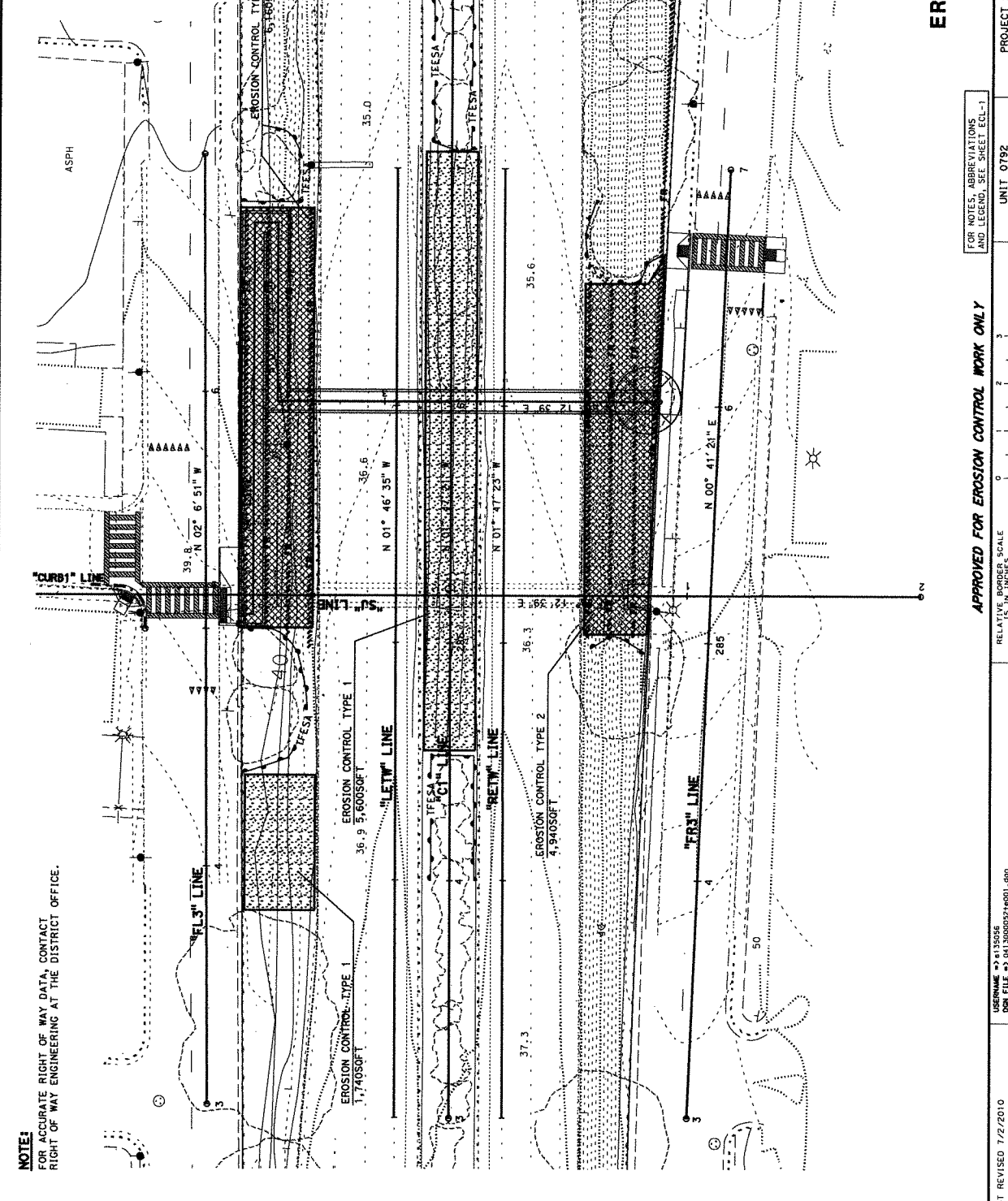
LICENSED LANDSCAPE ARCHITECT

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA ON ITS BEHALF
 CERTIFIES THAT THE ARCHITECT HAS
 REVIEWED THIS PLAN SHEET



DATE PLOTTED => 12-NOV-2016
 TIME PLOTTED => 15:31



EROSION CONTROL PLAN
 SCALE: 1" = 20'

FOR NOTES, ABBREVIATIONS
 AND LEGEND, SEE SHEET EC-1

APPROVED FOR EROSION CONTROL WORK ONLY

USERNAME => s135056
 DOW FILE => 0413000021w01.dgn

BORDER LAST REVISED 7/2/2010

PROJECT NUMBER & PHASE
 UNIT: 0792

04130000521

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	SENIOR LANDSCAPE ARCHITECT	DESIGNED BY	ANGELA KWAN	REVISOR	DATE REVISED
WATER QUALITY	SALLY BANG	CHECKED BY	SALLY BANG		

SHEET NO. _____
 SHEETS TOTAL _____
 COUNTY _____ ROUTE _____ TOTAL PROJECT _____
 LICENSED LANDSCAPE ARCHITECT
 PLANS APPROVAL DATE _____
 THE STATE OF CALIFORNIA ON ITS OFFICERS
 THE ARCHITECTURE BOARD OF REGISTERED
 LANDSCAPE ARCHITECTS
 HAS REVIEWED AND APPROVED THESE
 PLANS ON THIS DATE.

EROSION CONTROL QUANTITIES

SHEET No.	DESCRIPTION	FIBER ROLLS	COMPOST	HYDROMULCH	ROLLED EROSION CONTROL PRODUCT (NETTING)	HYDROSEED
EC-1	EROSION CONTROL TYPE 1	LF	SQFT	SQFT	SQFT	SQFT
	EROSION CONTROL TYPE 2	-	7,340	7,340	-	7,340
	TOTAL	1,472	11,100	11,100	11,100	11,100
		1,472	18,440	18,440	11,100	18,440

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	SENIOR LANDSCAPE ARCHITECT	DESIGNED BY	ANGELA KMAN	REVISD BY	
WATER QUALITY	SALLY BANG	CHECKED BY	SALLY BANG	DATE REVISED	

DIRT COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET TOTAL NO. SHEETS
04	SM 1	44.1	18

REGISTERED ELECTRICAL ENGINEER DATE _____
 PLANS APPROVAL DATE _____
 PROFESSIONAL ENGINEER
 THE STATE OF CALIFORNIA OR ITS OFFICERS
 THE ACCURACY OR COMPLETENESS OF ANY
 COPIES OF THIS PLAN SHEET.

PROJECT NOTES:

- 1 MAKE 12" DEEP TRENCH FROM THE RELOCATED POWER CABINET TO THE ENTRANCE OF THE (N) P.O.C. INSTALL 2" C, 2#6, 1#6 GROUND IN THE TRENCH.
- 2 CONNECT THE CONDUIT IN THE TRENCH TO THE (E) EMBEDDED CONDUIT IN (N) P.O.C.
- 3 INSTALL 2#6, 1#6 GROUND IN THE EMBEDDED CONDUIT AND CONNECT TO P.O.C LIGHT.
- 4 INSTALL LIGHTS ON THE BRIDGE AS PER SHEET E-3.
- 5 CONNECT THE 2#6, 1#6 GROUND TO THE (E) 30 AMP BREAKER.
- 6 INSTALL SOLAR POWER OPERATED RRF8 IN THE LOCATIONS SHOWN, AS PER SHEET E-5.
- 7 THIS LIGHTING POLE WITH LUMINAIRE AND IT'S PULL BOX TO REMAIN AFTER DEMOLITION OF EXISTING P.O.C.
- 8 **RC** (E) P.O.C LIGHT POLE WITH LUMINAIRE.
- 9 **RC** ALL CONDUITS AND CONDUCTORS FEEDING THE (E) P.O.C LIGHT UP TO (E) 30A BREAKER LOCATED IN THE (E) POWER CABINET SHOWN IN SHEET E-2.
- 10 CONNECT TO 2" C, 2#6, 1#6 GROUND TO THE REMAINING P.O.C LIGHT.
- 11 CALTRANS UTILITY ENGINEERS TO MAKE ARRANGEMENTS WITH PO&E FOR RELOCATION OF THESE PULL BOXES.
- 12 (E) POWER CABINET #44041 TO BE RELOCATED TO 10-15 FEET TOWARDS THE LEFT AS SHOWN IN SHEET E-3.

NOTES:

1. ALL CONDUIT ROUTING IS DIAGRAMATIC. PULL BOX LOCATION MUST BE LOCATED AS SHOWN. PROVIDE ADDITIONAL PULL BOXES AS REQUIRED. EXACT LOCATION OF EQUIPMENT AND DEVICES MAYBE ADJUSTED DEPENDING ON FIELD CONDITION. VERIFY ALL FIELD DIMENSIONS.
2. MINIMUM SIZE OD CONDUITS MUST BE 3/4". ALL CONDUITS MUST BE PVC COATED RIGID GALVANIZED STEEL (TYPE 2).
3. CONDUIT FITTING FOR 3" CONDUITS AND LARGER MUST HAVE A 24" MINIMUM BENDING RADIUS.
4. CONDUITS INCLUDING SPARES, MUST BE PROVIDED WITH FULL TAPE PRIOR TO CABLE INSTALLATION.
5. EQUIPMENT AND DEVICES MUST BE PROVIDED WITH NAMEPLATE TAGS PER DRAWINGS.
6. FEEDERS AND BRANCH CIRCUITS MUST BE PROVIDED WITH GROUND WIRE.
7. LABEL ALL CONDUCTORS AND CABLES PER WIRING DIAGRAMS.

ELECTRICAL INDEX

- E-1 ELECTRICAL INDEX, SYMBOLS, NOTES, ABBREVIATIONS AND PROJECT NOTES
- E-2 REMOVING EXISTING ELECTRICAL SYSTEM
- E-3 ELECTRICAL PLAN
- E-4 ELECTRICAL DETAILS
- E-5 ELECTRICAL DETAILS
- E-6 ELECTRICAL QUANTITY

SYMBOLS:

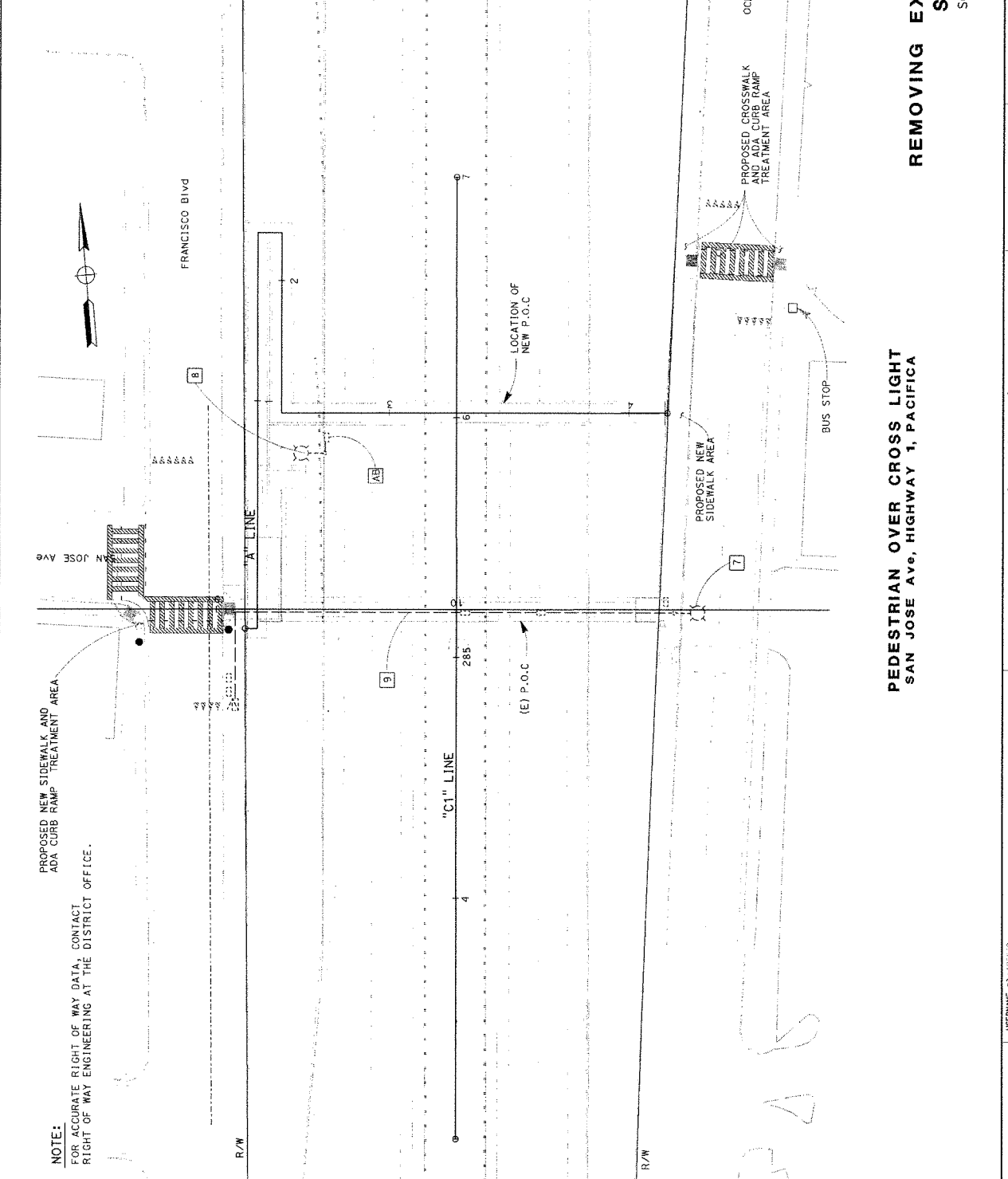
EXISTING	PROPOSED	
		PULL BOX
		SERVICE EQUIPMENT CLOSURE WITH DOOR
		FLUORESCENT LIGHT FIXTURE
		ELECTROLIER
		SOLAR POWER OPERATED RECTANGULAR RAPID FLASHING BEACON
		CONDUIT IN TRENCH OR EXISTING EMBEDDED CONDUIT

ABBREVIATIONS:

A	AMPERE
(E)	EXISTING
(N)	NEW
P.O.C	PEDESTRIAN OVER CROSS
RRFB	SOLAR POWER OPERATED RECTANGULAR RAPID FLASHING BEACON

ELECTRICAL INDEX, SYMBOLS, NOTES, ABBREVIATIONS AND PROJECT NOTES

DIST	COUNTY	ROUTE	POST MILES TO PROJECT	SHEET NO. OF SHEETS
04	SM	1	4.1	17 A
REGISTERED ELECTRICAL ENGINEER			DATE	
PLANS APPROVAL DATE				



NOTE:
FOR ACCURATE RIGHT OF WAY DATA, CONTACT
RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

PROPOSED NEW SIDEWALK AND
ADA CURB RAMP TREATMENT AREA

PROPOSED NEW SIDEWALK AREA

PROPOSED CROSSWALK AND ADA TREATMENT AREA

PROPOSED CROSSWALK AND ADA TREATMENT AREA

PROPOSED CROSSWALK AND ADA TREATMENT AREA

PROPOSED CROSSWALK AND ADA TREATMENT AREA

PROPOSED CROSSWALK AND ADA TREATMENT AREA

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PROPOSED CROSSWALK AND ADA TREATMENT AREA

PROPOSED CROSSWALK AND ADA TREATMENT AREA

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	PROJECT: LOCAL SUPERVISOR	DATE: 04/21/16	DESIGNED BY: JOHN PRESENTATION	REVIEWED BY: A
ELECTRICAL	CHECKED BY: PARVIZ BOOZARPOUR	DATE: 04/21/16	DATE RECEIVED: GIZACHEW MERID	DATE RECEIVED: A

OFFICE: LAST REVISED: 7/2/2010
 USER: NAME: 031516843
 DGN FILE: 01_3000043.dwg
 UNIT: 0980
 PROJECT NUMBER & PHASE: 06600000801
 SCALE: 1" = 20'

REMOVING EXISTING ELECTRICAL SYSTEMS

PEDESTRIAN OVER CROSS LIGHT SAN JOSE Ave, HIGHWAY 1, PACIFICA

DIST	COUNTY	ROUTE	TOTAL SHEETS	SHEET NO.
04	SM	1	41	17
REGISTERED ELECTRICAL ENGINEER		DATE	PLANS APPROVAL DATE	
[Signature]		4.1	[Signature]	



NOTE:
 FOR ACCURATE RIGHT OF WAY DATA, CONTACT
 RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

PROPOSED NEW SIDEWALK AND
 ADA CURB RAMP TREATMENT AREA

PROPOSED NEW SIDEWALK AND
 ADA CURB RAMP TREATMENT AREA

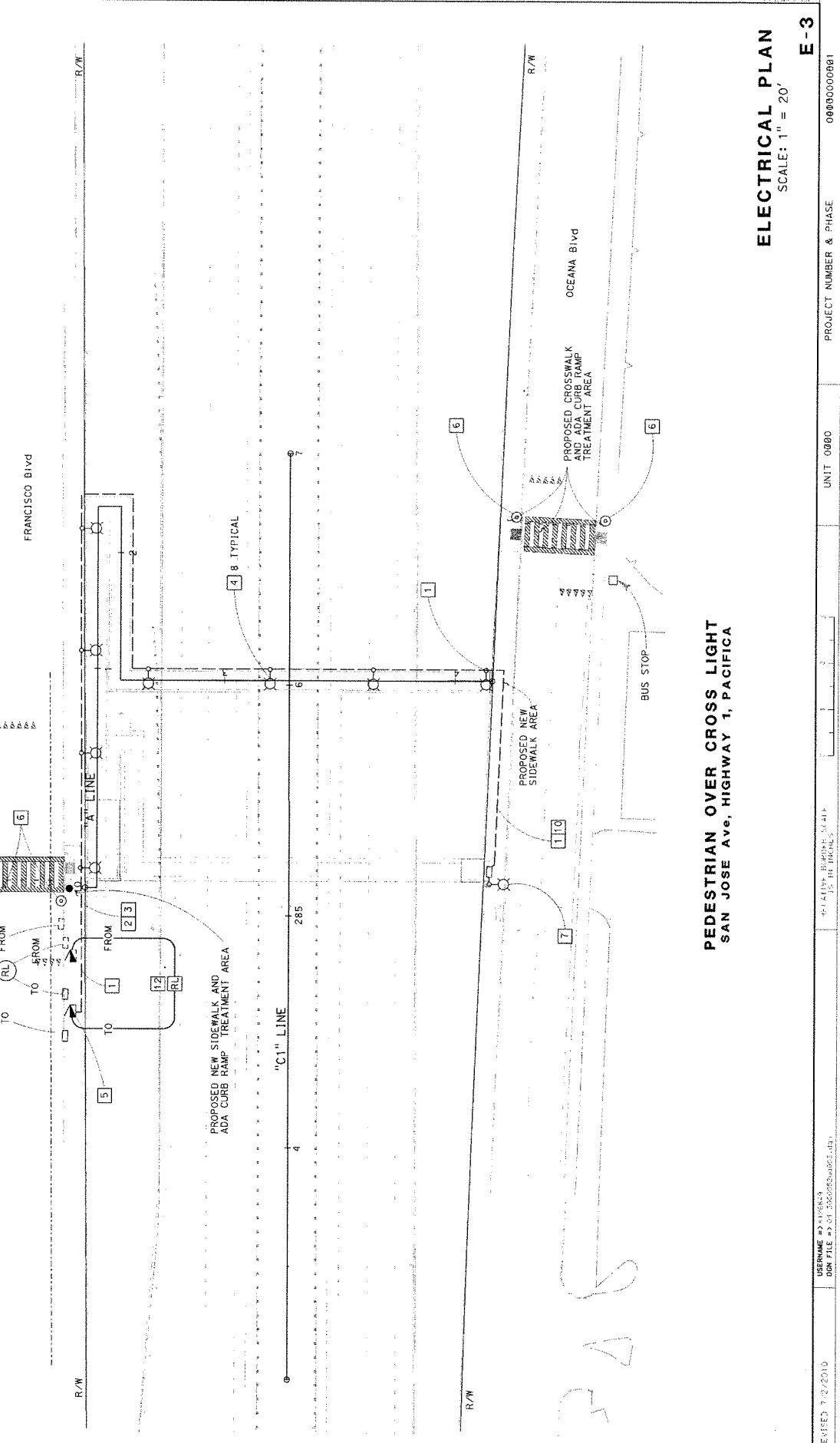
PROPOSED NEW SIDEWALK AND
 ADA CURB RAMP TREATMENT AREA

PROPOSED NEW SIDEWALK AND
 ADA CURB RAMP TREATMENT AREA

PROPOSED NEW SIDEWALK AND
 ADA CURB RAMP TREATMENT AREA

PROPOSED NEW SIDEWALK AND
 ADA CURB RAMP TREATMENT AREA

PROPOSED NEW SIDEWALK AND
 ADA CURB RAMP TREATMENT AREA



ELECTRICAL PLAN
 SCALE: 1" = 20'

E-3

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	ELECTRICAL	PARVIZ BOOZARPOUR	PARVIZ BOOZARPOUR	PARVIZ BOOZARPOUR	PARVIZ BOOZARPOUR
PROJECT LEADER	DESIGNED BY	CHECKED BY	CHECKED BY	DATE REVISION	REVISION BY
JOHN PRESENTATION	JOHN PRESENTATION	CIZACHEW MERID	CIZACHEW MERID		A
					A

Dist	County	Route	Post Miles Total Project	Sheet Total No. Sheets
04	SM	1	44.1	XX

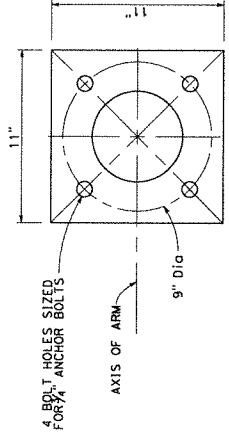
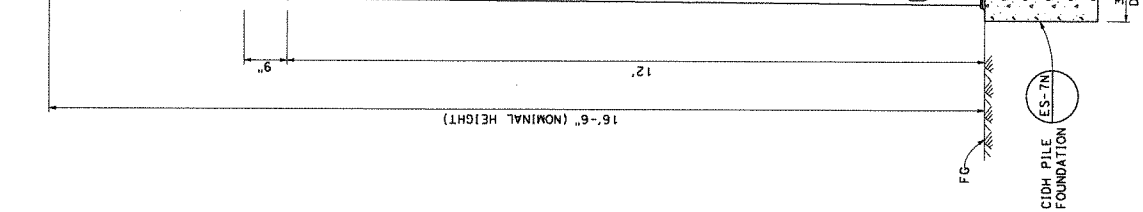
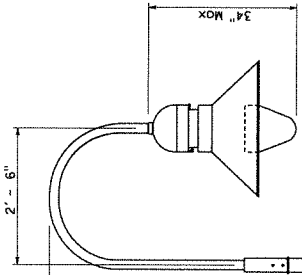
REGISTERED ELECTRICAL ENGINEER DATE

PLANS APPROVAL DATE

PROFESSIONAL ENGINEER

THE STATE OF CALIFORNIA OR ITS OFFICERS
THE ACCURACY OR COMPLETENESS OF STAMPED
COPIES OF THIS PLAN SHEET.

NOTE:
1. BASE PLATE SHALL BE 1" THICK.



BASE PLATE

LIGHTING POLE AND LUMINAIRE
ELEVATION

ELECTRICAL DETAILS
NO SCALE
E-4

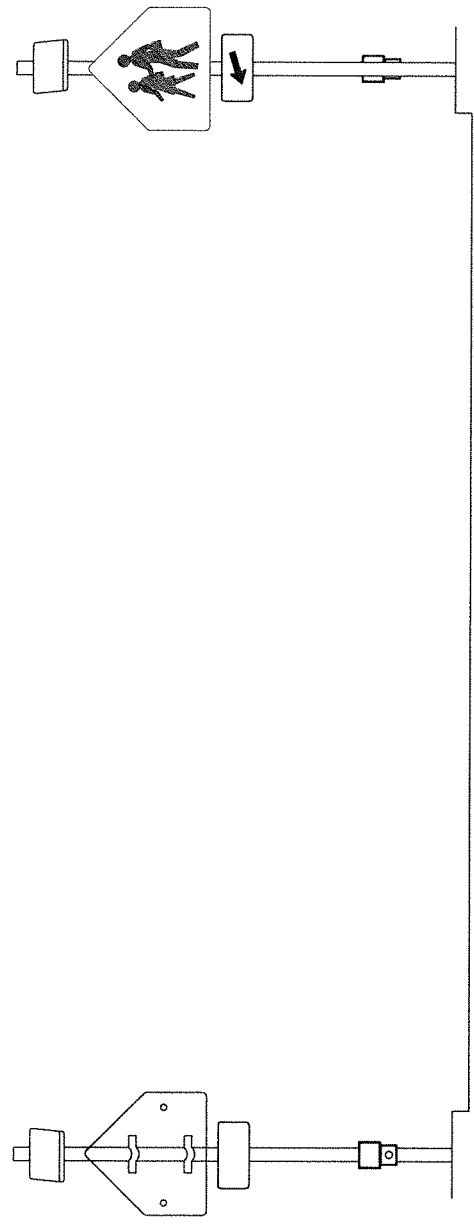
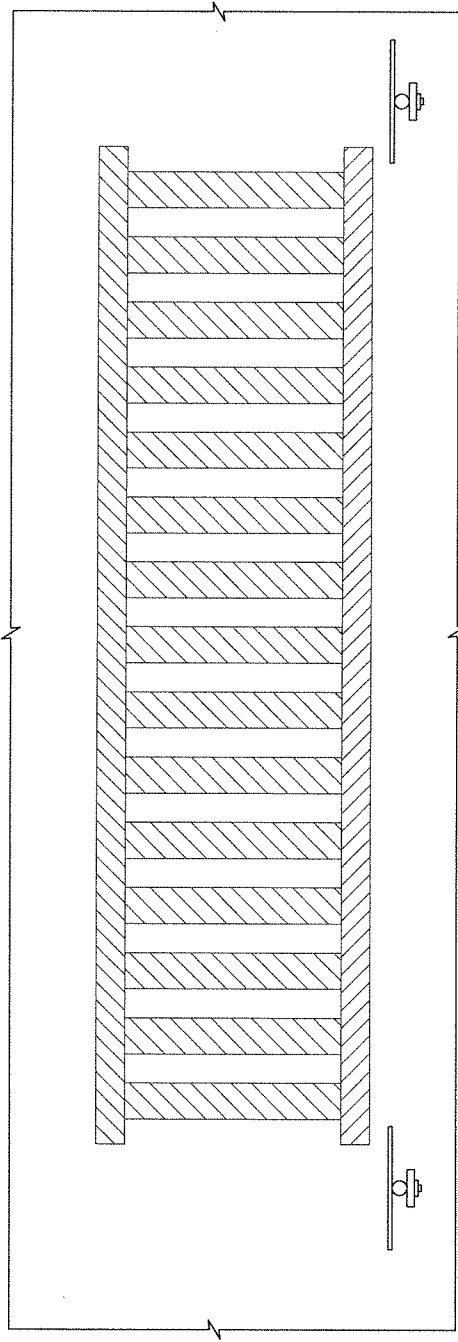
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	FUNCTIONAL SUPERVISOR	PARVIZ BOOZARPOUR	CHECKED BY	GIZACHEW MERID	DATE REVISED	REVISOR	JOHN PRESENTATION
ELECTRICAL		DESIGNED BY					

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION		FUNCTIONAL SUPERVISOR		PARVIZ BOOZARPOUR		CHECKED BY		GIZACHEW MERID		DATE REVISED		REVISED BY	
ELECTRICAL		DESIGNED BY		JOHN PRESENTATION		CALCULATED		REVISOR		DATE REVISED		REVISED BY	

BORDER LAST REVISED: 7/2/2010

USERNAME: *2131732
DOT FILE: *0413000052.dwg

RECTANGULAR RAPID FLASHING BEACON



RELATIVE BORDER SCALE IS IN INCHES

0 1 2 3

UNIT: 0720

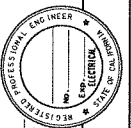
PROJECT NUMBER & PHASE

04130000521

E-5

ELECTRICAL DETAILS
NO SCALE

DATE PLOTTED: 06-DEC-2016	11-10-16	REVISION:
POST MILES TOTAL PROJECT: 44.1	SHEET NO.: XX	TOTAL SHEETS: XX
COUNTY: SM	ROUTE: 1	DATE: 4.1
REGISTERED ELECTRICAL ENGINEER		
PLANS APPROVAL DATE		
REGISTERED ELECTRICAL ENGINEER		
THE STATE OF CALIFORNIA OR ITS OFFICERS		
THE ACTUARY OR COMPLETENESS OF SCANNED		
COPIES OF THIS PLAN SHEET.		



ELECTRICAL QUANTITIES

E-6

ELECTRICAL QUANTITIES

SHEET NO.	12" DEEP TRENCHING	2" CONDUIT	#6 CONDUCTOR	LIGHT POLE WITH LUMINAIRE	FUSE UNIT	PULL BOX	RELOCATE PULL BOX	RELOCATE POWER CABINET	LIGHT POLE WITH LUMINAIRE
E-1	FT	LF	EA						
E-2	120	120	360	8	8	2	2	1	1
TOTAL	120	120	360	8	8	2	2	1	1

DIS+ COUNTY ROUTE POST MILES TOTAL PROJECT SHEET TOTAL SHEETS
 04 SM 1 44.1 XX

REGISTERED ELECTRICAL ENGINEER DATE

PLANS APPROVAL DATE

REGISTERED PROFESSIONAL ENGINEER

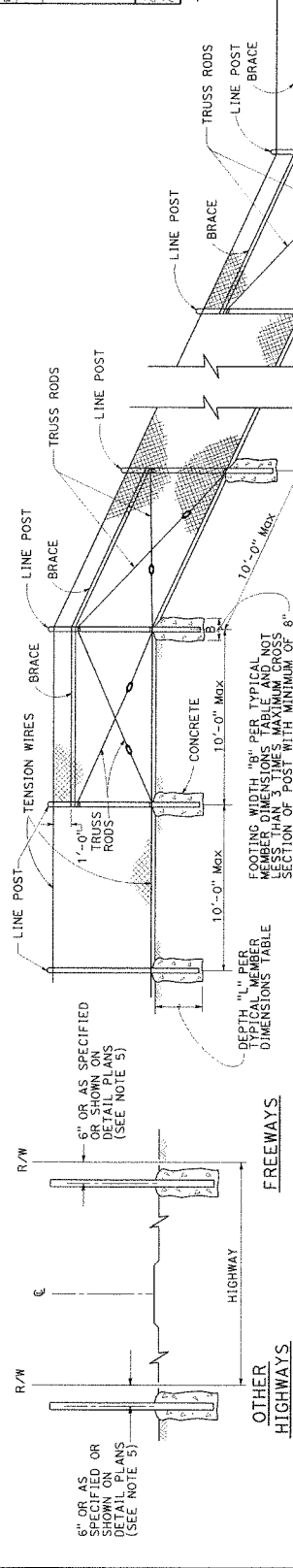
THE STATE OF CALIFORNIA OR ITS OFFICERS
 SHALL NOT BE HELD RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ANY
 COPIES OF THIS PLAN SHEET.

WEST COUNTY	SHEET NO.	TOTAL SHEETS	DATE
	123	150	07/15/2016

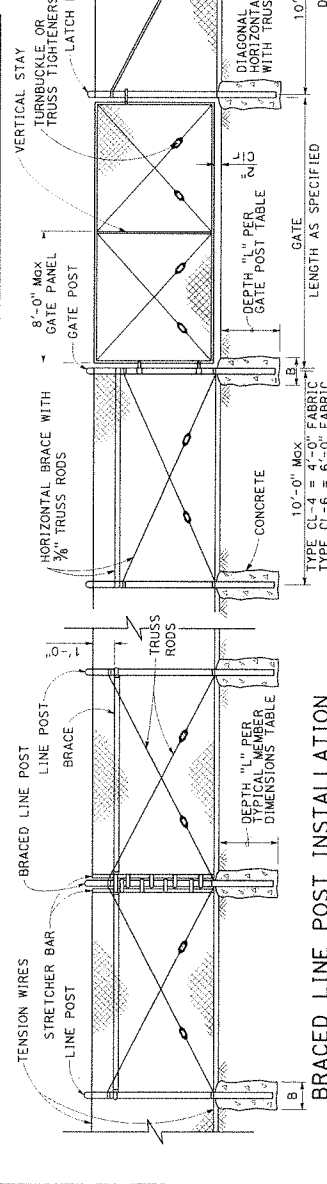
REVISED LEVEL ENGINEER

July 15, 2016

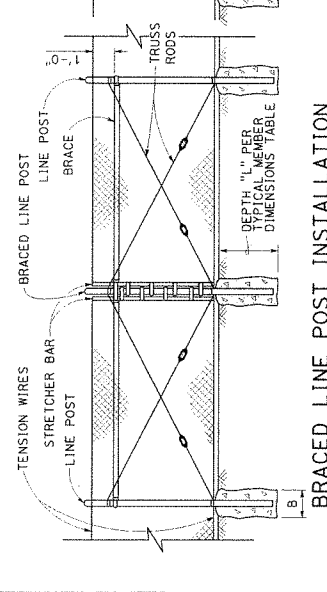
Stamp: **Salvador Singh**, No. C74993, Civil Engineer, State of California, License No. 11111, Exp. 12-31-17



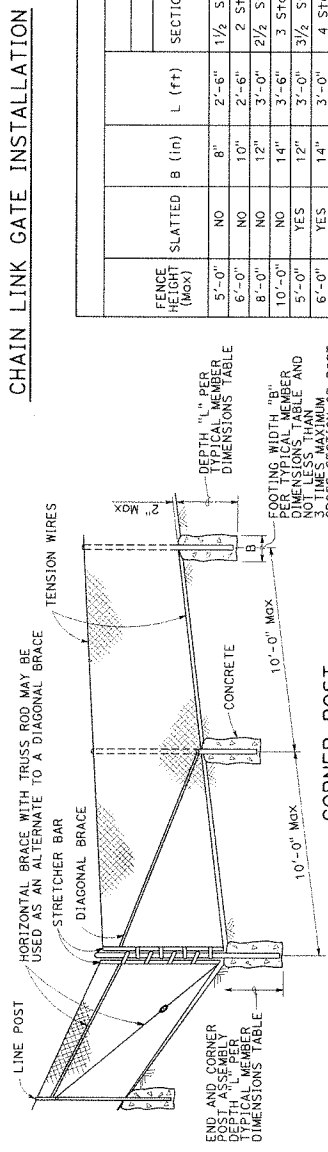
CHAIN LINK FENCE ON SHARP BREAK IN GRADE



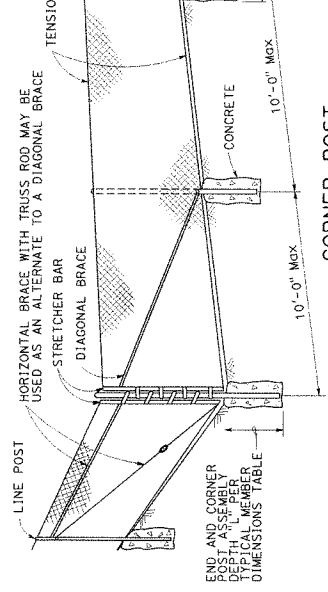
FENCE LOCATION



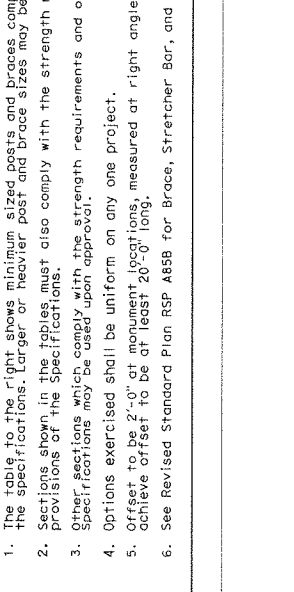
CHAIN LINK GATE INSTALLATION



BRACED LINE POST INSTALLATION



CORNER POST



NOTES:

1. The table to the right shows minimum sized posts and braces complying with the specifications. Larger or heavier post and brace sizes may be used upon approval.
2. Sections shown in the tables must also comply with the strength requirements and other provisions of the Specifications.
3. Other sections which comply with the strength requirements and other provisions of the Specifications may be used upon approval.
4. Options exercised shall be uniform on any one project.
5. Offset to be 2'-0" at monument locations, measured at right angles to R/W lines. Taper to achieve offset to be at least 20'-0" long.
6. See Revised Standard Plan RSP A658 for Brace, Stretcher Bar, and Truss Tightener Details.

FENCE HEIGHT (Max)	SLATTED	B (in)	L (ft)	ROUND PIPE		WEIGHT (lb/ft)
				SECTION	WEIGHT (lb/ft)	
5'-0"	NO	12"	2'-6"	3 Std	3.50"	7.58
6'-0"	NO	12"	2'-6"	3 Std	3.50"	7.58
8'-0"	NO	12"	3'-0"	3 Std	3.50"	7.58
10'-0"	NO	14"	3'-6"	3 Std	3.50"	7.58
5'-0"	YES	12"	3'-0"	3/2 Std	4.00"	9.12
6'-0"	YES	14"	3'-6"	4 Std	4.50"	10.80
8'-0"	YES	18"	3'-6"	5 Std	5.56"	14.60
10'-0"	YES	20"	4'-0"	6 Std	6.63"	19.00

Above post dimensions and weights are minimums. Larger sizes may be used upon approval. Maximum Gate Width is 24'-0".

FENCE HEIGHT (Max)	SLATTED	B (in)	L (ft)	LINE POSTS				BRACES					
				ROLL FORMED		ROUND PIPE		ROLL FORMED		ROUND PIPE			
				SECTION	WEIGHT (lb/ft)	SECTION	WEIGHT (lb/ft)	SECTION	WEIGHT (lb/ft)	SECTION	WEIGHT (lb/ft)		
				WEIGHT (lb/ft)	SECTION	WEIGHT (lb/ft)	SECTION	WEIGHT (lb/ft)	SECTION	WEIGHT (lb/ft)			
5'-0"	NO	8"	2'-6"	1/2 Std	1.90"	2.72	1.875" x 1.625"	1.85	2 Std	2.38"	3.66	1.625" x 1.250"	1.35
6'-0"	NO	10"	2'-6"	2 Std	2.38"	3.66	1.875" x 1.625"	2.40	2 Std	2.38"	3.66	1.625" x 1.250"	1.35
8'-0"	NO	12"	3'-0"	2/2 Std	2.88"	5.80	3.250" x 2.500"	4.50	2 Std	2.38"	3.66	1.625" x 1.250"	1.35
10'-0"	NO	14"	3'-6"	3 Std	3.50"	7.58	3.250" x 2.500"	4.50	2/2 Std	2.88"	5.80	1.625" x 1.250"	1.35
5'-0"	YES	12"	3'-0"	3/2 Std	4.00"	9.12	N/A	-	2 Std	2.38"	3.66	N/A	-
6'-0"	YES	14"	3'-6"	4 Std	4.50"	10.80	N/A	-	2 Std	2.38"	3.66	N/A	-
8'-0"	YES	18"	3'-6"	5 Std	5.56"	14.60	N/A	-	2 Std	2.38"	3.66	N/A	-
10'-0"	YES	20"	4'-0"	6 Std	6.63"	19.00	N/A	-	2/2 Std	2.88"	5.80	N/A	-

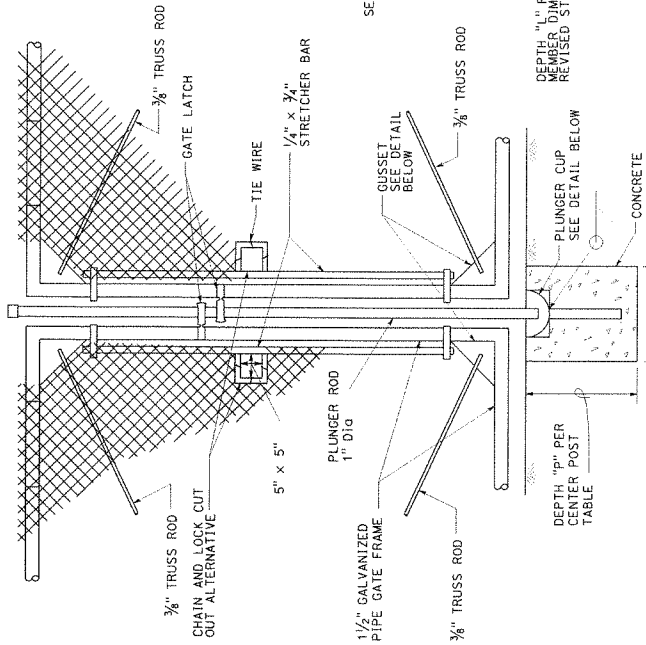
TYPICAL MEMBER DIMENSIONS (See Notes)

STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION
CHAIN LINK FENCE
NO SCALE

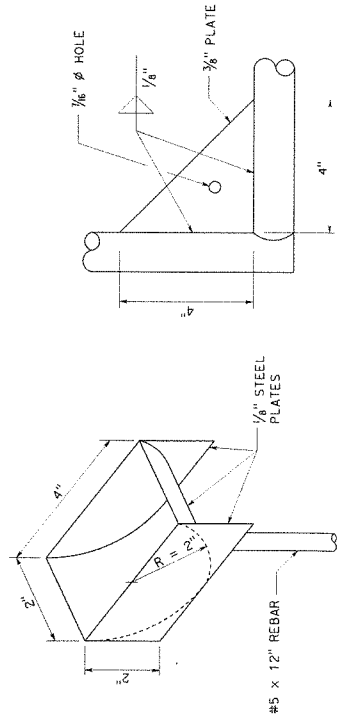
REGISTERED CIVIL ENGINEER
 JULY 15, 2016
 STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 1234567890
 1234567890
 1234567890
 1234567890

- NOTES:**
1. B is not less than 3 times maximum cross section of post with minimum of 8".
 2. See Revised Standard Plan RSP A85 for Chain Link Fencing dimensions.
 3. See Detail A on Standard Plan A866 for connection of headwall.
 4. See Detail D on Standard Plan A86B for connection of headwall.

CENTER POST		P
FENCE HEIGHT (Max)	SLATTED	
ALL HEIGHTS	NO	1'-6"
5'-0"	YES	3'-0"
6'-0"	YES	3'-0"
8'-0"	YES	3'-6"
10'-0"	YES	4'-0"



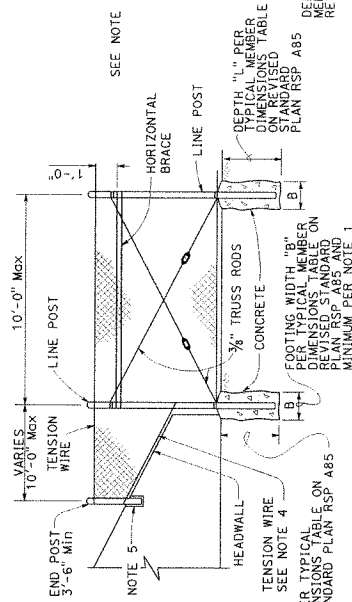
DOUBLE GATE REMOVABLE CENTER POST



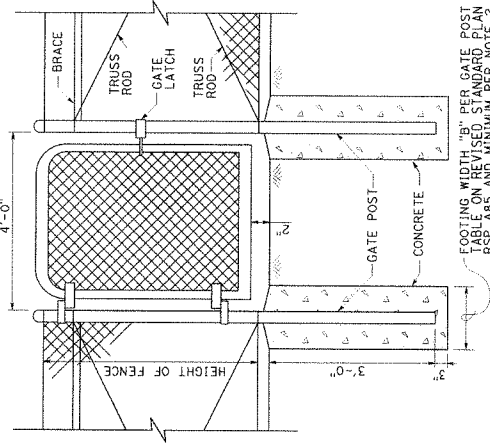
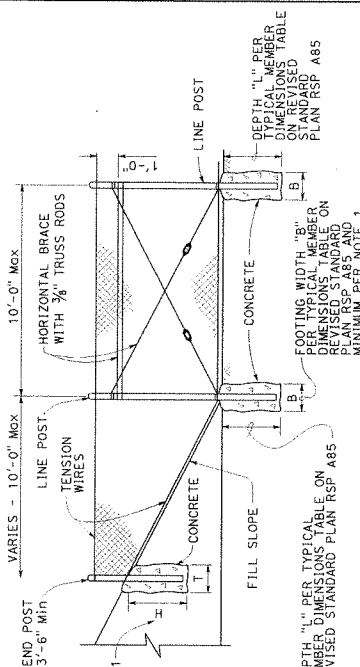
PLUNGER CUP DETAIL

GUSSET DETAIL

METHOD OF TYING FENCE TO HEADWALL

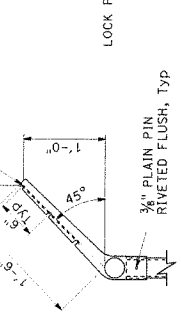


METHOD OF ERECTING FENCE FOR FILL SLOPE



WALK GATE

BARBED WIRE POST TOP



LINE POST

CORNER POST



STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION

CHAIN LINK FENCE DETAILS

NO SCALE

RSP A85A DATED JULY 15, 2016 SUPERSEDES STANDARD PLAN A85A
 DATED OCTOBER 30, 2015 - PAGE 118 OF THE STANDARD PLANS BOOK DATED 2015.

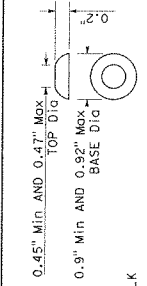
REVISED STANDARD PLAN RSP A85A

DIST. COUNTY	PROJECT NUMBER	DATE

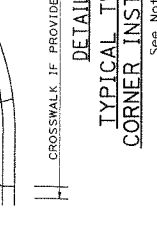
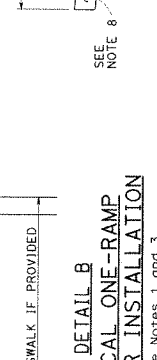
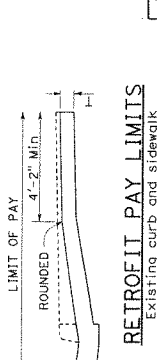
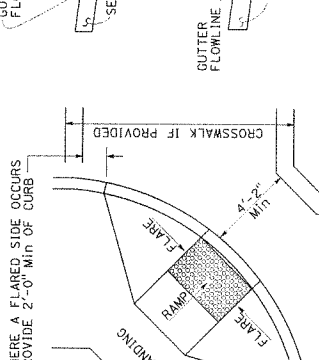
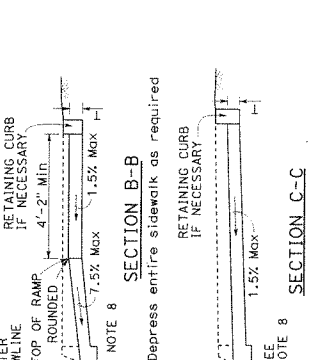
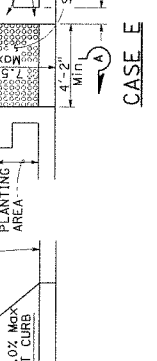
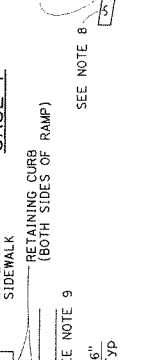
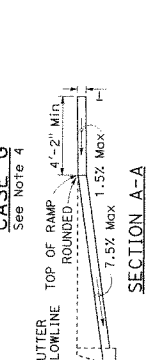
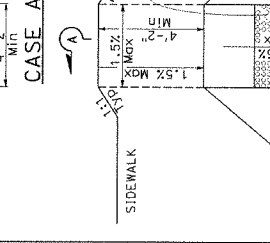
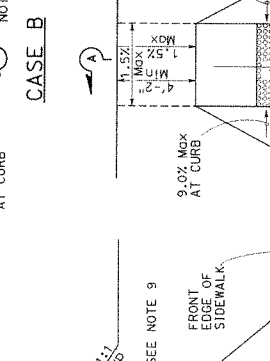
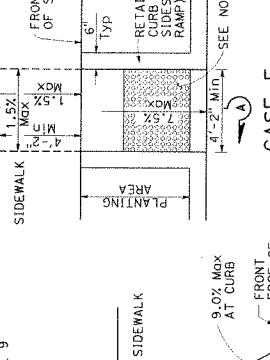
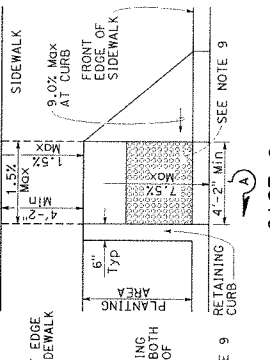
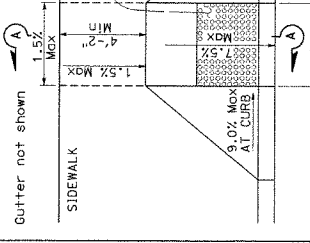
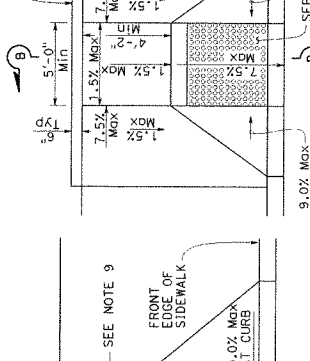
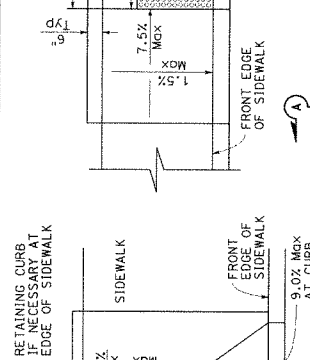
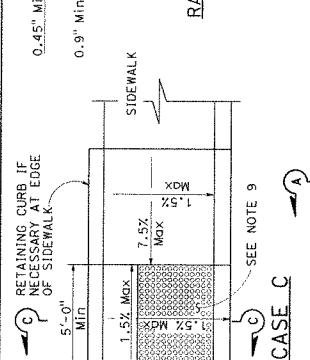
REGISTERED CIVIL ENGINEER
 JULY 15, 2016
 PLANS APPROVAL DATE

PROJECT: [REDACTED]
 SHEET: [REDACTED] OF [REDACTED]
 SCALE: [REDACTED]

RAISED TRUNCATED DOME



- NOTES:
- As site conditions dictate, Case A through Case G curb ramps may be used. The case shown shall be used unless otherwise noted. Case A and Case B are the preferred cases. Case C through Case G curb ramps also may be used at mid block locations, as site conditions dictate.
 - If distance from curb to back of sidewalk is too short to accommodate ramp and 4'-2" platform (landing) as shown in Case A, the sidewalk may be depressed longitudinally as in Case B or C or may be widened as in Case D.
 - When ramp is located in center of curb return, crosswalk configuration must be similar to that shown for Detail B.
 - As site conditions dictate, the retaining curb side and the flared side of the case G ramp shall be constructed in reversed position.
 - If located on a curve, the sides of the ramp need not be parallel, but the minimum width of the ramp shall be 4'-2".
 - Slope of ramp flares vary uniformly from a maximum of 9.0% at curb to conform with longitudinal sidewalk slope adjacent to top of the ramp, except in Case C and Case F.
 - The adjacent surfaces at transitions at curb ramps to walks, gutters, and streets shall be at the same level.
 - Counter slopes of adjoining gutters and road surfaces immediately adjacent to and within 24 inches of the curb ramp shall not be deeper than 1/2" (5.0%). Gutter pan slope shall not exceed 1% of depth for each 2'-0" of width.
 - Curb ramps shall have 9 detectable warning surface that extends the full width and 3'-0" depth of the ramp. A 4'-0" wide detectable warning surface may be used on a 4'-2" wide curb ramp. Detectable warning surfaces shall conform to the requirements in the Standard Specifications.
 - Sidewalk and ramp thickness, "T", shall be 3/2" minimum.
 - Utility pull boxes, manholes, vaults and all other utility facilities within the boundaries of the curb ramp will be relocated or adjusted to grade by the owner prior to, or in conjunction with, curb ramp construction.
 - Detectable warning surface may have to be cut to allow removal of utility covers while maintaining full detectable warning width and depth.



TO ACCOMPANY PLANS DATED _____

NO SCALE

RAISED TRUNCATED DOME PATTERN (IN-LINE)
 DETECTABLE WARNING SURFACE

STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
CURB RAMP DETAILS
 REVISION: [REDACTED]
 DATE: [REDACTED]

DATE: APRIL 15, 2016
 PROJECT: [REDACTED]
 SHEET NO.: [REDACTED]
 TOTAL SHEETS: [REDACTED]

Professional Engineer Seal:
 Theresa Gabriel
 No. E-39118
 State of California
 Electrical Engineering

APPROVAL DATE: APRIL 15, 2016
 PROJECT NO.: [REDACTED]
 SHEET NO.: [REDACTED]

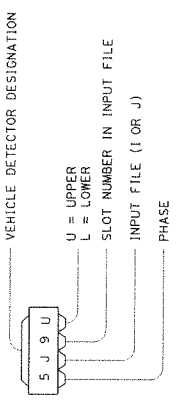
WIRING DIAGRAM LEGEND

- EXTERNAL CONDUCTOR CONDUCTOR OR BUS
- TIE POINT
- CONTACTOR COIL
- ⊗ CONTACTOR, NO CONTACT
- ⊙ CONTACTOR, NC CONTACT
- ENCLOSURE BOND
- ⊥ GROUNDING ELECTRODE
- ⊕ CIRCUIT BREAKER
- ⊖ RECEPTACLE

EQUIPMENT IDENTIFICATION

- ILLUMINATED SIGN IDENTIFICATION:**
 SIGN No. 12345
 10 ISL SCJ 1.0
 SIGN EQUIPMENT IDENTIFICATION CHARACTERS - PLACE ON POST OR STRUCTURE
 DO NOT PLACE ON STANDARD OR STRUCTURE
 TRANSFORMER RATING (KVA)
 LIGHTING CONTROL TYPE
 NUMBER AND TYPE OF FIXTURES
- ELECTROLIER OR EQUIPMENT IDENTIFICATION:**
 L2745 - 15'-0"
 MAST ARM LENGTH, IF SHOWN. DO NOT PLACE ON STANDARD OR STRUCTURE
 EQUIPMENT IDENTIFICATION CHARACTERS - PLACE ON STANDARD OR STRUCTURE
 IDENTIFICATION CHARACTERS ARE SHOWN IN PARENTHESES
- CONDUIT AND CONDUCTOR IDENTIFICATION:**
 1/2" C, 2#10, 15#14, 2 DLC, 12P#18
 NUMBER AND SIZE OF CONDUCTORS AND CABLES
 SIZE OF CONDUIT IN INCHES
 TRAFFIC PHASE IDENTIFICATION FOR SIGNALS, DETECTORS AND PHASE DIAGRAMS
 LEGEND NUMBERS
 EQUIPMENT, SIGNAL POLES, INSTALLATION OR ITEMS
 CONDUIT RUN NUMBERS

VEHICLE DETECTORS



TO ACCOMPANY PLANS DATED _____
 VEHICLE DETECTOR DESIGNATION

PULL BOXES

- NEW**
- 3 No. 3/2 PULL BOX
 - 5 No. 5 PULL BOX
 - 6 No. 6 PULL BOX
 - 7 No. 7 PULL BOX (CEILING)
 - 8 No. 8 PULL BOX
 - 9 No. 9 PULL BOX (STRUCTURE)
 - 9A No. 9A PULL BOX (STRUCTURE)
- EXISTING**
- 9A(21)
- PULL BOX, No. 5, UNLESS OTHERWISE INDICATED OR NOTED
- PULL BOX, ADDITIONAL DESIGNATIONS OR DESCRIPTIONS
 (C) COMMUNICATIONS PULL BOX
 (E) PULL BOX WITH EXTENSION
 (S) SPRINKLER CONTROL PULL BOX
 (21) ANCHOR BOLTS AND CONDUIT TYPE 21 STANDARD
 (T) TRAFFIC PULL BOX

SIGNAL AND LIGHTING STANDARD (TYPICAL DESIGNATION):

- 19A - 3' - 100 WIND VELOCITY = 100 MPH
- CASE 3 ARM LOADING
- STANDARD TYPE
- DETAIL, NUMBER, SECTION, TYPE OR ELEVATION

MISCELLANEOUS EQUIPMENT

- NEW**
- EMS CHANGEABLE MESSAGE SIGN
 - EMS CLOSED CIRCUIT TELEVISION CAMERA
 - EMS HIGHWAY ADVISORY RADIO POLE AND ANTENNA
 - EMS LED EXTINGUISHABLE MESSAGE SIGN
 - EMS DETECTION DEVICE
 - EMS M = MICROWAVE SENSOR
 - EMS V = VIDEO IMAGE SENSOR
- EXISTING**
- EMS CHANGEABLE MESSAGE SIGN
 - EMS CLOSED CIRCUIT TELEVISION CAMERA
 - EMS HIGHWAY ADVISORY RADIO POLE AND ANTENNA
 - EMS LED EXTINGUISHABLE MESSAGE SIGN
 - EMS DETECTION DEVICE
 - EMS M = MICROWAVE SENSOR
 - EMS V = VIDEO IMAGE SENSOR

ELECTRICAL SYSTEMS (LEGEND AND ABBREVIATIONS)

NO SCALE

STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION

RSP ES-1C DATED APRIL 15, 2016 SUPERSEDES STANDARD PLAN ES-1C DATED OCTOBER 30, 2015 - PAGE 420 OF THE STANDARD PLANS BOOK DATED 2015.

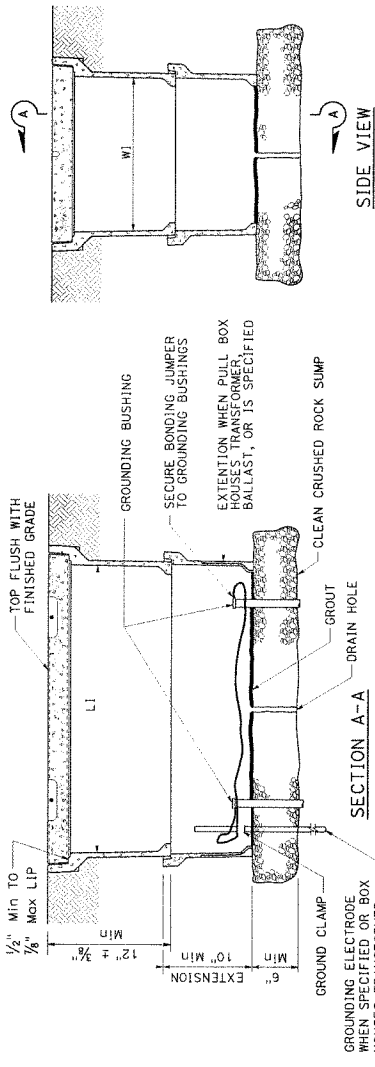
REVISED STANDARD PLAN RSP ES-1C

DIST. COUNTY: _____
 MODEL: _____
 TOTAL SHEETS: _____
 SHEET NO.: _____
 REGISTERED ELECTRICAL ENGINEER
 Thomas Gabriel
 No. E19129
 Exp. 6-30-16
 APRIL 15, 2016
 PLANS APPROVAL DATE
 THE ABOVE SHALL BE THE RESPONSIBILITY OF THE REGISTERED ELECTRICAL ENGINEER.
 ALL WORK SHALL BE IN ACCORDANCE WITH THE STANDARD PLANS BOOK DATED 2015.

TO ACCOMPANY PLANS DATED _____

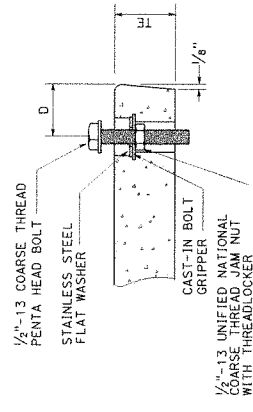
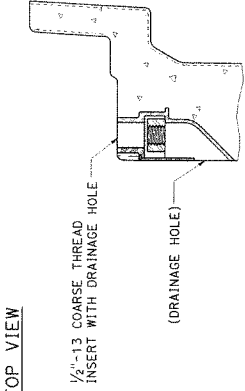
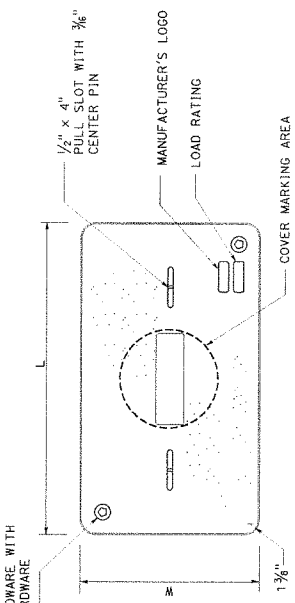
NOTES:

1. The nominal dimensions of the opening in which the cover sets shall be the same as the cover dimensions except the length and width dimensions shall be $\frac{1}{8}$ " greater.
2. Covers and boxes shall be interchangeable with California standard male and female gages. When interchanged with a standard male or female gage, the top surfaces shall be flush within $\frac{1}{8}$ ". Top outside radius of covers and pull boxes shall have a $\frac{1}{8}$ " radius.
3. Dimensions for the cover for non-traffic pull box are nominal values.



INSTALLATION DETAILS

DETAIL A



DIMENSION TABLE

PULL BOX	PULL BOX		COVER				MINIMUM WEIGHT			
	MINIMUM DEPTH	MINIMUM DEPTH EXTENSION	LI Min	WI Min	TE	D		L	W	
No. 3/2	12"	N/A	1'-3"	9"	1 3/4"	1 3/4"	1'-3 3/4"	1'-5 3/8"	10" - 10 1/8"	30 lb
No. 5	12"	10"	1'-8"	11"	2"	1 3/4"	1'-11 1/4"	1'-1 3/4"	1'-1 3/4"	60 lb
No. 6	12"	10"	2'-4 1/4"	1'-3 1/4"	2"	2"	2'-6 1/2"	1'-5 1/2"	1'-5 1/2"	85 lb

STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
ELECTRICAL SYSTEMS
(NON-TRAFFIC PULL BOX)
 NO SCALE
 RSP ES-8A DATED APRIL 15, 2016. SUPERSEDES STANDARD PLAN ES-8A
 DATED OCTOBER 30, 2015. PAGE 473 OF THE STANDARD PLANS BOOK DATED 2015.
REVISED STANDARD PLAN RSP ES-8A

DIST. COUNTY	NO. SHEETS	TOTAL SHEETS	DATE
Theresa Gabriel	1	1	April 15, 2016

REG. STAMP: ELECTRICAL ENGINEER
 No. E15129
 Exp. 6-30-16

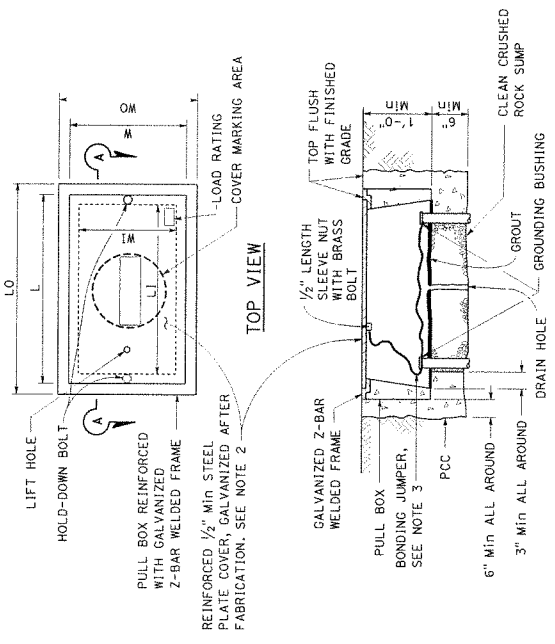
PLANS APPROVAL DATE

FOR ANY CHANGES TO BE MADE TO THESE PLANS, THE ENGINEER MUST BE CONSULTED. ANY CHANGES TO BE MADE TO THESE PLANS SHALL BE MADE BY THE ENGINEER'S OFFICE.

TO ACCOMPANY PLANS DATED _____

NOTES:

- Traffic pull box shall be provided with steel cover and special concrete footing. Steel reinforcing shall be as regularly used in the standard products of the respective manufacturer.
- Bonding jumper for metal covers shall be 3' long, minimum.
- The nominal dimensions of the opening in which the cover sets shall be the same as the cover dimensions except the length and width dimensions shall be $\frac{1}{8}$ " greater.
- Covers and boxes shall be interchangeable with California standard male and female gages. When interchanged with a standard male or female gage, the top surfaces shall be flush within $\frac{1}{8}$ ".



SECTION A-A
 No. 3/2(T), No. 5(T) AND
 No. 6(T) TRAFFIC PULL BOX

PULL BOX	PULL BOX					COVER		
	MINIMUM * THICKNESS	MINIMUM DEPTH BOX AND EXTENSION	LO	LI	WO	WI	L **	W **
No. 3/2(T)	1 1/2"	1'-0"	1'-10" - 1'-11"	1'-5" - 1'-6 1/2"	1'-3" - 1'-4"	10" - 1'-0"	1'-8" - 1'-8 1/2"	1'-1" - 1'-2"
No. 5(T)	1 3/4"	1'-0"	2'-5" - 2'-6"	2'-0" - 2'-1"	1'-6" - 1'-7"	1'-1" - 1'-2"	2'-3" - 2'-3 1/2"	1'-4" - 1'-4 1/2"
No. 6(T)	2"	1'-0"	2'-11" - 3'-1"	2'-6" - 2'-7"	1'-10" - 2'-0"	1'-5" - 1'-6"	2'-9" - 2'-9 1/2"	1'-8" - 1'-8 1/2"

* EXCLUDING CONDUIT WEB ** TOP DIMENSION

STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
**ELECTRICAL SYSTEMS
 (TRAFFIC PULL BOX)**
 NO SCALE

RSP ES-8B DATED APRIL 15, 2015. SUPERSEDES STANDARD PLAN ES-8B DATED OCTOBER 30, 2015 - PAGE 474 OF THE STANDARD PLANS BOOK DATED 2015.

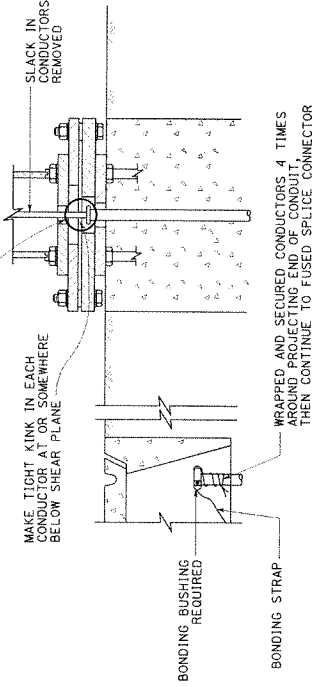
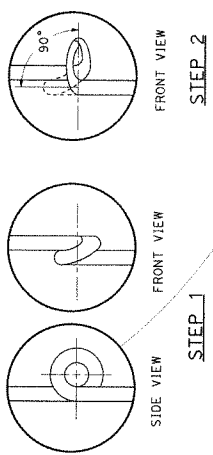
PROJECT: _____ SHEET: _____
 COUNTY: _____ ROUTE: _____
 REGISTERED ELECTRICAL ENGINEER
 Thomas Gabriel
 No. E15129
 Exp. 6-30-16
 APR 11 15, 2016
 FLSIS APPROVAL DATE
 1900 ASTOR DRIVE, SUITE 200, SAN JOSE, CA 95128
 (408) 253-1100 FAX: (408) 253-1101
 WWW.TGEE.COM

CIRCUIT VOLTAGE	FUSE CURRENT RATING			
	HPS LAMP BALLAST	LOW PRESSURE SODIUM BALLAST	INDUCTION SIGN LIGHTING TRANSFORMERS (PRIMARY SIDE)	SINGLE PHASE (TWO WIRE) TRANSFORMERS (PRIMARY SIDE)
120 V	70 W	180 W	85 W	1 KVA
240 V	5 A	5 A	5 A	2 KVA
480 V	5 A	5 A	5 A	10 A
	5 A	5 A	(SEE NOTE 2)	20 A
	5 A	3 A		3 A
				6 A
				10 A

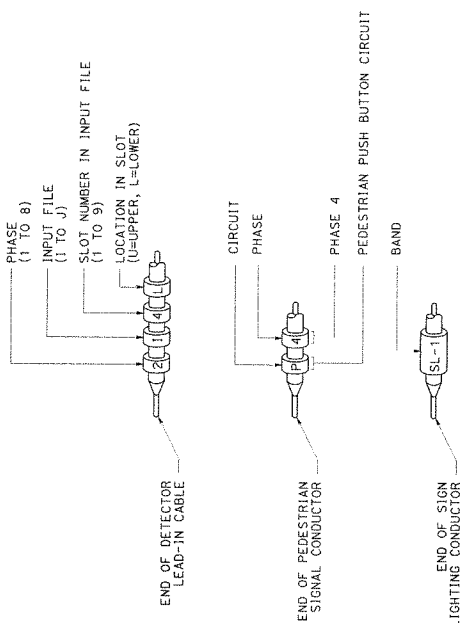
NOTES:

1. Primary lines of multiple ballasts shall be provided with fused connectors. Fuse ratings shall be as noted above.
2. See Standard Plan ES-15D, Type SC3 control.

FUSE RATINGS FOR FUSED CONNECTORS



KINKING DETAIL FOR SLIP BASE STANDARDS
 DETAIL A



TYPICAL BANDING DETAILS
 DETAIL B

STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
ELECTRICAL SYSTEMS
(FUSE RATING, KINKING AND BANDING DETAIL)

NO SCALE

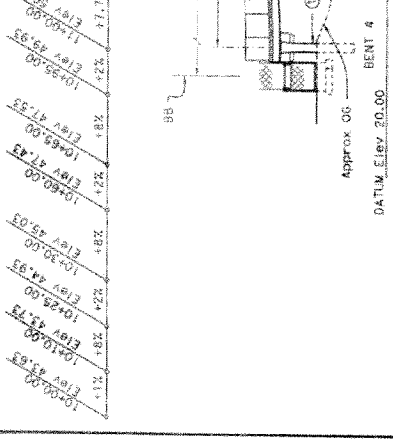
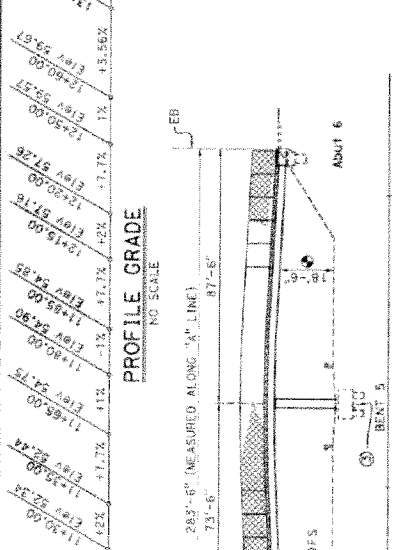
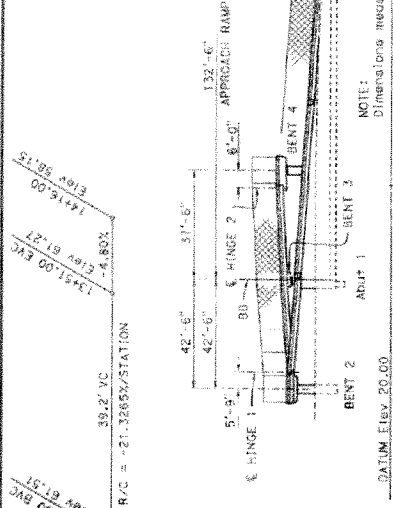
RSP ES-13B DATED APRIL 15, 2015. SUPERSEDES STANDARD PLAN ES-13B DATED OCTOBER 30, 2015. - PAGE 465 OF THE STANDARD PLANS BOOK DATED 2015.

SHEET NO.	ROUTE	TOTAL SHEETS	PAGE NO.
04	SM	1	1

REGISTERED CIVIL ENGINEER	DATE
[Signature]	4-28-18

PROJECT NO.	DATE
35-0306	4-28-18

INCOMPLETE PLAN
FOR REVIEW
DATE 10-DEC-2018
OFFICE OF STRUCTURAL ENGINEERING
STATE OF CALIFORNIA



ELEVATION A-A
1" = 20'

ELEVATION
1" = 20'

PLAN
1" = 20'

- NOTES:
- POINT "B" No. 35-0306"
 - POINT "SAN JOSE AVE POC"
 - EXISTING IS & POC TO BE RELOCATED, SEE ROADWAY PLANS
 - FOR "TYPICAL SECTION", SEE "GENERAL PLAN No. 2" SHEET
 - FOR "INDEX TO PLANS" AND "GENERAL NOTES", SEE "INDEX TO PLANS No. 1" SHEET
- LEGEND:
- Indicates point of minimum vertical clearance
 - Indicates existing structure
 - Indicates new construction
 - Indicates Stage 2, existing bridge removal
- Vehicular Traffic:
- New alignment, No traffic at the site.
 - Traffic will be detoured away from the site.
 - Traffic will be carried on the structure.
 - Stage construction will/will not be required.
 - Traffic will pass under the structure on temporary vertical clearances (Name of St. or Hwy.)
- Temporary Vertical Clearances:
- | | |
|-------------------------|---------|
| Clearance | Opening |
| 25' | 31' |
| 15' | 15' |
| Two-way | Two-way |
| For framing excavation. | |
- Pedestrian Traffic:
- Falswork opening required on _____ (Name of St.)
Location _____ Height _____ Width _____

NOTE: THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIAL.

DIVISION OF ENGINEERING SERVICES		PROJECT NO.	35-0306
STRUCTURE DESIGN		POST-MILL	44.04
DESIGN BRANCH		3	
STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION			
DESIGNER	DATE	CHECKED	DATE
J. J. [Signature]	4/28/18	[Signature]	4/28/18
DESIGNED BY	DATE	CHECKED	DATE
[Signature]	4/28/18	[Signature]	4/28/18
PROJECT NUMBER & FINDER	35-0306-21		
UNIT	3578		
CONTRACT NO.	14-48594		
SAN JOSE AVE POC (REPLACE)		GENERAL PLAN NO. 1	

DATE	04	SM	1	SHEET NO.	1	TOTAL SHEETS	1
REGISTERED CIVIL ENGINEER	DATE		PROJECT NO.		SHEET NO.		

PLANS APPROVAL DATE: _____

APPROVED FOR THE PROJECT BY: _____

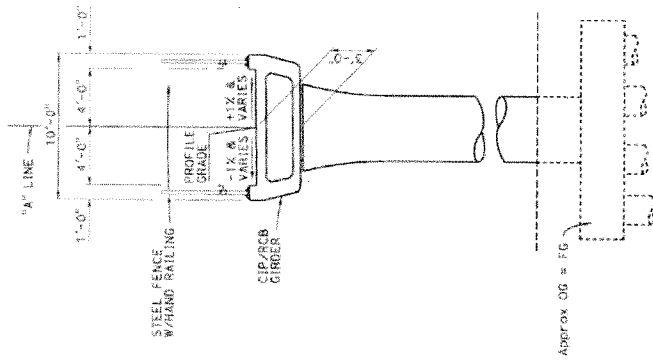
DATE: 07-08-2018

OFFICE OF TECHNICAL SERVICES

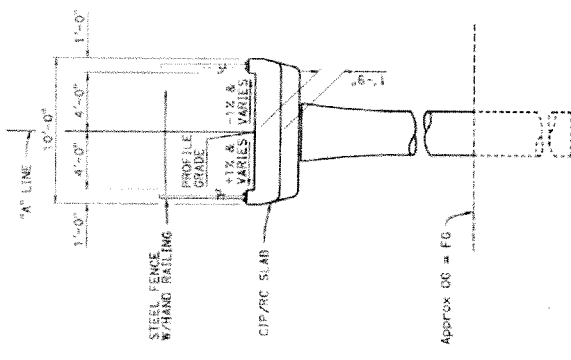
APPROVED FOR THE PROJECT BY: _____

DATE: 07-08-2018

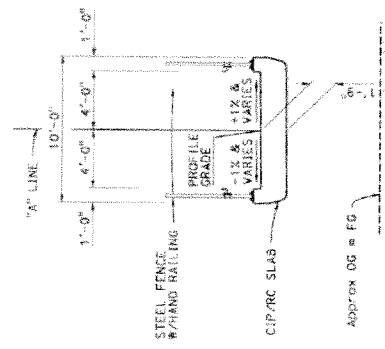
OFFICE OF TECHNICAL SERVICES



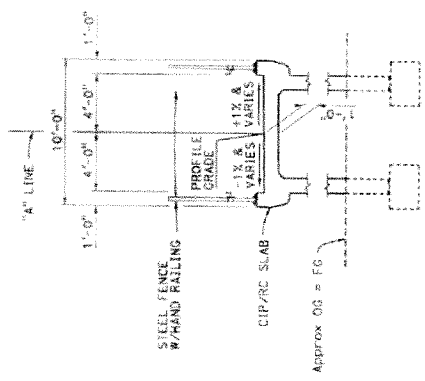
STAGE 1 CONSTRUCTION
STATION 12+49.00 TO 14+16.00



STAGE 1 CONSTRUCTION
STATION 11+64.25 TO 12+49.00



STAGE 1 CONSTRUCTION
STATION 11+32.50 TO 11+64.25



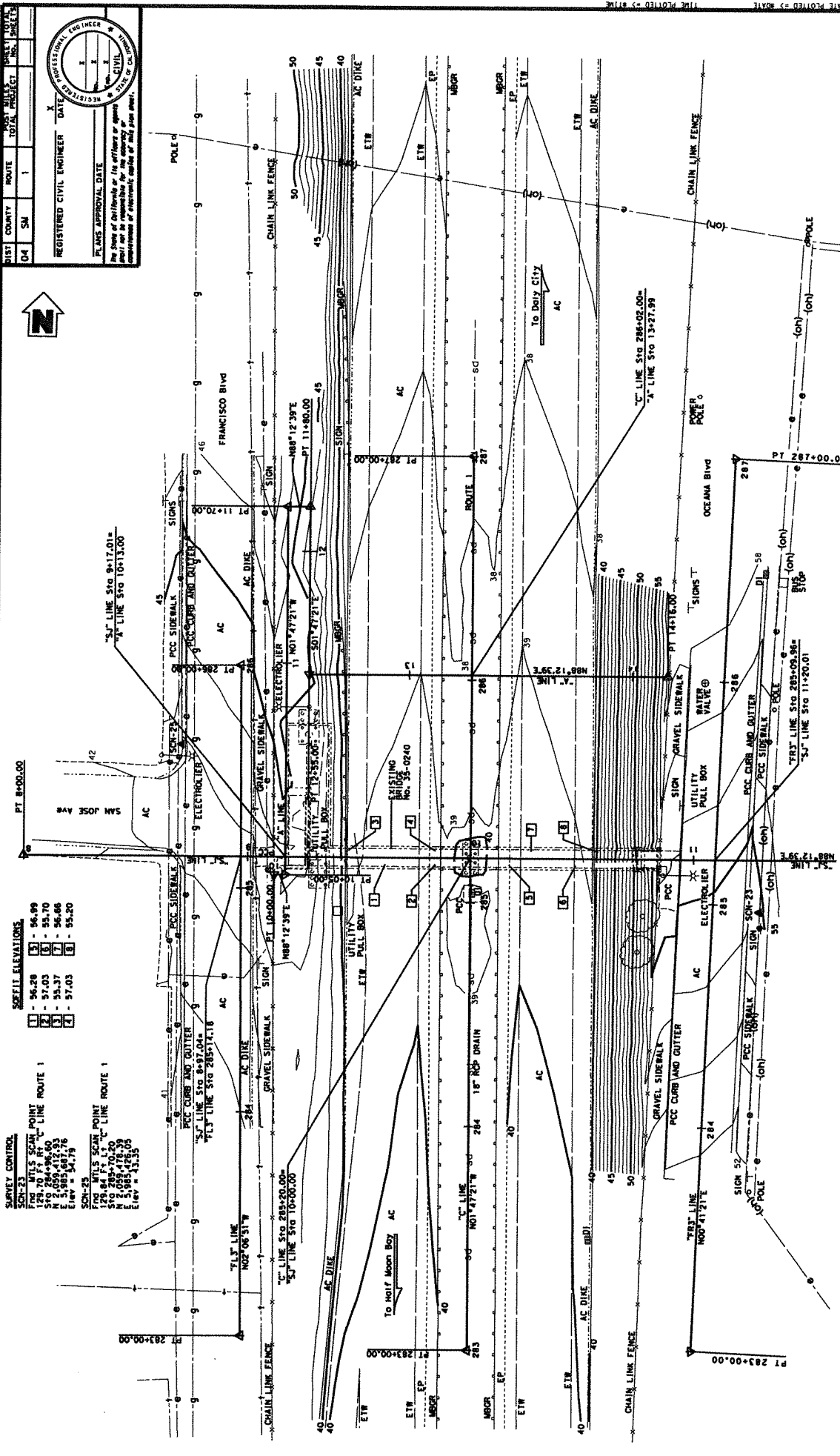
STAGE 3 CONSTRUCTION
STATION 10+00.00 TO 11+32.50

TYPICAL SECTION
1/4" = 1'-0"

DESIGNER J. BORDEN DESIGN ENGINEER	CHECKER MONSIEUR C. DAVILA	QUANTITIES BY J.	LAYOUT BY J.	LIVE CHARGING HEIGHT 12.5'	LIVE CHARGING WIDTH 10.0'	DESIGN VEHICLE MOTOR 10'	INVESTIGATIONS BY J.	LOAD & RESISTANCE FACTOR DESIGN BY J.	DIVISION OF ENGINEERING SERVICES STRUCTURE DESIGN DESIGN BRANCH 3	SHEET NO. 1	CONTRACT NO. 04-02554	SAN JOSE AVE POC (REPLACE) GENERAL PLAN No. 2	
												PROJECT NUMBER & PHASE 04-02554(1)	SHEET NO. 1

DIST	COUNTY	ROUTE	TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	SM	1			

REGISTERED CIVIL ENGINEER DATE _____
 PLANS APPROVAL DATE _____
 The State of California, its officers or agents, shall not be held responsible for the construction of structures under the plans shown hereon.



SURVEY CONTROL
 SON-23
 FID MTL'S SCAN POINT
 570 284+86.50
 N 2 059 415.93
 E 1 848 407.76
 ELEV = 54.76

SON-25
 FID MTL'S SCAN POINT
 129 84 F.I.
 N 12 059 470.50
 E 5 188 428.05
 ELEV = 43.35

SEWELL ELEVATIONS
 1 - 96.28
 2 - 97.03
 3 - 95.37
 4 - 97.03
 5 - 96.99
 6 - 95.70
 7 - 96.66
 8 - 95.20

PRELIMINARY INVESTIGATION SECTION

SCALE: VERTICAL CURVES 1"=20'
 HORIZONTAL CURVES 1"=50'
 ALIGNED TIE DIAL TRAVERSE SHEET 1"=20'

CHECKED BY: A. LASELL
 DATE: 06/20/11

CHECKED BY: S. SOU
 DATE: 06/20/11

DESIGN BY: S. SOU
 DATE: 06/20/11

QUANTITIES BY: S. SOU
 DATE: 06/20/11

STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION

SAN JOSE POC (REPLACE)
FOUNDATION PLAN

PROJECT NUMBER & PHASE: 041300025.1 CONTRACT NO.: 04-6289

DATE PLOTTED: 05/21/11

CITY	COUNTY	ROUTE	SHEET NO.	TOTAL SHEETS
SM	SM	1		

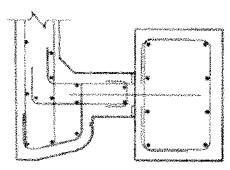
REGISTERED CIVIL ENGINEER DATE: 10/15/2014

PLANS: APPROVAL DATE: 10/15/2014

THE STATE OF CALIFORNIA OFFICE OF THE ENGINEER OR SURVEYOR

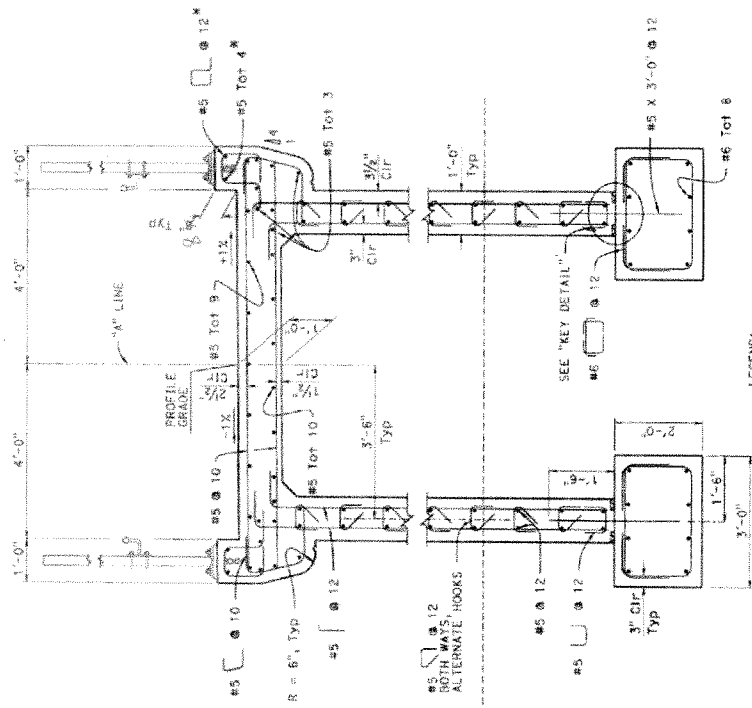
INCOMPLETE PLAN FOR DESIGN STUDY DATE: 10/15/2014 OFFICE OF THE ENGINEER OR SURVEYOR STATE OF CALIFORNIA

FOR REFERENCE ONLY



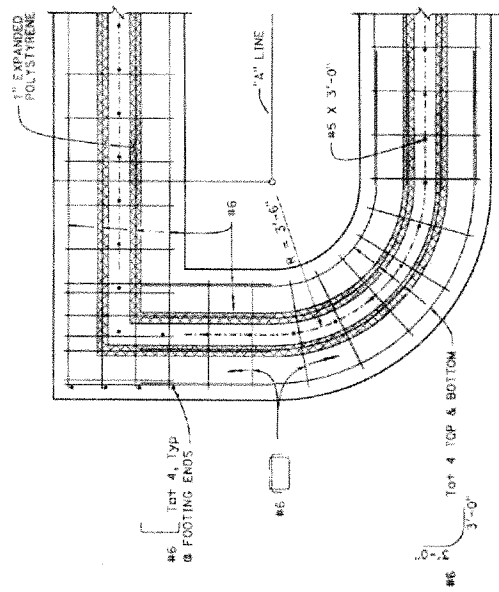
SECTION C-C
1/2" = 1'-0"

NOTE:
For details not shown,
see SECTION D-D



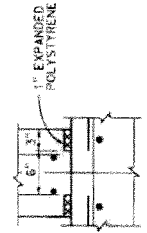
SECTION D-D
1/2" = 1'-0"

LEGEND:
* Indicates stainless steel
NOTE:
For details not shown, see
"PART FOOTING PLAN"



PART FOOTING PLAN
3/4" = 1'-0"

NOTE:
For details not shown,
see SECTION D-D



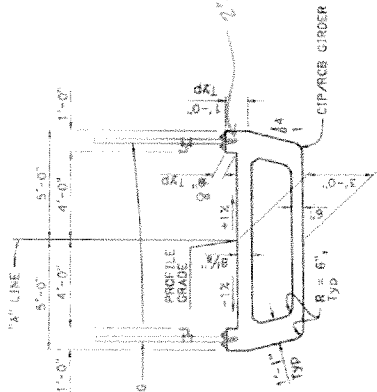
KEY DETAIL
1/2" = 1'-0"

STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION	DIVISION OF ENGINEERING SERVICES STRUCTURE DESIGN DESIGN BRANCH 3	BRIDGE NO. 35-0356	PROJECT NO. 44.0A	SHEET NO. 1	TOTAL SHEETS 1
PROJECT NUMBER & PHASE: 04130002E1	CONTRACT NO.: 04-46854	SAN JOSE AVE POC (REPLACE)			
APPROACH RAMP DETAILS No. 2					
DATE: 10/15/2014	PROJECT NUMBER & PHASE: 04130002E1	CONTRACT NO.: 04-46854			

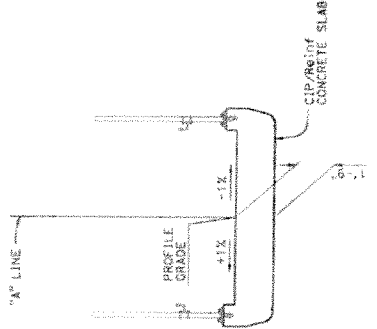
DATE	DESIGN	PROJECT	SHEET NO.	TOTAL SHEETS
04	SM			

REGISTERED CIVIL ENGINEER
 STATE OF CALIFORNIA
 LICENSE NO. 12441
 EXPIRES 12/31/16
 DATE OF SIGNATURE 04/11/16

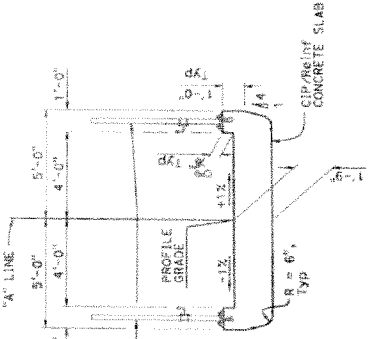
INCOMPLETE PLAN FOR DESIGN STUDY
 PRINTED
 DATE 10/06/2016
 OFFICE OF STRUCTURE DESIGN
 STATE OF CALIFORNIA



STATION 12+49.00 TO 14+16.00

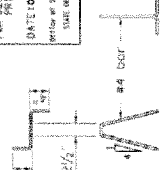


STATION 11+64.25 TO 12+49.00

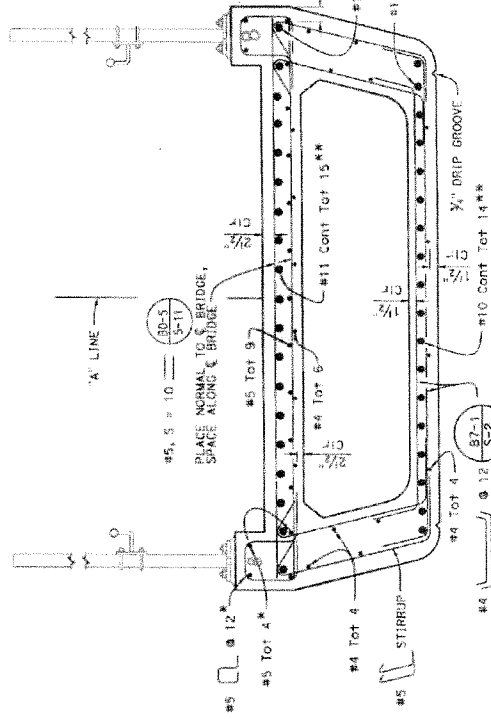


STATION 11+32.50 TO 11+64.25

BAR CHAIR DETAIL

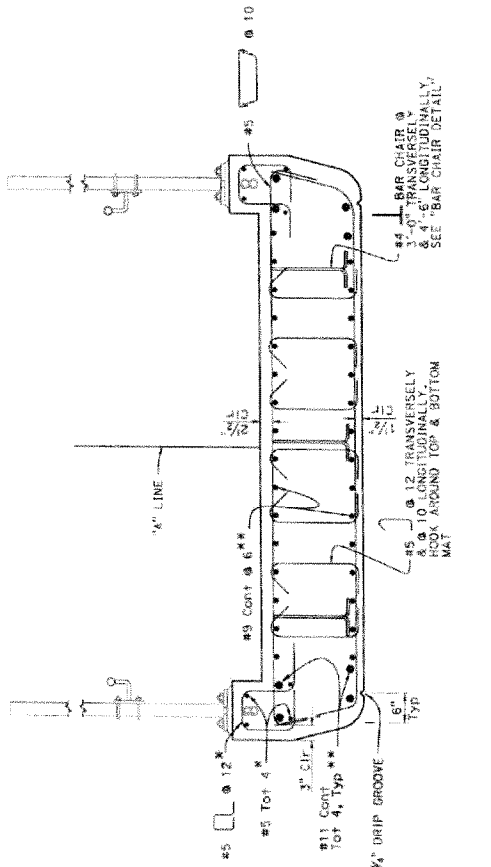


FOR REFERENCE ONLY



CIP/RCS GIRDER REINFORCEMENT

1" = 1'-0"



CIP/Reinf CONCRETE SLAB

1" = 1'-0"

LEGEND:
 * Indicates stainless steel reinf
 ** Only service butt splices are allowed, see "DETAIL A" for typical splice configuration and limitations on "TYPICAL SECTION No. 2" sheet

DIVISION OF ENGINEERING SERVICES STRUCTURE DESIGN		PROJECT NO.	CONTRACT NO.
DESIGN BRANCH 3		35-0356	44-04
STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION		PROJECT NUMBER & PHASE: (411000002)	CONTRACT NO. 04-0356
SAN JOSE AVE POC (REPLACE)		PROJECT NUMBER & PHASE: (411000002)	CONTRACT NO. 04-0356
TYPICAL SECTION No. 1		PROJECT NUMBER & PHASE: (411000002)	CONTRACT NO. 04-0356

XIV. Architectural Renderings



San Jose Ave POC (Replace) looking south

February 1, 2017

Via Fax 650-359-5807

Planning Commission
C/O Tina Wehrmeister
Planning Director
City of Pacifica
1800 Francisco Boulevard
Pacifica, CA 94044

Re: Coastal Development Permit CDP-375-16

Dear Planning Commission Members:

AIMCO Horizons West Apartments, LLC owns Horizons West Apartments located at 365 Talbot Avenue, Pacifica, CA 94044. Regarding the above-referenced CDP application, as an affected property owner we respectfully request City of Pacifica Planning Staff and the Planning Commission seriously consider mitigating the visual impact as seen from our property on the hillside directly above the proposed pedestrian overcrossing. Specifically, we recommend that Staff and the Planning Commission require the California Department of Transportation to:

1. Add mature landscape plant material both in tree and shrub form around the ramp and landing portions of the new bridge/ramp systems; and
2. Add decorative concrete stamping or graphics to the face and sides of the proposed bridge as seen from the highway and surrounding streets.

These mitigation measures should fit well with the picturesque town of Pacifica.

Thank you for your consideration.

Sincerely,

AIMCO Horizons West Apartments, LLC

By: _____


Sean Finnegan
Authorized Representative



Scenic Pacifica
Incorporated Nov. 22, 1957

PLANNING COMMISSION Staff Report

DATE: February 6, 2017

FILE: UP-080-16
PSD-816-16

ITEM: 3

PUBLIC NOTICE: Notice of Public Hearing was published in the Pacifica Tribune on January 25, 2017, and was mailed to 394 property owners and occupants within the 300 feet of the Project site.

APPLICANT/OWNER: City of Pacifica
170 Santa Maria Avenue
Pacifica, CA 94044

PROJECT LOCATION: 540 Crespi Drive (APN: 022-162-420)

PROJECT DESCRIPTION: Construction of a 2.1 million-gallon capacity equalization (EQ) basin, a 10-foot tall motor control center building, ventilation and odor-control system, and a cleaning system within the EQ basin at 540 Crespi Drive in Pacifica. The Project would also include construction of two diversion structures to passively divert excess flows from the existing Linda Mar and Arguello sanitary sewer lines and transport the flow via a conveyance pipeline to the EQ basin during storm events and an effluent conveyance pipeline routing flows to the existing Crespi Drive sanitary sewer line and Linda Mar Blvd Pump Station.

SITE DESIGNATIONS: General Plan: Public Facility
Zoning: M-1 (Controlled Manufacturing District)

RECOMMENDED CEQA STATUS: A Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program have been prepared and are recommended for adoption.

ADDITIONAL REQUIRED APPROVALS: None. Subject to appeal to the City Council.

RECOMMENDED ACTION: Approve, as conditioned.

PREPARED BY: Bonny O'Connor, Assistant Planner

PROJECT SUMMARY

1. Project Background

The City of Pacifica (City) would construct and utilize the proposed wet weather flow equalization basin (EQ basin) and associated pipelines (together referred to as “Project”) as a key element to mitigate storm-related sanitary sewer overflows (SSOs) in the City’s wastewater collection system and reduce peak wet weather flows to the City’s Calera Creek Water Recycling Plant. The City’s wastewater collection system is subject to infiltration and inflow (I/I) of extraneous groundwater and storm water into the collection system, resulting in high wet weather flows during storm events. As a result, SSOs have occurred at several locations in the system during large storms.

Since 2004, the City has been required to monitor and report occurrences of SSOs, initially to the San Francisco Bay Regional Quality Control Board (RWQCB), and since 2007 to the State Water Resources Control Board (SWRCB) under the Statewide General Waste Discharge Requirements for Sanitary Sewer Systems adopted in 2006. Under the Regional and State regulations, the City is also required to prepare and adopt a Sewer System Management Plan (SSMP), which must include plans and programs for addressing the operation and maintenance of the system and assessing its condition and capacity. The City prepared the Collection System Master Plan¹ (Master Plan) to meet specific SSMP requirements, as well as to develop a long-term plan for rehabilitation, replacement, and capacity improvements to its system.

As a result of SSOs that have occurred in the system, the RWQCB issued a Cease and Desist Order (CDO) to the City in May 2011, and the City entered into a Consent Decree with Our Children’s Earth Foundation in June 2011, requiring it to implement a number of measures targeted at reducing SSOs. The CDO and Consent Decree have similar requirements, including:

- Developing and implementing an SSO reduction plan;
- Implementing a computerized maintenance management system;
- Implementing programs addressing system-wide cleaning, root control, illicit discharge elimination, and private sewer lateral inspection and repair;
- Performing a condition assessment of the collection system;
- Preparing a System Evaluation and Capacity Assurance Plan (SECAP);
- Staff training in implementing the measures; and
- Developing an initial and long-term capital improvement plan (CIP) and financial plan.

The Master Plan specifically addresses the condition assessment of the system (based on inspection data collected), the SECAP, and development of a long-term CIP.

The City evaluated many options to meet the requirements presented in the CDO and Consent Decree. The Master Plan included evaluation of three alternatives, including a capacity enhancement only project, the installation of an EQ basin, and capacity improvements with sewer system rehabilitation. Based on this comparison, the best alternative was determined to be the implementation of an EQ basin. This alternative was determined to provide the best assurance of meeting regulatory and legal

¹ RMC. 2011. City of Pacifica Collection System Master Plan Final Report. October. Available Online: <http://www.cityofpacifica.org/civicax/filebank/blobdload.aspx?blobid=5267>

requirements to eliminate capacity-related SSOs in the near term, and has the lowest estimated capital cost of the three alternatives.

In August 2015 Wet Weather EQ basin Site Feasibility Evaluation^{2,3} was released and included an evaluation of multiple locations for the project. The Sites identified in the Feasibility Evaluation and Addendum are shown in Attachment D. The current site (Site 2C) was selected by the City Council as the preferred site for placement of the EQ basin as it was property the City already owned, had a lower cost for construction, and presented the least public impact.

The objective of the Project is to meet the requirements of the CDO and Consent Decree. Specifically, the Project was designed to meet the following condition of the CDO: "By January 1, 2019, the Discharger shall have no insufficient capacity-caused SSOs."

2. Project Description

The Project would include the construction of a 2.1-million-gallon capacity EQ basin, two diversion structures to passively divert excess flows from the existing Linda Mar and Arguello sanitary sewer lines and transport the flow via a conveyance pipeline to the EQ basin during storm events, an effluent conveyance pipeline routing flows to the existing Crespi Drive sanitary sewer line and Linda Mar Blvd Pump Station, a 10-foot tall motor control center building, ventilation and odor-control system, and a cleaning system within the EQ basin.

The EQ basin would be located within the parking lot for the City of Pacifica's Skatepark and Community Center (located at 540 Crespi Drive) on the east side of SR-1. The proposed two diversion structures and conveyance pipelines to and from the EQ basin would be constructed in City-owned right-of-ways or public utility easements.

A description of the construction and operation phases of the Project is provided in Attachment E. During construction of the EQ Basin, the Skatepark parking lot would be closed to the public, and normal parking in this area would be diverted to the Crespi Parking Lot west of the Community Center. The Skatepark and Community Center would maintain their regular hours of operation. Following completion of EQ basin construction, the Skatepark parking lot would be reconstructed over the EQ basin structure to provide, at minimum, the same number of parking spots as in the existing parking lot.

Project construction is anticipated to occur during a 17-month period starting in May 2017 and ending in September 2018. Construction would occur Monday through Friday during the hours as detailed in Table 1.

² RMC. 2015. City of Pacifica Wet Weather Equalization Basin Site Feasibility Evaluation Final Report. August. Available Online: <http://www.cityofpacifica.org/civicax/filebank/blobdload.aspx?blobid=7811>

³ RMC. 2015. City of Pacifica Wet Weather Equalization Basin Site Feasibility Evaluation Site Alternative 2C and Prioritization Addendum to Site Feasibility Evaluation Report. August. Available Online: <http://www.cityofpacifica.org/civicax/filebank/blobdload.aspx?blobid=7812>

Table 1. Construction Hours

Location	Hours of Construction
Within the EQ basin area (Skatepark parking lot area including the excavation for the jacking and receiving pits)	8:00 a.m. to 5:00 p.m.
Along City streets (except at the intersection of Linda Mar Boulevard and De Solo Drive)	9:00 a.m. to 4:00 p.m.
At the intersection of Linda Mar Boulevard and De Solo Drive	9:00 a.m. to 3:00 p.m.

3. General Plan, Zoning, and Surrounding Land Uses

The General Plan land use designation for the site is Public Facilities. The zoning district for the site is M-1 (Controlled Manufacturing District). The M-1 District has the same permitted uses and development regulations as the C-3 District. The proposed EQ basin construction site is bounded by vacant areas to the east and west, residential parcels approximately 80 feet to the southwest, and the Skatepark and Community Center complexes to the west and north, respectively. There is an open, vegetated drainage swale between the southeastern end of the parking lot and the nearby residences.

4. Municipal Code

The Pacifica Municipal Code (PMC) requires two discretionary permits for this project, including a Use Permit (UP) and Site Development Permit (PSD).

In accordance with PMC Section 9-4.2303, “in any district where public utility facilities are not expressly permitted, such facilities may be permitted upon securing a use permit [...]” Additionally, the Project site is adjacent to an R-1 (Single Family Residential District) zoning district and per PMC Section 9-4.1202(i), all uses in a C-3 zoning district abutting a residential district require a UP. Lastly, PMC Section 9-4.2306(a)(12) identifies wastewater treatment and reclamation facilities as a special use that Planning Commission can accommodate in any zoning district if it meets the special use permit review criteria detailed in PMC Section 9-4.2306(d).

- **Use Permit UP-080-16:** The Planning Commission must make the following findings in order to approve an UP [PMC Sec. 9-4.3303(a)]:
 - i. That the establishment, maintenance, or operation of the use or building applied for will not, under the circumstances of the particular case, be detrimental to the health, safety, and welfare of the persons residing or working in the neighborhood or to the general welfare of the City;
 - ii. That the use or building applied for is consistent with the applicable provisions of the General Plan and other applicable laws of the City and, where applicable, the local Coastal Plan; and
 - iii. Where applicable, that the use or building applied for is consistent with the City's adopted Design Guidelines.

In addition, the Planning Commission must determine if the Project meets the following special use permit criteria detailed in PMC Section 9-4.2306(d).

- iv. That the proposed use will be of such size, design, and operating characteristics as will tend to keep it compatible with permitted uses in the district under consideration with respect to bulk, scale, coverage, density, noise, and generation of traffic;
- v. That the proposed development will enhance the successful operation of the community or will provide a service to the community;
- vi. That particular attention is given to the provision of buffering of uses from the surrounding neighborhood;
- vii. That the project conforms with the setback, coverage, landscaping, and other zoning regulations of the district where a use is proposed; and
- viii. That the project is consistent with the goals and policies of the General Plan, Local Coastal Plan and with the adopted Design Guidelines.

PMC Section 9-4.3201(a) requires a PSD for all new construction in a commercial District.

- **Site Development Permit PSD-816-16:** The Planning Commission shall not issue a Site Development Permit if the Commission makes any of the following findings [PMC Sec. 9-4.3204(a)]:
 - i. That the location, size, and intensity of the proposed operation will create a hazardous or inconvenient vehicular or pedestrian traffic pattern, taking into account the proposed use as compared with the general character and intensity of the neighborhood;
 - ii. That the accessibility of off-street parking areas and the relation of parking areas with respect to traffic on adjacent streets will create a hazardous or inconvenient condition to adjacent or surrounding uses;
 - iii. That insufficient landscaped areas have been reserved for the purposes of separating or screening service and storage areas from the street and adjoining building sites, breaking up large expanses of paved areas, and separating or screening parking lots from the street and adjoining building areas from paved areas to provide access from buildings to open areas;
 - iv. That the proposed development, as set forth on the plans, will unreasonably restrict or cut out light and air on the property and on other property in the neighborhood, or will hinder or discourage the appropriate development and use of land and buildings in the neighborhood, or impair the value thereof;
 - v. That the improvement of any commercial or industrial structure, as shown on the elevations as submitted, is substantially detrimental to the character or value of an adjacent R District area;
 - vi. That the proposed development will excessively damage or destroy natural features, including trees, shrubs, creeks, and rocks, and the natural grade of the site, except as provided in the subdivision regulations as set forth in Chapter 1 of Title 10 of this Code;
 - vii. That there is insufficient variety in the design of the structure and grounds to avoid monotony in the external appearance;

- viii. That the proposed development is inconsistent with the City's adopted Design Guidelines; or
- ix. That the proposed development is inconsistent with the General Plan, Local Coastal Plan, or other applicable laws of the City.

5. Required Findings

A. In order to approve UP-080-16 the Planning Commission must make the following three findings required by PMC Sections 9-4.3303(a):

- i. That the establishment, maintenance, or operation of the use or building applied for will not, under the circumstances of the particular case, be detrimental to the health, safety, and welfare of the persons residing or working in the neighborhood or to the general welfare of the City;

Discussion: As further discussed below in Section 5 and in Attachment E of this staff report, the MND/IS prepared for the proposed Project concludes that the construction, operation and maintenance of the Project would not have a significant impact on the environment with the implementation of the incorporated mitigation measures (MMs). The Project would meet all applicable building code and engineering requirements and would comply with all applicable regulatory requirements.

- ii. That the use or building applied for is consistent with the applicable provisions of the General Plan and other applicable laws of the City and, where applicable, the local Coastal Plan; and

Discussion: The proposed use is consistent with the General Plan and the local Coastal Plan is not applicable to the Project site. The issue of I/I into the sanitary sewer system during wet weather events and the need to improve the sanitary sewer system capacity is mentioned in multiple locations within the General Plan, as noted below.

Conservation Element

Inflow and infiltration into the sewage collection system, particularly in the Linda Mar area, results in overflows during long periods of wet weather. The exact locations of this problem are unknown and difficult to assess. The magnitude of the problem and the cost of correction are estimated to be great. The City should seek assistance to investigate and reduce this problem. (Page 114)

Action Programs, Short Term, 1: *Seek outside assistance to study and correct the infiltration problem in Linda Mar sewage collection system. (Page 16)*

Community Facilities Element

The Regional Board is presently concerned about compliance during wet weather conditions. Reduction of the peak wet weather flows through an inflow and infiltration reduction program and modification to the treatment plant may be required within the next five years. (Page 96)

Policy 1: Maintain and improve the present level of City services. (Page 21)

The City is separately working on directly addressing the I/I issue by replacing sanitary sewer piping as funding will allow. However, the cost and work associated with correcting the I/I in the Linda Mar area is significant and is a long-term goal. Meanwhile, the Linda Mar area experiences SSOs as a result of the I/I. The proposed Project would address the SSOs that result from the I/I during wet weather events by creating additional capacity to the Linda Mar sanitary sewer collection system. SSOs create a significant impact on the environment, and in particular on water quality. The Project would prevent capacity related SSOs from occurring during wet weather events. The objective of the proposed Project would be consistent with the General Plan as the proposed Project would address the results of the I/I issue discussed in the General Plan.

Additionally, construction of the Project is consistent with other resources considered in the General Plan as further discussed below:

Noise Element

Action Programs, Short Term, 5: The noise impact on land uses should be considered when development plans are reviewed and approved. Where existing ambient noise levels are high, or where the proposed use will create additional noise, the builder should be required to mitigate the noise. (Page 18)

Discussion: The MND/IS analyzed the impact of the proposed Project on the existing ambient noise levels (See Attachment E and Section 5 of this staff report). It was concluded that, with the incorporation of MM NOISE-1, impacts on noise levels for surrounding receptors would be less than significant during construction and operation of the Project.

Seismic Safety and Safety Element

Policy 1. Prohibit development in hazardous areas, including flood zones, unless detailed site investigations ensure that risks can be reduced to acceptable levels and the structure will be protected for its design life. Development shall be design to withstand a minimum of a 100 year hazard event regardless of the specific nature of the hazard. (Page 110)

Discussion: The southern part of the Community Center property, including the existing Skatepark parking lot, and the influent pipeline alignment are located within the Special Flood Hazard Zone AH, which is a Federal Emergency Management Agency (FEMA) 100-year flood plain⁴. Proposed site improvements including site grading and construction of the EQ basin, the Motor Control Center (MCC) Building, and the odor control system (fan and granular activated carbon [GAC] absorber) are not anticipated to result in additional displacement of flood flows associated with the 100-year event as the overall grade following the

⁴ Federal Emergency Management Agency. 2017. FEMA National Flood Hazard Layer. Accessed January 4, 2017.

installation of the EQ basin would be similar to the existing grade. The elevation at the northern portion of the reconstructed parking lot would be slightly higher than the existing grade, and would slope to the south where the elevation would be slightly lower than existing grade. In addition, the proposed bioretention areas would increase the pervious area of the site, thereby allowing for greater infiltration during storm events. The existing street elevations along the new pipelines would not be changed from existing conditions.

The concrete slab for the MCC building and the odor control system would be constructed above the FEMA 100-year flood elevation of 14 feet above mean sea level⁵.

In addition, the Project would be consistent with the Pacifica Municipal Code, California Building Code, and other applicable regulations.

- iii. Where applicable, that the use or building applied for is consistent with the City's adopted Design Guidelines.

Discussion: The Project components are primarily located underground. Aboveground features of the Project include the MCC building, odor control system including ambient air intakes, and the redesigned parking lot. These features are consistent with the City's adopted Design Guidelines as further discussed below.

Building Design, Screening. All exposed mechanical and electrical equipment must be screened from public view. The design of such screening should be integral part of the building design. (Page 5)

Discussion: A prefabricated, 10-foot tall MCC building equipped with a 4-foot high metal antenna that would allow radio communication with the City's Calera Waste Water Treatment Plant and an odor control system would be located along the northeastern portion of the Skatepark parking lot area. The MCC building would contain four motor starters, one for each of the 10 horsepower (hp) duty pumps and one for each of the 2 hp dewatering pumps, a panel board, a telemetry panel, and a transformer to step down voltages to the ancillary electrical facilities. Two at-grade ambient air inlets would be located on the west side of the EQ basin. The odor control system would appear as an approximate 5.5 foot high cylindrical structure, surrounded by a 6 foot high chain linked fence, located next to the MCC building. Landscaping would be used to reduce the visibility of these structures from public view. Condition of Approval (COA) 2 would require the implementation of a landscaping plan to screen the new aboveground structures.

Electrical, water, and other utility connections would be provided to the Project through underground infrastructure.

Landscaping, Parking areas. All parking areas should be landscaped with fast growing trees and/or shrubs in order to screen vehicles from view and minimize the visual

⁵ *Ibid.*

impact of expansive areas of asphalt. Such landscaping should not obscure views necessary for traffic safety. (Page 6)

Discussion: The effected parking lot is located behind the existing Community Center building and Skatepark and not significantly visible from Crespi Drive or Highway 1. In addition to MM AES-1, which would require the replanting of removed heritage trees at the completion of construction, COA 2 would require the implementation of a landscaping plan in the bioretention areas, the landscaping island on the south side of the parking lot and the landscaping strip bordering the west, south, and north portions of the EQ basin to be planted with trees and shrubs to screen the new aboveground structures.

Landscaping, Screening. *Dense landscaping should be used to screen unattractive features such as storage areas, trash enclosures, transformers and generators, vacuum breakers, and elements which do not contribute to the visual enhancement of the surroundings. (Page 6)*

Discussion: See discussions under Design Guidelines: *Building Design, Screening and Landscaping, Parking areas* provided above.

Water Conservation Policy and Landscape Design Guidelines for New Development, Irrigation 1. *All parks, median strips, landscaped public areas, landscaped areas surrounding residential and commercial developments and industrial parks shall have separately metered automatic irrigation systems designed by a landscape architect or other competent person. (Page 8)*

Discussion: COA 2 would require the implementation of a landscaping plan. The plan would detail the irrigation needs of the property. However, the landscaping plan would include use of drought tolerant and mostly native vegetation; therefore, irrigation would likely only be required during the establishment of the vegetation and would not require long-term irrigation.

In addition, the Planning Commission must determine if the Project meets the following special use permit criteria detailed in PMC Section 9-4.2306(d).

- ix. That the proposed use will be of such size, design, and operating characteristics as will tend to keep it compatible with permitted uses in the district under consideration with respect to bulk, scale, coverage, density, noise, and generation of traffic;

Discussion: The Project components are primarily located underground. Aboveground components of the Project include the MCC building, odor control system, and the redesigned parking lot. The MCC would be tallest above ground structure, which would be a 10-foot tall prefabricated building with a 4-foot high metal antenna on top. The aboveground components of the Project would not be out of scale or bulk with the M-1/C-3 zoning district. The M-1/C-3 zoning district does not have a coverage maximum and the project would not add any density to the site. As further discussed in MND/IS (Attachment E) the noise and traffic generated from the construction and operation of the project would not be significant after the

implementation of identified mitigation measures. Therefore, the proposed use would be compatible with the permitted use of the site.

- x. That the proposed development will enhance the successful operation of the community or will provide a service to the community;

Discussion: The proposed Project would address the SSOs that result from the I/I during wet weather events by creating additional capacity to the Linda Mar sewer system. SSOs create a significant impact on the environment, and in particular on water quality. The Project would prevent capacity related SSOs from occurring during wet weather events. This project would also meet the requirements of the CDO and Consent Decree. Therefore the project would enhance the successful operation of the community.

- xi. That the project conforms with the setback, coverage, landscaping, and other zoning regulations of the district where a use is proposed; and

Discussion: PMC Section 9-4.102 details the development regulations applicable to the M-1/C-3 zoning district. The project site is conforming with the minimum building site area and dimension. The M-1/C-3 zoning district does not establish any setbacks or coverage maximums. The existing landscaping on the site, primarily located south of the Skatepark and west and north of the community center would meet the 10 percent landscape minimum of the zoning district. The proposed Project would not alter these areas, therefore the landscaping minimum of the site would be met. The MCC would be tallest above ground structure, which would be a 10-foot tall prefabricated building with a 4-foot high metal antenna on top, and would be below the maximum allowed height of 35 feet. Lastly, as detailed above, the project would obtain a Use Permit and Site Development Permit as the project would abut a R District and the project would include construction within a commercial district. The project would conform with the development regulations of the M-1/C-3 zoning district.

- xii. That the project is consistent with the goals and policies of the General Plan, Local Coastal Plan and with the adopted Design Guidelines.

Discussion: See discussion provided under Sections 4.A.ii and 4.A.iii of this staff report. The Project would be consistent with the City's General Plan and with the adopted Design Guidelines. The Local Coastal Plan is not applicable to the Project site.

B. In order to approve PSD-816-16, the Planning Commission shall not make any of the following findings [PMC Sec. 9-4.3204(a)]:

- i. That the location, size, and intensity of the proposed operation will create a hazardous or inconvenient vehicular or pedestrian traffic pattern, taking into account the proposed use as compared with the general character and intensity of the neighborhood;

Discussion: During construction, the Pacifica Skatepark parking lot would be closed to the public. Construction activities for the proposed pipeline along City residential streets would occur in stages in order to minimize disturbance and to maintain circulation and access through the Project area. The Project would require temporary lane closures of sections of

City residential streets during construction. Lane closures would be required during the pipeline installation in the rights-of-way along Anza Drive, Balboa Way, Arguello Boulevard, De Solo Drive, and Linda Mar Boulevard. The construction associated with the influent pipeline and two diversion structure installations would occur over approximately 14 weeks, and would impact each segment of roadway for a shorter period of time. Additionally, a section of the parking on Crespi Drive may be closed temporarily during import or export of materials by truck to the EQ basin location. This could result in disruption to commutes, or confusion by drivers taking a detour. The Contractor would be required to implement MM TRANS-1. This MM, as detailed in Attachment E, would require the preparation and implementation of Traffic Control Plan to manage traffic flow, maintain safety, and identify alternative routes when temporary changes are made to traffic or pedestrian routes.

Additionally, COA 10 would require the contractor would obtain an encroachment permit from the City for all work in the City right-of-way (street/sidewalk), public easements, or utility easements. The encroachment permit would be conditioned with measures to ensure that the construction workers, pedestrian, and motorists safety is maintained through notification and rerouting.

During operation of the Project, traffic and pedestrian patterns along the new pipeline orientation would not be affected. The Project would primarily be located underground and the above ground structures would be located away from vehicular or pedestrian facilities. Access hatches for designated City staff to enter the EQ basin would be located in areas within the reconstructed parking lot as shown in Attachment C. A minimum clearance of 10 feet by 10 feet would be required to provide access with the hatches open and for City staff to place fall protection barriers and confined space entry equipment. Therefore, the parking spaces adjacent to the hatches may become temporarily unavailable when access to the EQ basin is necessary. Barricades with "No Parking" signs (or similar device) would be posted in affected parking spaces in advance of planned entry. The northwest hatch located in the traffic flow lane of the parking lot would require an additional City staff person when the hatch is opened to manage vehicular and pedestrian traffic around the hatch.

Therefore, for all the reasons provided above, the proposed Project would not create a hazardous or inconvenient vehicular or pedestrian traffic pattern.

- ii. That the accessibility of off-street parking areas and the relation of parking areas with respect to traffic on adjacent streets will create a hazardous or inconvenient condition to adjacent or surrounding uses;

Discussion: During construction, the Skatepark parking lot would be closed to the public. As mentioned above in Section 5.A.i of this staff report, the vehicular and pedestrian traffic around the Project area would be managed through the implementation of MM TRANS-1. In addition, a section of street parking along the south side Crespi Drive and located immediately north of the Community Center would be restricted to parking by the Wheels on Wheels staff, Community Center kitchen deliveries, and Senior Citizen buses between the weekday hours of 6 a.m. to 5 p.m. throughout the construction of the EQ basin. Parking along this portion of Crespi Drive would be restored to existing conditions at the completion of the Project.

Lane closures along the pipeline alignment may temporarily block access to private driveways. COA 10 would require the contractor to obtain an encroachment permit from the City would be required for all work in the City right-of-way (street/sidewalk), public easements, or utility easements. A condition of the encroachment permit would ensure that local homeowners are notified in advance of any work that would potentially block access to and from private driveways and would require that access to private driveways be restored at the end of each day. Following construction of the new pipeline and during operation, access to off-street parking would be restored to preconstruction conditions. Therefore, the Project would not create hazardous or inconvenient conditions to the accessibility of off-street parking areas.

- iii. That insufficient landscaped areas have been reserved for the purposes of separating or screening service and storage areas from the street and adjoining building sites, breaking up large expanses of paved areas, and separating or screening parking lots from the street and adjoining building areas from paved areas to provide access from buildings to open areas;

Discussion: The Project would primarily be located underground. The aboveground components of the Project include the odor control system and the MCC building. COA 2 would require the preparation of a Landscape Plan to include the planting of species to reduce the visual appearance of the aboveground structures. The Landscaping Plan would detail the type and number of species to be planted around the perimeter of the EQ basin, the landscaping island on the south of the parking lot, as well as the bioretention basins. The species listed in the Landscaping Plan would be mostly native and drought tolerant species.

The redesigned parking lot would remove the existing planting strip in the center of the parking lot as the top of the basin would not be able to accommodate landscaping. However, two bioretention areas, and a landscaped island would be located on the south end of the redesigned parking lot. Additionally, the existing perimeter of the parking lot would be revegetated. Therefore the Project would include sufficient landscaping to break up large expansive paved areas.

- iv. That the proposed development, as set forth on the plans, will unreasonably restrict or cut out light and air on the property and on other property in the neighborhood, or will hinder or discourage the appropriate development and use of land and buildings in the neighborhood, or impair the value thereof;

Discussion: The Project components are primarily located underground. Aboveground features of the Project include the MCC building, odor control system, and the redesigned parking lot. The MCC would be tallest above ground structure, which would be a 10-foot tall prefabricated building with a 4-foot high metal antenna on top. This structure would be located approximately 14.5 feet from the east property line and 240 feet from the south property line, and would not unreasonably restrict or cut out light on neighboring property.

The Project includes an odor control system. During active operation of the EQ basin, the odor control system would circulate fresh, ambient air from intake vents located along the west side of the basin and draw it along underground piping into the EQ basin. The air from the EQ basin would then be pushed through a GAC absorber vessel on the east side of the basin to remove any associated odor from the EQ basin before being released back into the environment. Air released from the GAC absorber would be odorless and would not impact air

quality on the surrounding properties. Therefore the Project would not hinder or discourage appropriate development of adjacent properties.

- v. That the improvement of any commercial or industrial structure, as shown on the elevations as submitted, is substantially detrimental to the character or value of an adjacent R District area;

Discussion: The propose Project does not include improvements of a commercial or industrial structure; therefore, this finding does not apply.

- vi. That the proposed development will excessively damage or destroy natural features, including trees, shrubs, creeks, and rocks, and the natural grade of the site, except as provided in the subdivision regulations as set forth in Chapter 1 of Title 10 of this Code;

Discussion: The proposed Project would include removal of 10 heritage trees from the Project site to allow for the construction of the EQ basin and staging of materials and equipment during construction. As detailed in MM AES-1, the City would replant trees at a one-to-one ratio of the removed heritage trees. The proposed Project would not excessively damage or destroy any other natural features of the Project site.

- vii. That there is insufficient variety in the design of the structure and grounds to avoid monotony in the external appearance;

Discussion: The Project is primarily located underground. Above ground features of the Project include the MCC building, odor control system, and the redesigned parking lot. A prefabricated, 10-foot tall MCC building equipped with a 4-foot high metal antenna would be located along the southern border of the property. The approximately 5.5 foot high cylindrical odor control system (fan and GAC absorber) would be located on the east side of the EQ basin and would be surrounded by a 6 foot high chain linked fence. Landscaping would be used to reduce the visibility of these structures from public view. COA 2 would require the implementation of a landscaping plan to screen the new aboveground structures. Additionally, the redesigned parking lot includes the bioretention areas, the landscaping island on the south side of the parking lot and the landscaping strip bordering portions of the basin

Therefore, due to the small size of the above ground structures and the screening that would be provided by landscaping, the design of the above ground structures would not have a monotonous external appearance.

- viii. That the proposed development is inconsistent with the City's adopted Design Guidelines; or

Discussion: See discussion provided under Section 5.A.iii of this staff report. The Project would be consistent with the City's adopted Design Guidelines.

- ix. That the proposed development is inconsistent with the General Plan, Local Coastal Plan, or other applicable laws of the City.

Discussion: See discussion provided under Section 5.A.ii of this staff report. The Project would be consistent with the General Plan, and other applicable laws of the City. The Local Coastal Plan is not applicable to the Project site.

6. CEQA Recommendation

The proposed Project is not exempt from the California Environmental Quality Act (CEQA). Terraphase Engineering Inc. (Terraphase), on behalf of the City of Pacifica as the CEQA Lead Agency, prepared an Initial Study (IS) to determine whether the Project may have a significant adverse effect on the environment. Based on the results of the IS, the City determined that the potential adverse effects would be reduced to a level that is less than significant through Project revisions, therefore the City determined adoption of a Mitigated Negative Declaration (MND) would be appropriate for the Project.

Draft MND/IS

Terraphase, on the City of Pacifica's behalf, prepared a Draft MND/IS. Table 2 summarizes the level of impact the Project would have on each environmental resource considered under CEQA.

Table 2. Summary of Environmental Impacts Identified in the Draft MND/IS

Resource	Level of Impact
Aesthetics	Less than Significant Impact with Mitigation
Agricultural and Forestry Resources	No Impact
Air Quality	Less than Significant Impact with Mitigation
Biological Resources	Less than Significant Impact with Mitigation
Cultural Resources/Tribal Resources	Less than Significant Impact with Mitigation
Geology and Soils	Less than Significant Impact
Greenhouse Gas Emissions	Less than Significant Impact
Hazards and Hazardous Materials	Less than Significant Impact with Mitigation
Hydrology and Water Quality	Less than Significant Impact with Mitigation
Land Use and Planning	Less than Significant Impact
Mineral Resources	No Impact
Noise	Less than Significant Impact with Mitigation
Population and Housing	No Impact
Public Services	Less than Significant Impact with Mitigation
Recreation	Less than Significant Impact
Transportation and Circulation	Less than Significant Impact with Mitigation
Utilities and Service Systems	Less than Significant Impact

The Draft MND/IS identified that the proposed Project would have potential significant impacts on nine resources, however, with the implementation of the mitigation measures detailed in the Draft MND, it was concluded no impact would remain significant. The potential significant impacts and the MMs are fully detailed in the MND/IS (Attachment E), and are summarized below.

- **Aesthetics.** Removal of heritage trees would impact scenic resources along eligible State Scenic Highway 1 and existing visual character of the site and surroundings. MM AES-1 would require replacement of the trees as close as possible to the original tree location.
- **Air Quality.** Dust and emissions from construction equipment would exceed Bay Area Air Quality Management District (BAAQMD) significance threshold for particulate matter emissions. MM AQ-1 would require implementation of BAAQMD recommended dust control measures and

utilization of more construction equipment with greater emissions controls than typical equipment.

- **Biological Resources.** Sensitive species, sensitive natural communities, and movement of wildlife that could be present within the Project boundaries could be disturbed. MMs BIO-1 and BIO-2 would require pre-construction surveys for species by biologist, wildlife exclusion fencing to prevent access of species to work area, daily inspections by biologist, best management practices to avoid disturbance of previously undisturbed areas, etc.
- **Cultural Resources.** Previously undiscovered prehistoric, historic, paleontological, human remains, or tribal cultural resources which may be present could be disturbed. MMs CUL-1 and CUL-2 would require training of work crew on identification of cultural resources and stop work if potential cultural resources are observed, and consultation with archaeologist or paleontologist.
- **Hazards and Hazardous Materials.** Contaminated soil or groundwater could be encountered during construction, and there is the potential for accidental release of hazardous materials during construction. MM HAZ-1 would require pre-construction survey for utilities, training for workers on identifying contaminated soil, stop work if contamination is observed, and consult with an environmental consultant. MM HAZ-2 would require the implementation of best management practices for onsite hazardous materials to mitigate spill potential.
- **Hydrology and Water Quality.** Improper management of stormwater and dewatered could result in a violation of waste discharge requirements. MM HYDRO-1 would require the development of a dewatering plan, including specified elements.
- **Noise.** Construction of the Project, and operation of the ventilation system blower, could increase ambient noise or the thresholds in the City's Draft General Plan. MM NOISE-1 would require the contractor to prepare a Noise Control Plan including specified elements, and shall construct a temporary noise barrier to shield nearby receptors.
- **Public Services.** Road closures during construction could impact emergency vehicle routes. MM PUB-1 would require 48-hour notification to emergency service providers prior to road closures.
- **Transportation and Circulation.** Construction traffic would impact the level of service of local highways and roads. MM TRANS-1 would require the preparation of a Traffic Control Plan with specified elements.

Public Outreach and Comments

The Draft MND/IS was released for public review on December 8, 2016, which commenced a 37 day public review period.⁶ A Notice of Intent to Adopt a Mitigated Negative Declaration (NOI) was prepared in accordance with Section 15072(g) of the CEQA Guidelines. The NOI was provided to the State Clearinghouse, the San Mateo County Clerk on December 8, 2016. The NOI was also published on

⁶ CEQA Guideline Sections 15073(a) and 15105(b) requires at least a 30 day public review period for an MND/IS.

December 8, 2016 in the San Mateo County Times-Mercury News and mailed to owners and occupants of property within 300 feet of the proposed Project.⁷ Public outreach efforts are summarized in Table 3.

Table 3. Summary of Public Outreach Efforts

Media	Information Provided	Date(s) Published
California State Clearinghouse	NOI to Adopt a MND/IS, MND/IS Document	December 8, 2016
San Mateo County Times- Mercury News	NOI to Adopt a MND/IS	December 8, 2016
San Mateo County Clerk	NOI to Adopt a MND/IS	December 8, 2016
Owners/Occupants within 300 feet of Project.	NOI to Adopt a MND/IS	December 8, 2016
City of Pacifica Website	MND/IS Document, Notification of City Council Meeting, CEQA Public Meeting, Public Review Period	December 8, 2016
Connect with Pacifica (E-newsletter)	Notification of City Council Meeting, CEQA Public Meeting, Public Review Period	December 2, 2016 December 9, 2016 December 16, 2016 December 22, 2016
NextDoor	Notification of CEQA Public Meeting, Public Review Period	December 13, 2016
On site Posting	Public Review Period	December 29, 2016 through January 13, 2017
Notes: NOI – Notice of Intent CEQA – California Environmental Quality Act MND/IS –Mitigated Negative Declaration/Initial Study		

Staff hosted a public meeting for the CEQA document at the Community Center on December 15, 2016. Notification for the meeting was included in the NOI and was advertised as noted in Table 3. Approximately 12 members of the public attended the meeting. Staff noted that attendees mainly had questions to better understand the purpose, need, and description of the Project. Staff noted a verbal comment made on the document, which was regarding concerns of impacts on traffic during drop off and pickup of students at the nearby Cabrillo Elementary School during construction.

During the public comment period, a total of 5 comment letters were received. Table 4 provides a list of the commenters and a summary of the comment topics. The written comments and the verbal comment noted above were addressed in the Final MND/IS.

⁷ CEQA Guideline Section 15072(b) requires only one of these notification methods.

Table 4. Commenters on Draft MND/IS and Summary of Comment Topics

Commenter	Summary of Comment Topics
California Department of Fish and Wildlife, Bay Delta Region	San Francisco Garter Snake Mitigation Measure (BIO-1), Filing Fees
California Department of Transportation, District 4	Transportation and Encroachment Permits, Cultural Resources
Mike O’Connell	Sea level rise
Lorie Bowie	Flooding, sanitary sewer equipment status
Eamon Murphy	Soil off haul

Final MND/IS and Mitigation Monitoring and Reporting Program

The Final MND/IS includes written public comment letters on the Draft MND/IS (Table 4) and presents responses to the written public comments; and as necessary makes corrections and clarifications to the Draft MND/IS. The Final MND/IS document, together with the Draft MND/IS constitute the Final MND/IS for the Project (Attachment E). Public Resources Code Section 21081.6(a) requires the Lead Agency to adopt a Mitigation Monitoring and Reporting Program (MMRP) for projects subject to the preparation of MNDs. The MMRP for the Project is included as part of the Final MND in Attachment E.

The edits included in the Final MND as a result of the comments received provide clarification of information and do not constitute a “substantial revision” as defined under Section 15073.5(b) of the CEQA Guidelines. None of the circumstances that would require recirculation of the Draft MND/IS, as detailed in Section 15073.5(c), apply to the document. Information presented in Final MND support the City’s determination that recirculation of the Draft MND/IS is not required and the impacts of the project would be less than significant with the implementation of the identified mitigation. The City has prepared this document pursuant to Sections 15070 through 15074 of the CEQA Guidelines which address the preparation of MNDs.

7. Staff Analysis

Staff has carefully evaluated various project types and locations to address the serious issue that is SSOs in the Linda Mar area while balancing the cost and timing of implementation of the alternatives. Based on the results of the evaluation, staff finds that the proposed Project would best address the SSOs while minimizing potential environmental impacts of the Project itself as well as the known environmental impacts of SSOs. While the construction of the Project may create short-term inconveniences to the visitors to the Community Center and Skatepark, residences located adjacent to the pipeline route, and motorists traveling along Crespi Drive, the long-term benefit of the Project to the Linda Mar area and to the City would quickly overshadow these minor disruptions. The Project, as conditioned, would not create a significant impact on the environment and is consistent with the General Plan, the City’s adopted Design Guidelines, and other local regulations.

COMMISSION ACTION

MOTION FOR APPROVAL:

Move that the Planning Commission certify and adopt the Final Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program, pursuant to the California Environmental Quality Act and **APPROVE** Use Permit UP-080-16 and Site Development Permit PSD-816-16, by adopting the resolution

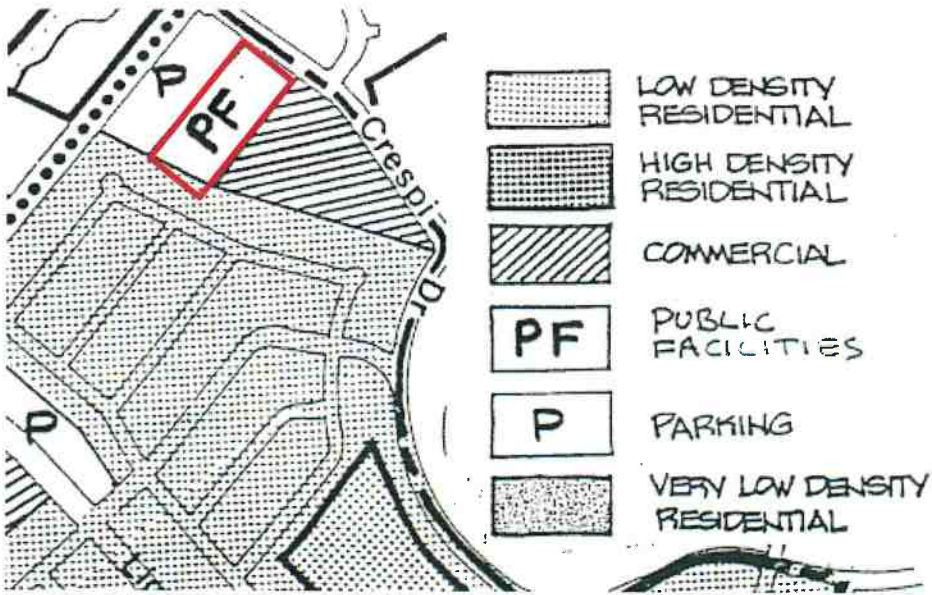
included as Attachment B to the staff report, including conditions of approval in Exhibit A to the resolution; and, incorporate all maps and testimony into the record by reference.

ATTACHMENTS:

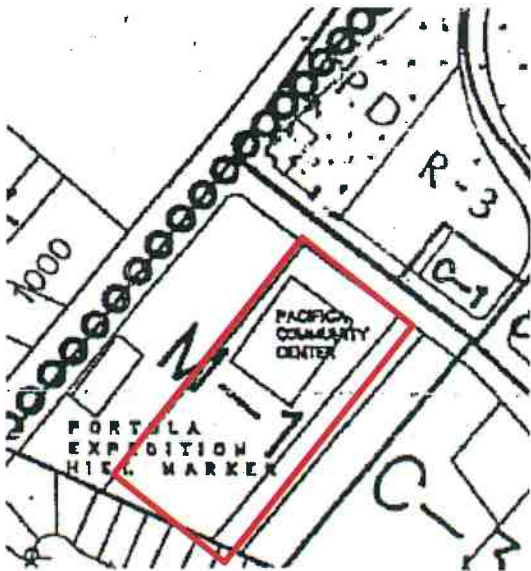
- A. Land Use and Zoning Exhibit
- B. Resolution of Approval and Conditions of Approval (Exhibit A)
- C. Selected Sheets of 100 percent Building Plans
- D. Figure of Alternative Locations Considered for the EQ Basin
- E. City of Pacifica Wet Weather Equalization Basin Project Final Mitigated Negative Declaration/Initial Study (The Draft MND/IS was provided to Planning Commissioners in December 2016 and is available online at http://www.cityofpacifica.org/depts/planning/environmental_documents/default.asp)
- F. Comment Letters Received After the Public Comment Period

Land Use and Zoning Exhibit

General Plan Land Use Designation



Zoning District



RESOLUTION NO. _____

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PACIFICA APPROVING USE PERMIT UP-080-16 AND SITE DEVELOPMENT PERMIT PSD-816-16, SUBJECT TO CONDITIONS, FOR CONSTRUCTION OF A WET WEATHER EQUALIZATION BASIN AT 540 CRESPI DRIVE (APN 022-162-420) AND CERTIFYING THE MITIGATED NEGATIVE DECLARATION AND ADOPTING THE MITIGATION MONITORING AND REPORTING PROGRAM IN ACCORDANCE WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA).

Initiated by: City of Pacifica (“Applicant”).

WHEREAS, an application has been submitted to construct a 2.1-million-gallon capacity equalization basin, a 10-foot tall motor control center building, ventilation and odor-control system, and a cleaning system within the equalization basin (Project) at 540 Crespi Drive in Pacifica (APN 022-162-420); and

WHEREAS, the Project would also include construction of two diversion structures to passively divert excess flows from the existing Linda Mar and Arguello sanitary sewer lines and transport the flow via a conveyance pipeline to the equalization basin during storm events and an effluent conveyance pipeline routing flows to the existing Crespi Drive sanitary sewer line and Linda Mar Blvd pump; and

WHEREAS, the Planning Commission considered the Project and application at its regular meeting of February 6, 2017. Prior to taking action on the application, the Planning Commission received written and oral reports by the staff, and received public testimony; and

WHEREAS, the Project site would be located in a zoning district where public utility facilities are not expressly permitted and a use permit is necessary per PMC Section 9-4.2303; and

WHEREAS, the Project site is adjacent to an R-1 (Single Family Residential District) zoning district and a use permit is necessary per PMC Section 9-4.1202(i) ; and

WHEREAS, wastewater treatment and reclamation facilities can be accommodated in any zoning district if it meets the special use permit review criteria detailed in PMC Section 9-4.2306(d); and

WHEREAS, new construction in a commercial district requires a Site Development Permit per PMC Section 9-4.3201(a); and

WHEREAS, City of Pacifica Planning Division is the Lead Agency for preparing the environmental review for the Project pursuant to the California Environmental Quality Act (CEQA) and for project approval,

ATTACHMENT B

WHEREAS, Terraphase Engineering Inc., on behalf of the City of Pacifica Planning Division prepared the *City of Pacifica Wet Weather Equalization Basin Project Draft Mitigated Negative Declaration/ Initial Study* (MND/IS) for the Project in accordance with Public Resources Code Section 21000 *et seq.* and CEQA Guidelines Section 15000 *et seq.*; and

WHEREAS, the Initial Study/Mitigated Negative Declaration concluded that implementation of the Project could result in a number of significant effects on the environment and identified mitigation measures that would reduce the significant effects to a less-than-significant level; and

WHEREAS, in connection with the approval of a project involving the preparation of an initial study/mitigated negative declaration that identifies one or more significant environmental effects, CEQA requires the decision-making body of the lead agency to incorporate feasible mitigation measures that would reduce those significant environment effects to a less-than-significant level; and

WHEREAS, whenever a lead agency approves a project requiring the implementation of measures to mitigate or avoid significant effects on the environment, CEQA also requires a lead agency to adopt a Mitigation Monitoring and Reporting Program to ensure compliance with the mitigation measures during project implementation; and

WHEREAS, on December 8, 2016 the City of Pacifica Planning Division issued a Notice of Intent to Adopt a MND/IS for the Project, which was distributed in compliance with CEQA Guidelines Section 15072 and Public Resources Code Section 21092; and

WHEREAS, on December 8, 2016, the City of Pacifica Planning Division distributed copies of the Draft MND/IS to public agencies which have jurisdiction by law with respect to the Project and to publically accessible repositories and invited comments on the Draft MND/IS in compliance with CEQA Guidelines Section 15072; and

WHEREAS, on December 15, 2016, City of Pacifica Planning Division noticed and held a public meeting to present the Project and the conclusions of the analysis in the Draft MND/IS; and

WHEREAS, on January 13, 2017, the 37-day public comment period for the Draft MND ended;

WHEREAS, written comments on the Draft MND/IS were collected and responses to comments were considered in the revisions made to the Draft MND/IS to comprise the Final MND/IS; and

WHEREAS, the Final MND/IS identified certain potentially significant adverse environmental impacts and recommends certain mitigation measures regarding such effects; and

WHEREAS, there is no substantial evidence that the project would have significant effects on the environment after implementation of identified mitigation measures; and

WHEREAS, the Initial Study/Mitigated Negative Declaration and the Mitigation Monitoring and Reporting Program for the Project are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, the Project will not individually or cumulatively have an adverse effect on wildlife resources, as defined in Section 711.2 of the California Department of Fish and Game Code.

WHEREAS, the Planning Commission of the City of Pacifica did hold a duly noticed public hearing on February 6, 2017, at which time it considered all oral and documentary evidence presented, and incorporated all testimony and documents into the record by reference.

NOW, THEREFORE BE IT RESOLVED by the Planning Commission of the City of Pacifica as follows:

1. The above recitals are true and correct and material to this Resolution.
2. In making its findings, the Planning Commission relied upon and hereby incorporates by reference all correspondence, staff reports, and other related materials.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Pacifica does hereby make the following findings pertaining to the Use Permit:

- a) That the establishment, maintenance, or operation of the use or building applied for will not, under the circumstances of the particular case, be detrimental to the health, safety, and welfare of the persons residing or working in the neighborhood or to the general welfare of the City;

Discussion: The MND/IS prepared for the proposed Project concludes that the construction, operation and maintenance of the Project would not have a significant impact on the environment with the implementation of the incorporated mitigation measures (MMs). The Project would meet all applicable building code and engineering requirements and would comply with all applicable regulatory requirements.

- b) That the use or building applied for is consistent with the applicable provisions of the General Plan and other applicable laws of the City and, where applicable, the local Coastal Plan; and

Discussion: The proposed use is consistent with the General Plan and the local Coastal Plan is not applicable to the Project site. The issue of I/I into the sanitary sewer system

during wet weather events and the need to improve the sanitary sewer system capacity is mentioned in multiple locations within the General Plan, as noted below.

Conservation Element

Inflow and infiltration into the sewage collection system, particularly in the Linda Mar area, results in overflows during long periods of wet weather. The exact locations of this problem are unknown and difficult to assess. The magnitude of the problem and the cost of correction are estimated to be great. The City should seek assistance to investigate and reduce this problem. (Page 114)

Action Programs, Short Term, 1: Seek outside assistance to study and correct the infiltration problem in Linda Mar sewage collection system. (Page 16)

Community Facilities Element

The Regional Board is presently concerned about compliance during wet weather conditions. Reduction of the peak wet weather flows through an inflow and infiltration reduction program and modification to the treatment plant may be required within the next five years. (Page 96)

Policy 1: Maintain and improve the present level of City services. (Page 21)

The City is separately working on directly addressing the I/I issue by replacing sanitary sewer piping as funding will allow. However, the cost and work associated with correcting the I/I in the Linda Mar area is significant and is a long-term goal. Meanwhile, the Linda Mar area experiences SSOs as a result of the I/I. The proposed Project would address the SSOs that result from the I/I during wet weather events by creating additional capacity to the Linda Mar sanitary sewer collection system. SSOs create a significant impact on the environment, and in particular on water quality. The Project would prevent capacity related SSOs from occurring during wet weather events. The objective of the proposed Project would be consistent with the General Plan as the proposed Project would address the results of the I/I issue discussed in the General Plan.

Additionally, construction of the Project is consistent with other resources considered in the General Plan as further discussed below:

Noise Element

Action Programs, Short Term, 5: The noise impact on land uses should be considered when development plans are reviewed and approved. Where existing ambient noise levels are high, or where the proposed use will create additional noise, the builder should be required to mitigate the noise. (Page 18)

Discussion: The MND/IS analyzed the impact of the proposed Project on the existing ambient noise levels (See Attachment E and Section 5 of this staff report). It was concluded that, with the incorporation of MM NOISE-1, impacts on noise levels for surrounding receptors would be less than significant during construction and operation of the Project.

Seismic Safety and Safety Element

Policy 1. Prohibit development in hazardous areas, including flood zones, unless detailed site investigations ensure that risks can be reduced to acceptable levels and the structure will be protected for its design life. Development shall be design to withstand a minimum of a 100 year hazard event regardless of the specific nature of the hazard. (Page 110)

Discussion: The southern part of the Community Center property, including the existing Skatepark parking lot, and the influent pipeline alignment are located within the Special Flood Hazard Zone AH, which is a Federal Emergency Management Agency (FEMA) 100-year flood plain¹. Proposed site improvements including site grading and construction of the EQ basin, the Motor Control Center (MCC) Building, and the odor control system (fan and granular activated carbon [GAC] absorber) are not anticipated to result in additional displacement of flood flows associated with the 100-year event as the overall grade following the installation of the EQ basin would be similar to the existing grade. The elevation at the northern portion of the reconstructed parking lot would be slightly higher than the existing grade, and would slope to the south where the elevation would be slightly lower than existing grade. In addition, the proposed bioretention areas would increase the pervious area of the site, thereby allowing for greater infiltration during storm events. The existing street elevations along the new pipelines would not be changed from existing conditions.

The concrete slab for the MCC building and the odor control system would be constructed above the FEMA 100-year flood elevation of 14 feet above mean sea level².

In addition, the Project would be consistent with the Pacifica Municipal Code, California Building Code, and other applicable regulations.

- c) Where applicable, that the use or building applied for is consistent with the City's adopted Design Guidelines.

¹ Federal Emergency Management Agency. 2017. FEMA National Flood Hazard Layer. Accessed January 4, 2017.

² *Ibid.*

Discussion: The Project components are primarily located underground. Aboveground features of the Project include the MCC building, odor control system including ambient air intakes, and the redesigned parking lot. These features are consistent with the City's adopted Design Guidelines as further discussed below.

***Building Design, Screening.** All exposed mechanical and electrical equipment must be screened from public view. The design of such screening should be integral part of the building design. (Page 5)*

Discussion: A prefabricated, 10-foot tall MCC building equipped with a 4-foot high metal antenna that would allow radio communication with the City's Calera Waste Water Treatment Plant and an odor control system would be located along the northeastern portion of the Skatepark parking lot area. The MCC building would contain four motor starters, one for each of the 10 horsepower (hp) duty pumps and one for each of the 2 hp dewatering pumps, a panel board, a telemetry panel, and a transformer to step down voltages to the ancillary electrical facilities. Two at-grade ambient air inlets would be located on the west side of the EQ basin. The odor control system would appear as an approximate 5.5 foot high cylindrical structure, surrounded by a 6 foot high chain linked fence, located next to the MCC building. Landscaping would be used to reduce the visibility of these structures from public view. Condition of Approval (COA) 2 would require the implementation of a landscaping plan to screen the new aboveground structures.

Electrical, water, and other utility connections would be provided to the Project through underground infrastructure.

***Landscaping, Parking areas.** All parking areas should be landscaped with fast growing trees and/or shrubs in order to screen vehicles from view and minimize the visual impact of expansive areas of asphalt. Such landscaping should not obscure views necessary for traffic safety. (Page 6)*

Discussion: The effected parking lot is located behind the existing Community Center building and Skatepark and not significantly visible from Crespi Drive or Highway 1. In addition to MM AES-1, which would require the replanting of removed heritage trees at the completion of construction, COA 2 would require the implementation of a landscaping plan in the bioretention areas, the landscaping island on the south side of the parking lot and the landscaping strip bordering the west, south, and north portions of the EQ basin to be planted with trees and shrubs to screen the new aboveground structures.

***Landscaping, Screening.** Dense landscaping should be used to screen unattractive features such as storage areas, trash enclosures, transformers and*

generators, vacuum breakers, and elements which do not contribute to the visual enhancement of the surroundings. (Page 6)

Discussion: See discussions under Design Guidelines: *Building Design, Screening and Landscaping, Parking areas* provided above.

Water Conservation Policy and Landscape Design Guidelines for New Development,

Irrigation 1. *All parks, median strips, landscaped public areas, landscaped areas surrounding residential and commercial developments and industrial parks shall have separately metered automatic irrigation systems designed by a landscape architect or other competent person. (Page 8)*

Discussion: COA 2 would require the implementation of a landscaping plan. The plan would detail the irrigation needs of the property. However, the landscaping plan would include use of drought tolerant and mostly native vegetation; therefore, irrigation would likely only be required during the establishment of the vegetation and would not require long-term irrigation.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Pacifica does hereby determine that the Project meets the following special use permit criteria:

- a) That the proposed use will be of such size, design, and operating characteristics as will tend to keep it compatible with permitted uses in the district under consideration with respect to bulk, scale, coverage, density, noise, and generation of traffic;

Discussion: The Project components are primarily located underground. Aboveground components of the Project include the MCC building, odor control system, and the redesigned parking lot. The MCC would be tallest above ground structure, which would be a 10-foot tall prefabricated building with a 4-foot high metal antenna on top. The aboveground components of the Project would not be out of scale or bulk with the M-1/C-3 zoning district. The M-1/C-3 zoning district does not have a coverage maximum and the project would not add any density to the site. As further discussed in MND/IS (Attachment E) the noise and traffic generated from the construction and operation of the project would not be significant after the implementation of identified mitigation measures. Therefore, the proposed use would be compatible with the permitted use of the site.

- b) That the proposed development will enhance the successful operation of the community or will provide a service to the community;

Discussion: The proposed Project would address the SSOs that result from the I/I during wet weather events by creating additional capacity to the Linda Mar sewer system. SSOs create a significant impact on the environment, and in particular on water

quality. The Project would prevent capacity related SSOs from occurring during wet weather events. This project would also meet the requirements of the CDO and Consent Decree. Therefore the project would enhance the successful operation of the community.

- c) That the project conforms with the setback, coverage, landscaping, and other zoning regulations of the district where a use is proposed; and

Discussion: PMC Section 9-4.102 details the development regulations applicable to the M-1/C-3 zoning district. The project site is conforming with the minimum building site area and dimension. The M-1/C-3 zoning district does not establish any setbacks or coverage maximums. The existing landscaping on the site, primarily located south of the Skatepark and west and north of the community center would meet the 10 percent landscape minimum of the zoning district. The proposed Project would not alter these areas, therefore the landscaping minimum of the site would be met. The MCC would be tallest above ground structure, which would be a 10-foot tall prefabricated building with a 4-foot high metal antenna on top, and would be below the maximum allowed height of 35 feet. Lastly, as detailed above, the project would obtain a Use Permit and Site Development Permit as the project would abut a R District and the project would include construction within a commercial district. The project would conform with the development regulations of the M-1/C-3 zoning district.

- d) That the project is consistent with the goals and policies of the General Plan, Local Coastal Plan and with the adopted Design Guidelines.

Discussion: The Project would be consistent with the City's General Plan and with the adopted Design Guidelines. The Local Coastal Plan is not applicable to the Project site.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Pacifica does not hereby make the following findings pertaining to the Site Development Permit:

- a) That the location, size, and intensity of the proposed operation will create a hazardous or inconvenient vehicular or pedestrian traffic pattern, taking into account the proposed use as compared with the general character and intensity of the neighborhood;

Discussion: During construction, the Pacifica Skatepark parking lot would be closed to the public. Construction activities for the proposed pipeline along City residential streets would occur in stages in order to minimize disturbance and to maintain circulation and access through the Project area. The Project would require temporary lane closures of sections of City residential streets during construction. Lane closures would be required during the pipeline installation in the rights-of-way along Anza Drive, Balboa Way, Arguello Boulevard, De Solo Drive, and Linda Mar Boulevard. The construction associated with the influent pipeline and two diversion structure installations would occur over approximately 14 weeks, and would impact each segment of roadway for a shorter period of time. Additionally, a section of the parking

on Crespi Drive may be closed temporarily during import or export of materials by truck to the EQ basin location. This could result in disruption to commutes, or confusion by drivers taking a detour. The Contractor would be required to implement MM TRANS-1. This MM, as detailed in Attachment E, would require the preparation and implementation of Traffic Control Plan to manage traffic flow, maintain safety, and identify alternative routes when temporary changes are made to traffic or pedestrian routes.

Additionally, COA 10 would require the contractor would obtain an encroachment permit from the City for all work in the City right-of-way (street/sidewalk), public easements, or utility easements. The encroachment permit would be conditioned with measures to ensure that the construction workers, pedestrian, and motorists safety is maintained through notification and rerouting.

During operation of the Project, traffic and pedestrian patterns along the new pipeline orientation would not be affected. The Project would primarily be located underground and the above ground structures would be located away from vehicular or pedestrian facilities. Access hatches for designated City staff to enter the EQ basin would be located in areas within the reconstructed parking lot as shown in Attachment C. A minimum clearance of 10 feet by 10 feet would be required to provide access with the hatches open and for City staff to place fall protection barriers and confined space entry equipment. Therefore, the parking spaces adjacent to the hatches may become temporarily unavailable when access to the EQ basin is necessary. Barricades with "No Parking" signs (or similar device) would be posted in affected parking spaces in advance of planned entry. The northwest hatch located in the traffic flow lane of the parking lot would require an additional City staff person when the hatch is opened to manage vehicular and pedestrian traffic around the hatch.

Therefore, for all the reasons provided above, the proposed Project would not create a hazardous or inconvenient vehicular or pedestrian traffic pattern.

- b) That the accessibility of off-street parking areas and the relation of parking areas with respect to traffic on adjacent streets will create a hazardous or inconvenient condition to adjacent or surrounding uses;

Discussion: During construction, the Skatepark parking lot would be closed to the public. As mentioned above in Section 5.A.i of this staff report, the vehicular and pedestrian traffic around the Project area would be managed through the implementation of MM TRANS-1. In addition, a section of street parking along the south side Crespi Drive and located immediately north of the Community Center would be restricted to parking by the Wheels on Wheels staff, Community Center kitchen deliveries, and Senior Citizen buses between the weekday hours of 6 a.m. to 5 p.m. throughout the construction of the EQ basin. Parking along this portion of Crespi Drive would be restored to existing conditions at the completion of the Project.

Lane closures along the pipeline alignment may temporarily block access to private driveways. COA 10 would require the contractor to obtain an encroachment permit from the City would be required for all work in the City right-of-way (street/sidewalk), public easements, or utility easements. A condition of the encroachment permit would ensure that local homeowners are notified in advance of any work that would potentially block access to and from private driveways and would require that access to private driveways be restored at the end of each day. Following construction of the new pipeline and during operation, access to off-street parking would be restored to preconstruction conditions. Therefore, the Project would not create hazardous or inconvenient conditions to the accessibility of off-street parking areas.

- c) That insufficient landscaped areas have been reserved for the purposes of separating or screening service and storage areas from the street and adjoining building sites, breaking up large expanses of paved areas, and separating or screening parking lots from the street and adjoining building areas from paved areas to provide access from buildings to open areas;

Discussion: The Project would primarily be located underground. The aboveground components of the Project include the odor control system and the MCC building. COA 2 would require the preparation of a Landscape Plan to include the planting of species to reduce the visual appearance of the aboveground structures. The Landscaping Plan would detail the type and number of species to be planted around the perimeter of the EQ basin, the landscaping island on the south of the parking lot, as well as the bioretention basins. The species listed in the Landscaping Plan would be mostly native and drought tolerant species.

The redesigned parking lot would remove the existing planting strip in the center of the parking lot as the top of the basin would not be able to accommodate landscaping. However, two bioretention areas, and a landscaped island would be located on the south end of the redesigned parking lot. Additionally, the existing perimeter of the parking lot would be revegetated. Therefore the Project would include sufficient landscaping to break up large expansive paved areas.

- d) That the proposed development, as set forth on the plans, will unreasonably restrict or cut out light and air on the property and on other property in the neighborhood, or will hinder or discourage the appropriate development and use of land and buildings in the neighborhood, or impair the value thereof;

Discussion: The Project components are primarily located underground. Aboveground features of the Project include the MCC building, odor control system, and the redesigned parking lot. The MCC would be tallest above ground structure, which would be a 10-foot tall prefabricated building with a 4-foot high metal antenna on top. This structure would be located approximately 14.5 feet from the east property line and 240

feet from the south property line, and would not unreasonably restrict or cut out light on neighboring property.

The Project includes an odor control system. During active operation of the EQ basin, the odor control system would circulate fresh, ambient air from intake vents located along the west side of the basin and draw it along underground piping into the EQ basin. The air from the EQ basin would then be pushed through a GAC absorber vessel on the east side of the basin to remove any associated odor from the EQ basin before being released back into the environment. Air released from the GAC absorber would be odorless and would not impact air quality on the surrounding properties. Therefore the Project would not hinder or discourage appropriate development of adjacent properties.

- e) That the improvement of any commercial or industrial structure, as shown on the elevations as submitted, is substantially detrimental to the character or value of an adjacent R District area;

Discussion: The proposed Project does not include improvements of a commercial or industrial structure; therefore, this finding does not apply.

- f) That the proposed development will excessively damage or destroy natural features, including trees, shrubs, creeks, and rocks, and the natural grade of the site, except as provided in the subdivision regulations as set forth in Chapter 1 of Title 10 of this Code;

Discussion: The proposed Project would include removal of 10 heritage trees from the Project site to allow for the construction of the EQ basin and staging of materials and equipment during construction. As detailed in MM AES-1, the City would replant trees at a one-to-one ratio of the removed heritage trees. The proposed Project would not excessively damage or destroy any other natural features of the Project site.

- g) That there is insufficient variety in the design of the structure and grounds to avoid monotony in the external appearance;

Discussion: The Project is primarily located underground. Above ground features of the Project include the MCC building, odor control system, and the redesigned parking lot. A prefabricated, 10-foot tall MCC building equipped with a 4-foot high metal antenna would be located along the southern border of the property. The approximately 5.5 foot high cylindrical odor control system (fan and GAC absorber) would be located on the east side of the EQ basin and would be surrounded by a 6 foot high chain linked fence. Landscaping would be used to reduce the visibility of these structures from public view. COA 2 would require the implementation of a landscaping plan to screen the new aboveground structures. Additionally, the redesigned parking lot includes the bioretention areas, the landscaping island on the south side of the parking lot and the landscaping strip bordering portions of the basin

Therefore, due to the small size of the above ground structures and the screening that would be provided by landscaping, the design of the above ground structures would not have a monotonous external appearance.

- h) That the proposed development is inconsistent with the City's adopted Design Guidelines;
or

Discussion: The Project would be consistent with the City's adopted Design Guidelines.

- i) That the proposed development is inconsistent with the General Plan, Local Coastal Plan, or other applicable laws of the City.

Discussion: The Project would be consistent with the General Plan, and other applicable laws of the City. The Local Coastal Plan is not applicable to the Project site.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Pacifica does hereby make the following **CEQA FINDINGS**:

- 1) The Planning Commission has independently reviewed and analyzed the Initial Study/Mitigated Negative Declaration and other information in the record and has considered the information contained therein, prior to acting upon or approving the Project,
- 2) The Initial Study/Mitigated Negative Declaration prepared for the Project has been completed in compliance with CEQA and consistent with state and local guidelines implementing CEQA,
- 3) On the basis of the whole record, there is no substantial evidence that the Project as designed, conditioned, and mitigated, will have a significant effect on the environment
- 4) The Initial Study/Mitigated Negative Declaration represents the independent judgment and analysis of the City as lead agency for the Project.
- 5) The Planning Commission hereby adopts the Mitigation Monitoring and Reporting Program prepared for the Project.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Pacifica does hereby adopt the *Wet Weather Equalization Basin Project* Final MND/IS and MMRP and approve Use Permit UP-080-16 and Site Development Permit PSD-816-16 for construction of the a 2.1-million-gallon capacity equalization basin and associated components as detailed above at 540 Crespi Drive (APN 022-162-420), subject to conditions of approval included as Exhibit A to this resolution.

Use Permit UP-080-16 and PSD-816-16
540 Crespi Drive (APN 022-162-420)
February 6, 2017
Page 13

* * * * *

Passed and adopted at a regular meeting of the Planning Commission of the City of Pacifica, California, held on the 6th day of February 2017.

AYES, Commissioner:

NOES, Commissioner:

ABSENT, Commissioner:

ABSTAIN, Commissioner:

Josh Gordon, Chair

ATTEST:

APPROVED AS TO FORM:

Tina Wehrmeister, Planning Director

Michelle Kenyon, City Attorney

Exhibit A

Conditions of Approval: Use Permit UP-080-16 and Site Development Permit PSD-816-16 to construct a wet weather equalization basin at 540 Crespi Drive (APN 022-162-420)

Planning Commission Meeting February 6, 2017

Planning Division

1. Development shall be substantially in accord with the plans entitled "*City of Pacifica, Department of Public Works, Waste Water Division, 100% Submittal Wet Weather Equalization Basin Project*" received by the City of Pacifica on January 18, 2017, except as modified by the following conditions.
2. A landscaping plan shall be prepared to detail the number and type of species that will be planted and location of plantings in the bioretention basins, the landscaping island on the south side of the parking lot, the landscaping strip the partially surrounds the southwest. Additionally, the landscaping plan shall detail the revegetation of the north side of the parcel that will be temporarily used for staging of equipment and materials during construction. Location and species type of replacement heritage trees, as required under MM AES-1 in the *City of Pacifica Wet Weather Equalization Basin Project Final Mitigated Negative Declaration/ Initial Study* (February 2017) for the project shall also be detailed. Landscaping shall include the drought tolerant and mostly native species as to minimize the need for irrigation after the plants are established. Additionally, plants shall be used to reduce the visibility of the motor control center building and the odor control system on the northeast side of the equalization basin.
3. Mitigation measures included in the *City of Pacifica Wet Weather Equalization Basin Project Mitigation Monitoring and Reporting Project* (February 2017) shall be incorporated into the project as detailed.
4. That the approval is valid for a period of one year from the date of final determination. If the use or uses approved is/are not established within such period of time, the approval shall expire unless Applicant submits a written request for an extension and applicable fee prior to the expiration date, and the Planning Director or Planning Commission approves the extension request as provided below. The Planning Director may administratively grant a single, one year extension provided, in the Planning Director's sole discretion, the circumstances considered during the initial project approval have not materially changed. Otherwise, the Planning Commission shall consider a request for a single, one year extension.

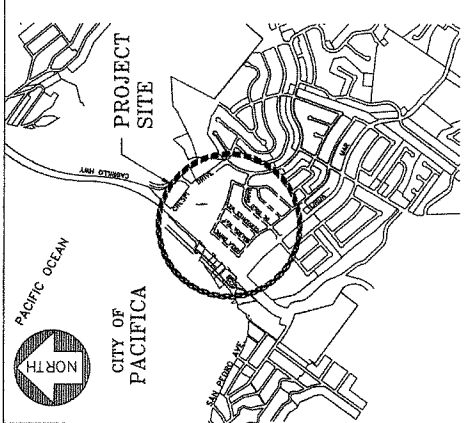
5. The Applicant shall incorporate all mitigation measures, as detailed, in the *Final Mitigated Negative Declaration* (February 2017).
6. The property owner shall continue to monitor and maintain the site after the EQ basin and associated equipment and pipelines are constructed. This includes cleaning the interior of the EQ basin after each use, periodic landscape maintenance, fence maintenance, removal of dumping or litter, and maintenance of drainage and storm water facilities.

Engineering Division of Public Works

7. Construction shall be in conformance with the San Mateo Countywide Storm Water Pollution Prevention Program. Best Management Practices (BMPs) such as straw mulch, silt fences, sediment basins or traps and/or other measures shall be employed during construction to control erosion/siltation. The project will comply with current State and Regional Water Quality Control Board permit requirements and the Municipal Regional Stormwater NPDES Permit (MRP) requirements and standards.
8. Roadways shall be maintained clear of construction equipment, materials and debris, especially mud and dirt tracked onto Crespi Drive. Dust control and daily road cleanup will be strictly enforced.
9. Existing curb, sidewalk or other street improvements adjacent to the property frontage that is damaged or displaced shall be repaired or replaced as deemed by the City Engineer even if damage or displacement occurred prior to any work performed for this project.
10. Encroachment Permit shall be obtained for work within City right-of-way, utility easements, and public easements. Permits shall be conditioned to require proper notification to affected motorists and residents.

Building Division

11. The project requires review and approval of a building permit by the Building Official. Applicant shall apply for and receive approval of a building permit prior to commencing any construction activity.



CITY OF PACIFICA

DEPARTMENT OF PUBLIC WORKS

WASTE WATER DIVISION

100% SUBMITTAL

WET WEATHER EQUALIZATION BASIN PROJECT

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C-02	SYMBOLS AND ABBREVIATIONS	E-103	GENERAL NOTES	M-02	ODOR CONTROL SYSTEM SECTIONS
C-20	PIPELINE PLAN & PROFILE - ANZA DR./BALBOA WAY	E-104	ABBREVIATIONS	M-03	ODOR CONTROL DETAILS
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C-22	PIPELINE PLAN & PROFILE - ARGUELLO BLVD.	E-301	TEMPORARY STAGE SLURRY WALL PANEL/BRACING LAYOUT PLAN		
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C-60	UTILITY PLAN - PARKING LOT	E-501	EAST AND WEST LINER WALL REINFORCEMENT		
C-61	UTILITY PLAN - COMMUNITY CENTER DRIVEWAY	E-502	NORTH AND SOUTH LINER WALL REINFORCEMENT		
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C-71	DETAIL SHEET	E-504	TYPICAL PRECAST CONCRETE DETAILS		
C-72	DETAIL SHEET	E-505	TYPICAL MICROPILE DETAILS		
C-73	DETAIL SHEET	E-506	ELECTRICAL LEGEND AND SYMBOLS		
C-74	DETAIL SHEET	E-507	ELECTRICAL SITE PLAN		
C-75	DETAIL SHEET		ELECTRICAL LEGEND AND SYMBOLS		
C-76	DETAIL SHEET		ELECTRICAL SITE PLAN		
C-77	DETAIL SHEET		MAIN SINGLE LINE DIAGRAM		
C-78	DETAIL SHEET		PANELBOARD SCHEDULE		
C-80	BASIN PIPING DIAGRAM		CIRCUIT SCHEDULE		
C-81	BASIN PIPING DIAGRAM		CONTROL SCHEMATICS (SHEET 1 OF 4)		
C-90	BASIN PIPING DETAILS				

SCALE: AS NOTED DESIGNED: RHL 06/06/18 BASIS OF DESIGN DRAWN: AFR 06/06/18 100% SUBMITTAL CHECKED: MQ 06/06/18 100% SUBMITTAL PROJ. ENGR: RHL 06/06/18 100% SUBMITTAL	CITY OF PACIFICA DEPARTMENT OF PUBLIC WORKS WASTE WATER DIVISION	CITY OF PACIFICA DEPARTMENT OF PUBLIC WORKS WASTE WATER DIVISION	CITY OF PACIFICA DEPARTMENT OF PUBLIC WORKS WASTE WATER DIVISION
EX DATE: _____ DESCRIPTION OF REVISIONS: _____ APP'D: _____	CITY OF PACIFICA DEPARTMENT OF PUBLIC WORKS WASTE WATER DIVISION	CITY OF PACIFICA DEPARTMENT OF PUBLIC WORKS WASTE WATER DIVISION	CITY OF PACIFICA DEPARTMENT OF PUBLIC WORKS WASTE WATER DIVISION
DIRECTOR OF PUBLIC WORKS VAN OCAMPO, P.E.	DEPUTY DIRECTOR LOUIS SUN, P.E.	ASSISTANT SUPERINTENDENT MARIA AGUILAR, P.E.	SHEET C-01 JOB NO. 24-0001

SHEET NO. MECHANICAL
 M-01
 M-02
 M-03

SHEET CONTENT
 ODOR CONTROL SYSTEM PLAN
 ODOR CONTROL SYSTEM SECTIONS
 ODOR CONTROL DETAILS

NOTES
 1. SURVEY CONTROL ESTABLISHED USING THE U.S. NATIONAL GEODETIC SURVEY DATA SHEET FOR BENCH MARK DISK LOCATED N 37°35'41.1" W 122°30'18.1" NAD83 (1986) AND NAVD 88 VERTICAL ELEVATION OF 16.55 FEET.

CIVIL ENGINEER
 FRYER & LAURETA, INC #4200
 201 N. CIVIC DR. #200
 LOS ANGELES, CA 90016
 CONTACT: ALEX HANSEN
 PHONE: 925-210-2522

ELECTRICAL ENGINEER
 BECKER ENGINEERING
 10000 RIVERSIDE DR #202
 ROSELIFE, CA 92471
 CONTACT: TODD BECKER
 PHONE: 941-580-8300

GEOTECHNICAL ENGINEER
 CONSTRUCTION TESTING SERVICES
 ONE EMBARCADERO CENTER, #535
 SAN FRANCISCO, CA 94111
 CONTACT: BRADFORD DUNN
 PHONE: 415-438-2337

STRUCTURAL ENGINEER
 THORNTON THOMSETTI
 707 WILSHIRE BLVD, #4450
 LOS ANGELES, CA 90017
 CONTACT: ALBERT CHEN
 PHONE: 213-339-7005

MECHANICAL ENGINEER
 BROWN AND CALDWELL
 201 N. CIVIC DR. #200
 LOS ANGELES, CA 90016
 CONTACT: ALEX HANSEN
 PHONE: 925-210-2522

CONTROL SCHEMATICS (SHEET 2 OF 4)
 GENERAL NOTES
 CONTROL SCHEMATICS (SHEET 3 OF 4)
 CONTROL SCHEMATICS (SHEET 4 OF 4)
 ELECTRICAL/STORAGE BUILDING POWER AND CONTROL PLAN
 ELECTRICAL DETAILS
 ELECTRICAL DETAILS
 ELECTRICAL DETAILS
 ELECTRICAL DETAILS
 DUCTBANK SECTIONS
 DUCTBANK SECTIONS
 DUCTBANK SECTIONS
 DUCTBANK SECTIONS
 PLC-EQB: BACKPAN LAYOUT
 PLC-EQB: INPUT/OUTPUT SIGNAL LIST
 PLC-EQB: POWER DISTRIBUTION DIAGRAM
 PLC-EQB: MODULE NO.1 CONNECTIONS (A)
 PLC-EQB: MODULE NO.2 CONNECTIONS (D)
 PLC-EQB: MODULE NO.3 CONNECTIONS (D)
 PLC-EQB: MODULE NO.4 CONNECTIONS (D)
 PLC-EQB: MODULE NO.5 CONNECTIONS (D)
 PLC-EQB: MODULE NO.6 CONNECTIONS (D)
 PLC-EQB: MODULE NO.7 CONNECTIONS (D)

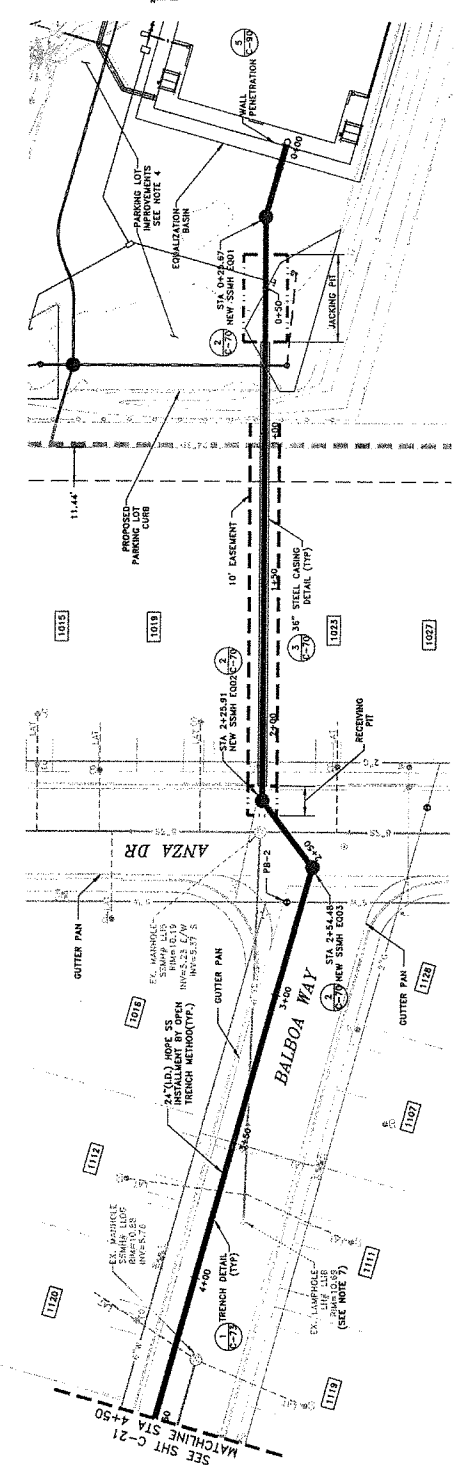
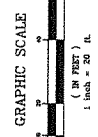
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 E-002
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 E-005
 E-101

ELECTRICAL
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STRUCTURAL
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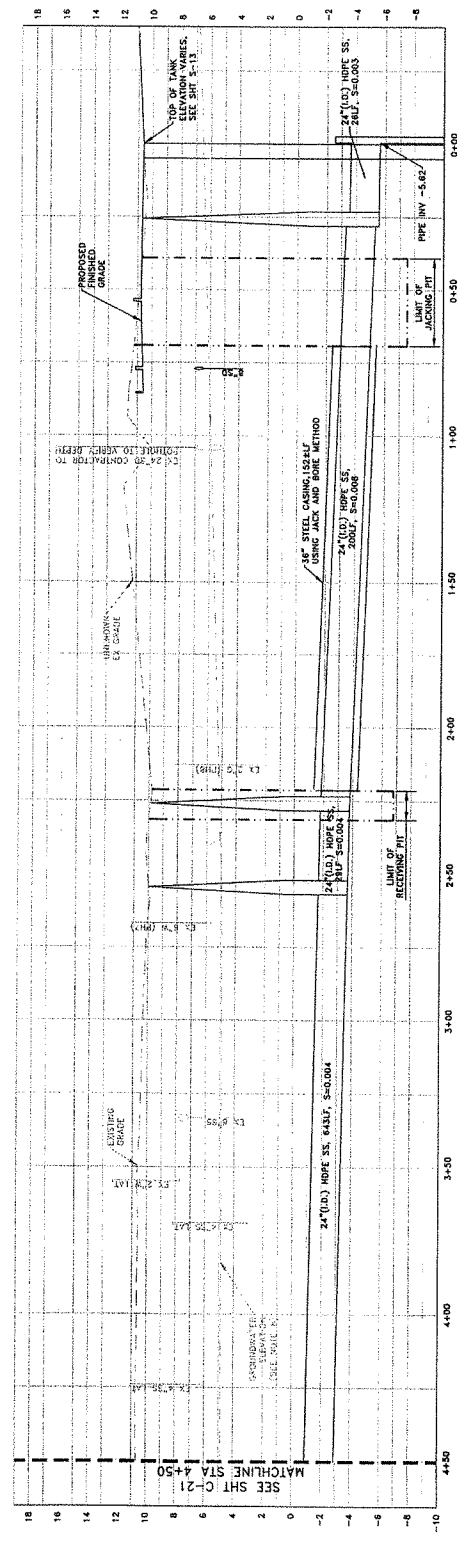
GENERAL NOTES
 ABBREVIATIONS
 FOUNDATION PLAN
 TEMPORARY STAGE SLURRY WALL PANEL/BRACING LAYOUT PLAN
 WALKWAY PLAN
 ROOF PLAN
 LAP SPlice, DEVELOPMENT LENGTH AND STANDARD HOOKS SCHEDULE
 TANK SECTIONS
 EAST AND WEST WALL ELEVATION
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 NORTH AND SOUTH WALL ELEVATION
 TEMPORARY STAGE
 TYPICAL BRACING CONNECTION DETAILS
 TYPICAL SLURRY WALL PANEL DETAILS
 EAST AND WEST LINER WALL REINFORCEMENT
 NORTH AND SOUTH LINER WALL REINFORCEMENT
 SECTIONS AND DETAILS
 TYPICAL PRECAST CONCRETE DETAILS
 TYPICAL MICROPILE DETAILS
 ELECTRICAL LEGEND AND SYMBOLS
 ELECTRICAL SITE PLAN
 MAIN SINGLE LINE DIAGRAM
 PANELBOARD SCHEDULE
 CIRCUIT SCHEDULE
 CONTROL SCHEMATICS (SHEET 1 OF 4)

WET WEATHER EQUALIZATION BASIN
 TITLE SHEET
 CITY OF PACIFICA, CALIFORNIA



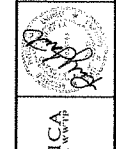
- NOTES:**
- PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
 - ALL SANITARY SEWER PIPE SHALL BE HOPE DR17.
 - EXISTING UTILITY DEPTH SHALL BE CONFIRMED BY CONTRACTOR ON SITE.
 - SEE SHEETS C20-C26 FOR PARKING LOT IMPROVEMENTS.
 - TRENCH DEWATERING WILL LIKELY BE NEEDED DUE TO A HIGH WATER TABLE. CONTRACTOR SHALL CONSULT THE GEOTECHNICAL REPORT DATED 04/10/18 FOR RECOMMENDATIONS OUTLINED IN SPECIFICATIONS SECTION 31 23 18.
 - SOIL TYPES WILL VARY ALONG THE PROPOSED PIPELINE. CONTRACTOR SHALL VERIFY SOIL TYPES AND SOILS SHALL EITHER BE SIGNED OR STOPPED BACK TO MEET ALL SOIL TRENCH EXCAVATION REQUIREMENTS.
 - EXISTING 8-IN SANITARY SEWER LINE TERMINATES AT THE LAMPPOLE.
 - APPROXIMATE GROUNDWATER ELEVATION AS INDICATED ACTUAL GROUNDWATER ELEVATION MAY VARY.

- LEGEND:**
- BORING PIT
 - EASEMENTS
 - PROPERTY LINE
 - STORM DRAIN PIPE
 - NEW SANITARY SEWER
 - NEW SSMH
 - POTHOLE LOCATION (PER EXARO REPORT)
 - STORM DRAIN INLET
- ABBREVIATIONS:**
- CO CLEANOUT
 - CR CURB
 - EX EXISTING
 - GA GAS
 - HP HOPE
 - JV JOINT ROLE
 - LA LATERAL
 - LAT LATERAL
 - ST STORM
 - SMH SMOKE
 - SS SANITARY SEWER
 - SSHR SANITARY SEWER MANHOLE
 - TP TYPICAL
 - W WATER
 - WM WATER METER



PROFILE VIEW
 HORIZ: 1" = 20'
 VERT: 1" = 4'

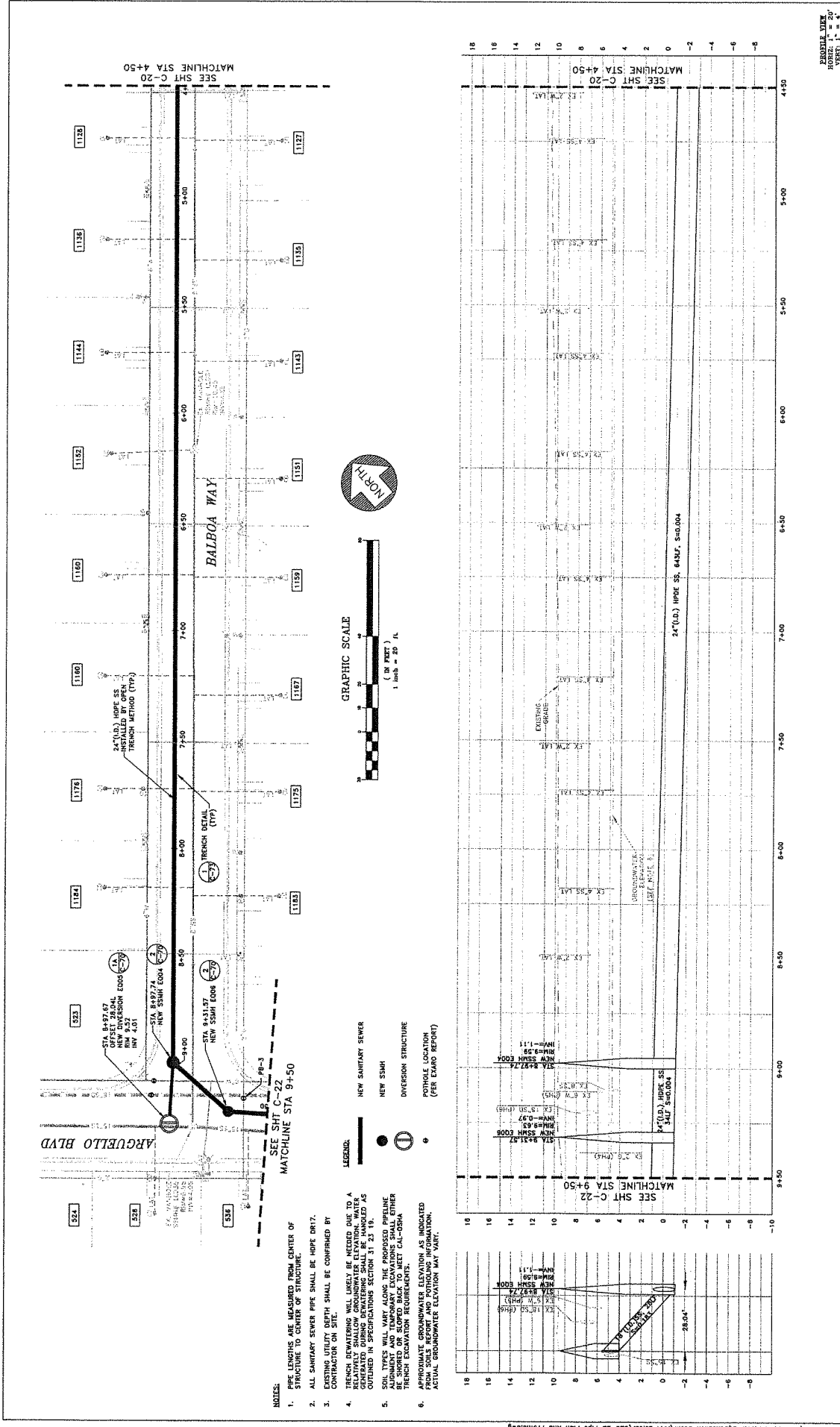
**WET WEATHER EQUALIZATION BASIN
 PIPELINE PLAN & PROFILE
 ANZA DR./ BALBOA WAY
 CITY OF PACIFICA, CALIFORNIA**



CITY OF PACIFICA
 DEPARTMENT OF PUBLIC WORKS
 1000 CALIFORNIA BLVD
 PACIFICA, CALIFORNIA 94026

PREYER LAURETA INC.
 2550 CALIFORNIA BLVD
 PACIFICA, CALIFORNIA 94026
 (415) 231-1111
 www.preyerlaureta.com

SCALE	AS NOTED	DATE	DESCRIPTION OF REVISIONS	APP'D
DESIGNED: RAL	06/05/18	06/05/18	BASES OF DESIGN	
DRAWN: A/R	06/05/18	06/05/18	10% SUBMITTAL	
CHECKED: M/Q	07/05/18	07/05/18	30% SUBMITTAL	
PROJ. ENGR: RAL	07/05/18	07/05/18	100% SUBMITTAL	



NOTES:

- PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
- ALL SANITARY SEWER PIPE SHALL BE HDPE DR17.
- CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND UTILITIES INFORMATION ON SITE.
- TRENCH DOWNGRADING WILL LIKELY BE NEEDED DUE TO A RELATIVELY SHALLOW GROUNDWATER ELEVATION. WATER TABLE DEPTH SHALL BE DETERMINED AS OBTAINED IN SPECIFICATIONS SECTION 31.23.14.
- SOIL TYPES WILL VARY ALONG THE PROPOSED PIPELINE ALIGNMENT AND TEMPORARY EXCAVATIONS SHALL EITHER BE STABILIZED WITH GEOTEXTILES OR SHIELDED WITH TRENCH ELEVATION EQUIPMENT.
- APPROXIMATE GROUNDWATER ELEVATION IS INDICATED FROM SOIL REPORT AND POTHOLES INFORMATION. ACTUAL GROUNDWATER ELEVATION MAY VARY.

LEGEND:

- NEW SANITARY SEWER
- NEW SSMH
- DIVERSION STRUCTURE
- POT HOLE LOCATION (PER EXISTING REPORT)

SCALE: AS NOTED

DESIGNED: RIL 06/06/18 BASIS OF DESIGN

DRAWN: AJR 10/09/18 90% SUBMITTAL

CHECKED: RIG 11/29/18 95% SUBMITTAL

PROJ. ENGR. RIL 01/07/19 100% SUBMITTAL

BY	DATE	DESCRIPTION OF REVISIONS	APP'D

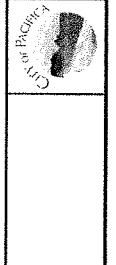
PROJ. TITLE: WET WEATHER EQUALIZATION BASIN PIPELINE PLAN & PROFILE
LOCATION: BALBOA WAY
CITY OF PACIFIC, CALIFORNIA

PROJECT NO.: 240001
SHEET NO.: C-21

SCALE: 1" = 20'

PROFILE VIEW
 SCALE: 1" = 20'
 SHEET
 C-22
 JOB NO.
 240001

**WET WEATHER EQUALIZATION BASIN
 PIPELINE PLAN & PROFILE
 ARGUELLO BLVD.
 CITY OF PACIFICA, CALIFORNIA**

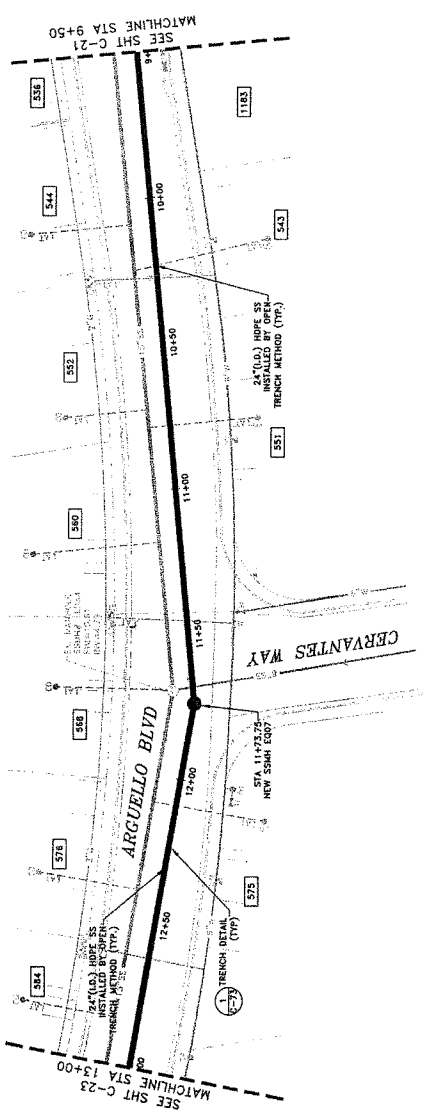
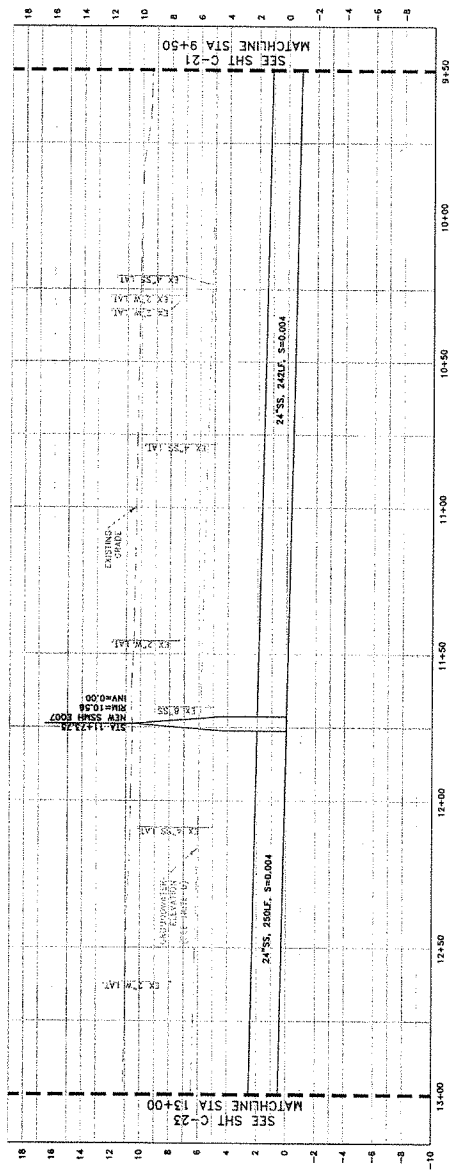


PREVEY LAURETTA INC.
 ENGINEERING ARCHITECTURE INTERIOR DESIGN
 1000 S. GARDEN AVENUE, SUITE 100
 PACIFICA, CA 94041
 TEL: 650.351.1000
 FAX: 650.351.1001
 WWW.PREVEYLAURETTA.COM

BY	DATE	DESCRIPTION OF REVISIONS
APP'D		

SCALE: AS NOTED
 DESIGNED: RLL
 DRAWN: AJR
 CHECKED: MQ
 PROJ. ENGR: RLL

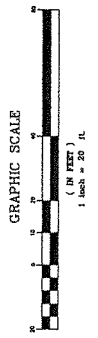
09/06/18 BASIS OF DESIGN
 09/06/18 60% SUBMITTAL
 10/05/18 80% SUBMITTAL
 11/05/18 85% SUBMITTAL
 01/19/17 100% SUBMITTAL

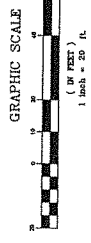
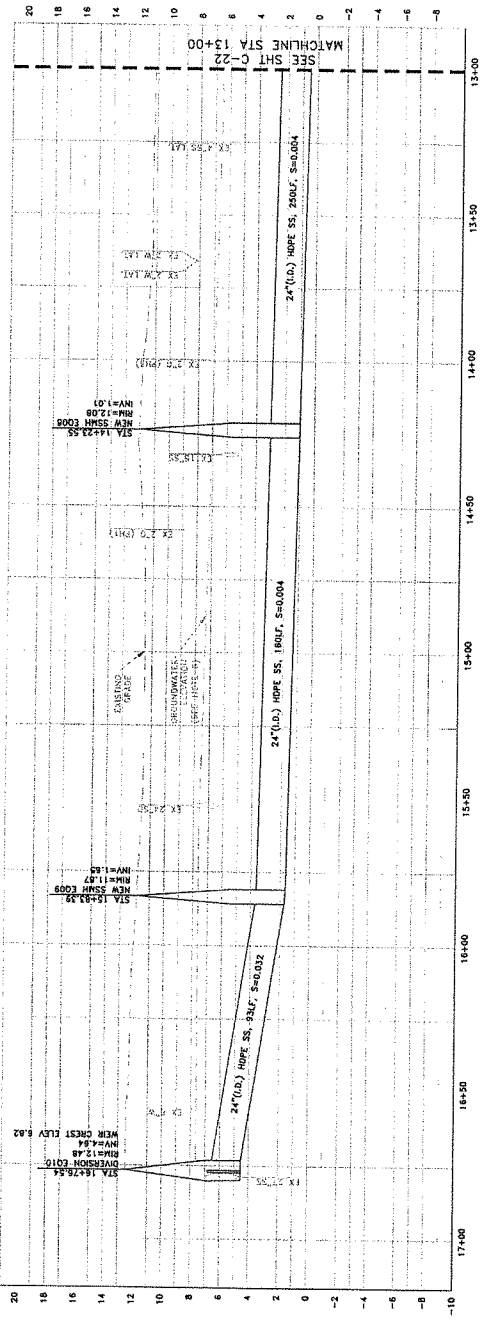
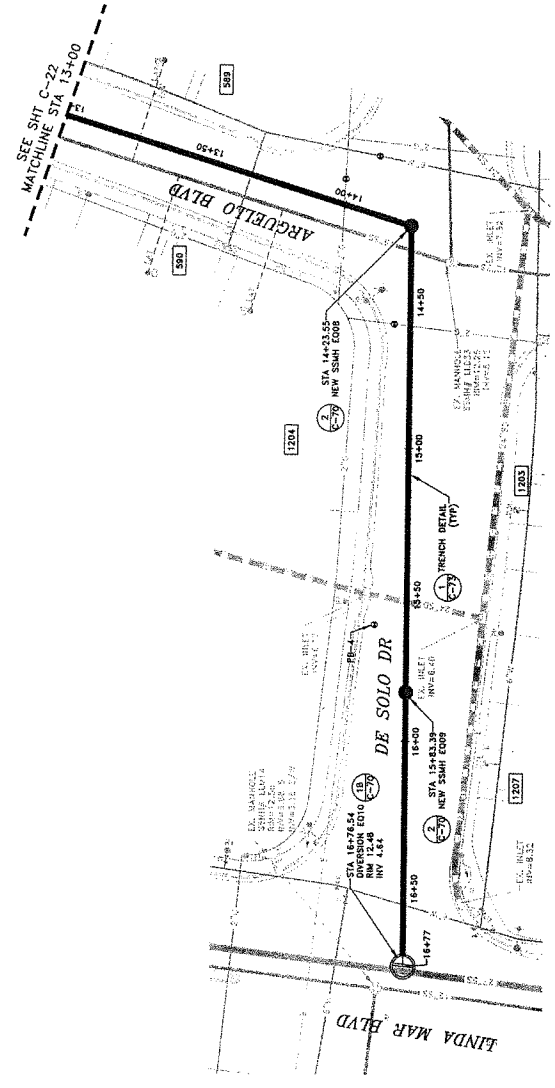


- NOTES:**
- PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
 - ALL SANITARY SEWER PIPE SHALL BE HDPE DRI7.
 - EXISTING UTILITY DEPTH SHALL BE CONFORMED BY CONTRACTOR ON SITE.
 - TRENCH DEWATERING WILL LIKELY BE NEEDED DUE TO A DEEP EXISTING SANITARY SEWER. GROUNDWATER ELEVATION WATER GENERATED DURING EXCAVATION SHALL BE COLLECTED AS OUTLINED IN SPECIFICATIONS SECTION 31 23 19.
 - SOIL TYPES WILL VARY ALONG THE PROPOSED PIPELINE. AS SOUGHT AND DISCOVERED, EXCAVATIONS SHALL OTHER TRENCH EXCAVATION REQUIREMENTS.
 - APPROXIMATE GROUNDWATER ELEVATION AS INDICATED FROM SOILS REPORT AND POTHOLES INFORMATION. ACTUAL GROUNDWATER ELEVATION MAY VARY.

LEGEND:

— NEW SANITARY SEWER
 - - - EXISTING SANITARY SEWER





- NOTES:**
1. PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
 2. ALL SANITARY SEWER PIPE SHALL BE HDPE DRI7.
 3. EXISTING UTILITY DEPTH SHALL BE CONFIRMED BY CONTRACTOR ON SITE.
 4. BENCH ELEVATIONS WILL LIKELY BE ADJUSTED DUE TO A REVISIONS GENERATED DURING PERMITTING AS OBTAINED IN SPECIFICATIONS SECTION 31 23 19.
 5. SOIL TYPES WILL VARY ALONG THE PROPOSED PIPELINE AND SHALL BE SHOWN ON SLOPED BACK TO MEET CAL-USA FRENCH EXCAVATION REQUIREMENTS.
 6. APPROXIMATE GROUNDWATER ELEVATION AS INDICATED BY WATER TABLE DATA FROM NEARBY WELLS. ACTUAL GROUNDWATER ELEVATION MAY VARY.

- LEGEND:**
- NEW SANITARY SEWER
 - NEW SSMH
 - ⊖ DIVERSION STRUCTURE
 - POTHOLE LOCATION (PER EXISTING REPORT)

PROFILES ONLY
 HORIZ: 1" = 20'
 VERT: 1" = 4'

SHEET
 C-23
 JOB NO.
 240001

**WET WEATHER EQUALIZATION BASIN
 PIPELINE PLAN & PROFILE
 DE SOLO DR./ARGUELLO BLVD.
 CITY OF PACIFICA, CALIFORNIA**



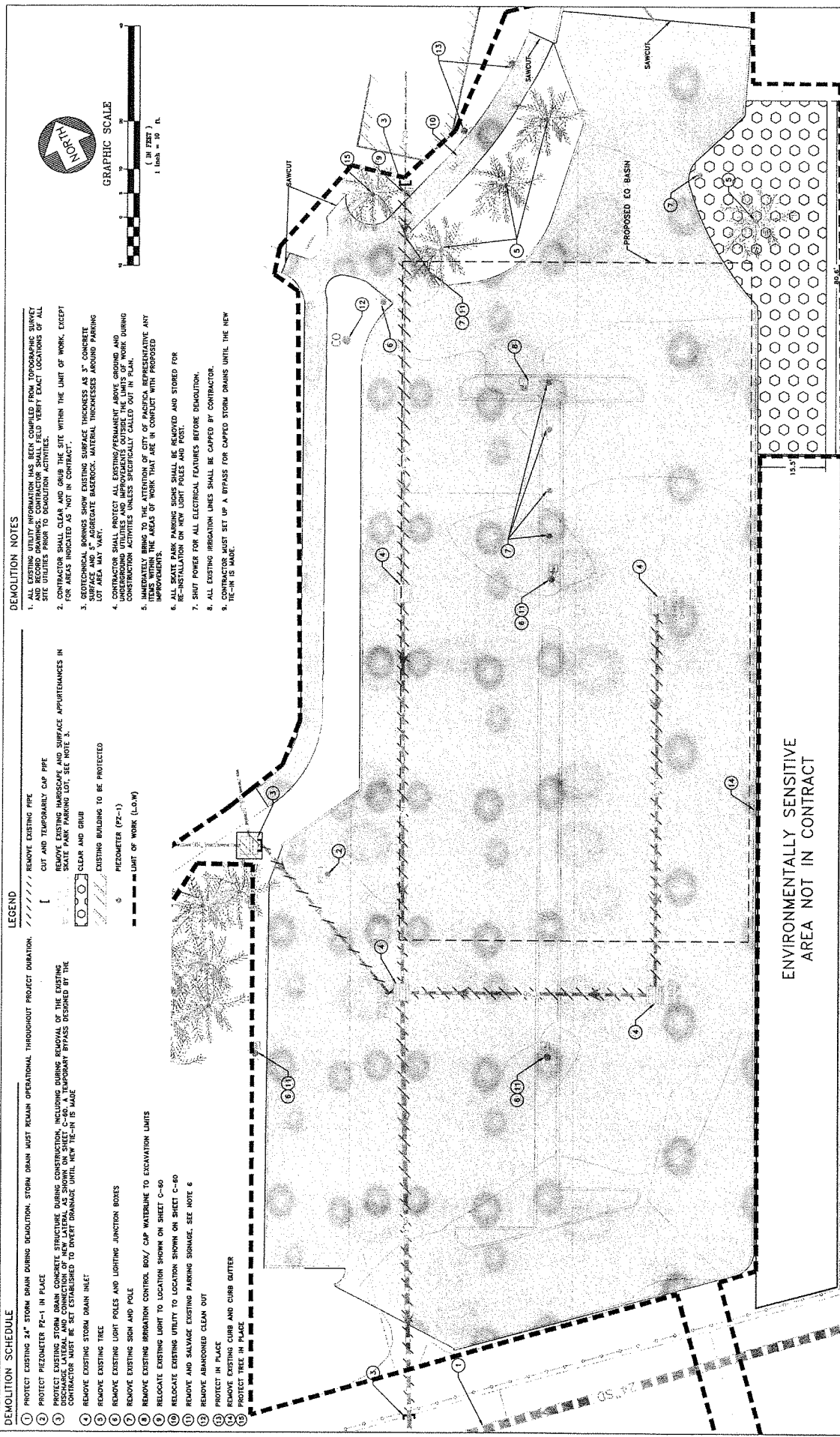
CITY OF PACIFICA
 DEPARTMENT OF PUBLIC WORKS & UTILITIES
 PACIFICA, CALIFORNIA 94026



FREYER & LAURET, INC.
 10000 DEER CREEK ROAD, SUITE 100
 SAN DIEGO, CALIFORNIA 92121
 (619) 444-1111
 WWW.FREYERANDLAURET.COM

BY	DATE	DESCRIPTION OF REVISIONS
		APP'D

SCALE: AS NOTED
 DESIGNED: RIL 08/09/14 100% SUBMITTAL
 DRAWN: AJR 10/09/14 90% SUBMITTAL
 CHECKED: RQ 11/25/14 50% SUBMITTAL
 PROJ. ENGR. RIL 01/27/17 100% SUBMITTAL



DEMOLITION SCHEDULE

1. PROTECT EXISTING 24" STORM DRAIN DURING DEMOLITION. STORM DRAIN MUST REMAIN OPERATIONAL THROUGHOUT PROJECT DURATION.
2. PROTECT PNEUMETER PZ-1 IN PLACE
3. REMOVE EXISTING STORM DRAIN CONCRETE STRUCTURE DURING CONSTRUCTION, INCLUDING DURING REMOVAL OF THE EXISTING DISCHARGE LATERAL AND CONNECTION OF NEW LATERAL AS SHOWN ON SHEET C-80. A TEMPORARY BYPASS DESIGNED BY THE CONTRACTOR MUST BE SET ESTABLISHED TO DIVERT DRAINAGE UNTIL NEW TIE-IN IS MADE.
4. REMOVE EXISTING STORM DRAIN INLET
5. REMOVE EXISTING TREE
6. REMOVE EXISTING LIGHT POLES AND LIGHTING JUNCTION BOXES
7. REMOVE EXISTING SIGN AND POLE
8. REMOVE EXISTING IRRIGATION CONTROL BOX/ CAP WATERLINE TO EXCAVATION LIMITS
9. RELOCATE EXISTING LIGHT TO LOCATION SHOWN ON SHEET C-80
10. RELOCATE EXISTING UTILITY TO LOCATION SHOWN ON SHEET C-80
11. REMOVE AND SALVAGE EXISTING PARKING SIGNAGE. SEE NOTE 6
12. REMOVE UNMANNED CLEAN OUT
13. PROTECT IN PLACE
14. REMOVE EXISTING CURB AND CURB CUTTER
15. PROTECT TREE IN PLACE

LEGEND

- REMOVE EXISTING PIPE
- CUT AND TEMPORARILY CAP PIPE
- REMOVE EXISTING HARDSCAPE AND SURFACE APPURTENANCES IN SKATE PARK PARKING LOT. SEE NOTE 3.
- CLEAR AND GRUB
- EXISTING BUILDING TO BE PROTECTED
- PNEUMETER (PZ-1)
- LIMIT OF WORK (L.O.W.)

DEMOLITION NOTES

1. ALL EXISTING UTILITY INFORMATION HAS BEEN COMPILED FROM TOPOGRAPHIC SURVEY AND FIELD DATA. CONTRACTOR SHALL VERIFY EXACT LOCATIONS OF ALL UTILITIES DURING CONSTRUCTION.
2. CONTRACTOR SHALL CLEAR AND GRUB THE SITE WITHIN THE LIMIT OF WORK, EXCEPT FOR AREAS INDICATED AS "NOT IN CONTRACT".
3. GEOTECHNICAL BORINGS SHOW EXISTING SURFACE THICKNESSES AS 12" SAND/CLAY SURFACE AND 2" AGGREGATE BASE/CURB. MATERIAL THICKNESSES AROUND PARKING LOT AREA MAY VARY.
4. CONTRACTOR SHALL PROTECT ALL EXISTING/PERMANENT ABOVE GROUND AND UNDERGROUND UTILITIES AND IMPROVEMENTS OUTSIDE THE LIMITS OF WORK DURING DEMOLITION. CONTRACTOR SHALL VERIFY ALL UTILITIES AND IMPROVEMENTS BY ANY ITEMS WITHIN THE AREAS OF WORK THAT ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS.
5. ALL SKATE PARK PARKING SIGNS SHALL BE REMOVED AND STORED FOR RE-INSTALLATION ON NEW LIGHT POLES AND POSTS.
6. SHUT POWER FOR ALL ELECTRICAL FEATURES BEFORE DEMOLITION.
7. ALL EXISTING IRRIGATION LINES SHALL BE CAPPED BY CONTRACTOR.
8. CONTRACTOR MUST SET UP A BYPASS FOR CAPPED STORM DRAINS UNTIL THE NEW TIE-IN IS MADE.

SCALE: AS NOTED

DESIGNED:	RAL	08/05/14	100% SUBMITTAL
DRAWN:	AJR	10/05/14	100% SUBMITTAL
CHECKED:	MQ	11/05/14	100% SUBMITTAL
PROJ. ENGR:	RAL	01/16/15	100% SUBMITTAL

DESCRIPTION OF REVISIONS

BY	DATE	DESCRIPTION OF REVISIONS	APP'D

ENVIRONMENTALLY SENSITIVE AREA NOT IN CONTRACT

WET WEATHER EQUALIZATION BASIN DEMOLITION PLAN PARKING LOT

CITY OF PACIFICA

CITY OF PACIFICA

CITY OF PACIFICA

STREET
C-30

JOB NO.
240001

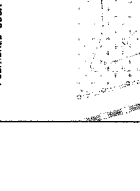
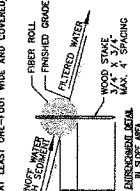
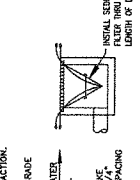
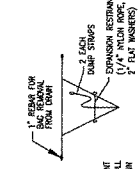
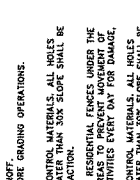
CITY OF PACIFICA
DEPARTMENT OF PUBLIC WORKS & UTILITY
PACIFICA CALIFORNIA



- LEGEND**
- FIBER ROLLS, SEE DETAIL 1
 - INLET CAPTURE BAG, SEE DETAIL 2
 - TEMPORARY FENCING
 - ENVIRONMENTALLY SENSITIVE AREA
 - ANNUAL EXCLUSION FENCING
 - LIMIT OF WORK

EROSION CONTROL NOTES

1. EROSION CONTROL MEASURES SHOWN ON THIS SHEET ARE GENERAL IN NATURE AND ALL EROSION CONTROL MEASURES FOR THE PROJECT SHALL BE IN ACCORDANCE WITH THE "PROPOSED EROSION CONTROL" PREPARED BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR DEVELOPING A SWEEP PREPARED BY A CALIFORNIA CSD FOR SUBMITTAL AND APPROVAL BY THE CITY AND PERFORMING ALL SWEEP REQUIRED. STORM WATER DEFLECTIONS BY CALIFORNIA CSD TO THE IMMEDIATE ATTENTION OF THE OWNER'S REPRESENTATIVE FOR APPROVAL.
2. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED YEAR ROUND AND RETURNED TO ORIGINAL CONDITION. CHANGES MADE TO SUIT FIELD CONDITIONS SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE OWNER'S REPRESENTATIVE FOR APPROVAL.
3. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED YEAR ROUND AND RETURNED TO ORIGINAL CONDITION. CHANGES MADE TO SUIT FIELD CONDITIONS SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE OWNER'S REPRESENTATIVE FOR APPROVAL.
4. IF SIGNIFICANT SEEDING OR OTHER VISUAL SYMPTOMS OF IMPAIRMENT ARE NOTICED IN THE STORM WATER CONTACT ZONE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMMEDIATE REMEDIATION.
5. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.
6. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.
7. EXCAVATION AREAS AND TEMPORARY STOCKPILES SHALL BE PROTECTED WITH APPROPRIATE EROSION CONTROL MEASURES TO PREVENT EROSION. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE EXCAVATION AREAS AND TEMPORARY STOCKPILES ARE RESTORED TO ORIGINAL CONDITION.
8. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.
9. STORM WATER SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL APPLICABLE REGULATIONS AND PERMITS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.
10. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.
11. NO CLEANING, FUELING OR MAINTAINING VEHICLES ON SITE SHALL BE PERMITTED TO ALLOW DEBRIS OR OILY MATERIALS TO ENTER THE STORM DRAIN SYSTEM.
12. THE DIMENSIONS AND LOCATION OF THE STORM INLET SEDIMENTATION TRAPS (INLET CAPTURE BAGS) ARE AS SHOWN ON THIS PLAN. THE SEDIMENTATION TRAPS SHOULD BE CONSTRUCTED BEFORE GRADING OPERATIONS.
13. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.
14. ANNUAL EXCLUSION FENCING INSTALLED BY OTHERS MUST BE PROTECTED THROUGHOUT THE ENTIRE DURATION OF THE PROJECT.
15. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.
16. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.
17. SUPERVISION OF A QUALIFIED BIOLOGIST: A WILDLIFE EXCLUSION/ENVIRONMENTAL FENCE (WITH ESCAPE ROUTES, SUCH AS EXIT FUNNELS) SHALL BE ERRECTED AROUND ACTIVE CONSTRUCTION AREAS TO PREVENT MOVEMENT OF WILDLIFE INTO CONSTRUCTION AREAS. THE FENCE SHALL BE MAINTAINED THROUGHOUT THE ENTIRE DURATION OF THE PROJECT.
18. BREAKS OR TRAPPED WILDLIFE BEFORE CONSTRUCTION ACTIVITIES COMMENCE AND DAMAGE TO THE FENCE SHALL BE REPAIRED IN A TIMELY MANNER.
19. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.
20. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED SO THAT SEDIMENT-LADEN RUNOFF DOES NOT ENTER THE STORM DRAIN SYSTEM. THESE PLANS SHALL REMAIN IN PLACE UNTIL THE STORM DRAIN SYSTEM IS RESTORED TO ORIGINAL CONDITION.



NOTES

1. STABILIZED CONSTRUCTION ENTRANCE SHALL BE CONSTRUCTED OF 3" TO 4" WOOD, WELL-WEARED PANEL OR CORRODED ROCK. MATERIAL SHALL BE PLACED TO A MINIMUM THICKNESS OF 6 INCHES.
2. LENGTH OF ENTRANCE SHALL BE A MINIMUM OF 50 FEET. WIDTH SHALL BE A MIN. OF 15 FT OR GREATER IF NECESSARY TO COVER ALL CIRCULAR WHEELS AND TRACKS. ENTRANCE SHALL BE POSITIONED TO MAXIMIZE CONVEYANCE OF THE ROCK AND ENTRANCE PROGRESS.
3. THE ENTRANCE SHALL BE KEPT IN GOOD CONDITION BY COORDINATING TOP DRESSING WITH MATERIAL AS IS SPECIFIED IN NOTE 1. PERIODIC ROLLING AND COMPACTING OF THE ENTRANCE SHALL BE PERFORMED TO MAINTAIN CONVEYANCE OF THE ROCK AND ENTRANCE PROGRESS.
4. MAINTENANCE PROVIDED AS NECESSARY. PERIODIC TOP DRESSING SHALL BE DONE AS NEEDED.
5. LOCATIONS FOR ALTERNATE/ADDITIONAL CONSTRUCTION ENTRANCES SHALL BE DETERMINED BY THE CONTRACTOR AND SHALL BE COVERED UNDER THE SWEEP.
6. EXACT LOCATION OF STABILIZED CONSTRUCTION ENTRANCES SHALL BE DETERMINED IN THE FIELD BY THE OWNER'S REPRESENTATIVE AND WILL BE DEFERRED BY THE CONTRACTOR BASED ON THEIR MEANS AND METHODS FOR CONSTRUCTING AND CONTROLLING THE SITE.

NOTES

1. SEEDMENT FILTERS SHALL BE INSPECTED REGULARLY THROUGHOUT THE COURSE OF CONSTRUCTION TO INSURE PROPER PLACEMENT AND CONDITION OF FILTER.
2. DAMAGED OR PUNCTURED SEEDMENT FILTERS SHALL BE REPLACED IMMEDIATELY AND ALL CAPTURED SEDIMENT/FILTERS SHALL BE PROPERLY DISPOSED OF.
3. THIS OF SEEDMENT BAG TO BE A CONTRACTOR SUBMITTAL FOR REVIEW AND APPROVAL BY THE CITY OF PACIFICA. THE BAG SHALL BE PROTECTED BY THE CONTRACTOR'S QUALIFIED SWEEP DEVELOPER (QSD) IN THE SWEEP REPORT.
4. EXACT LOCATION OF SEEDMENT BAG INSTALLATION LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR'S QUALIFIED SWEEP DEVELOPER (QSD) IN THE SWEEP REPORT.

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NOTES

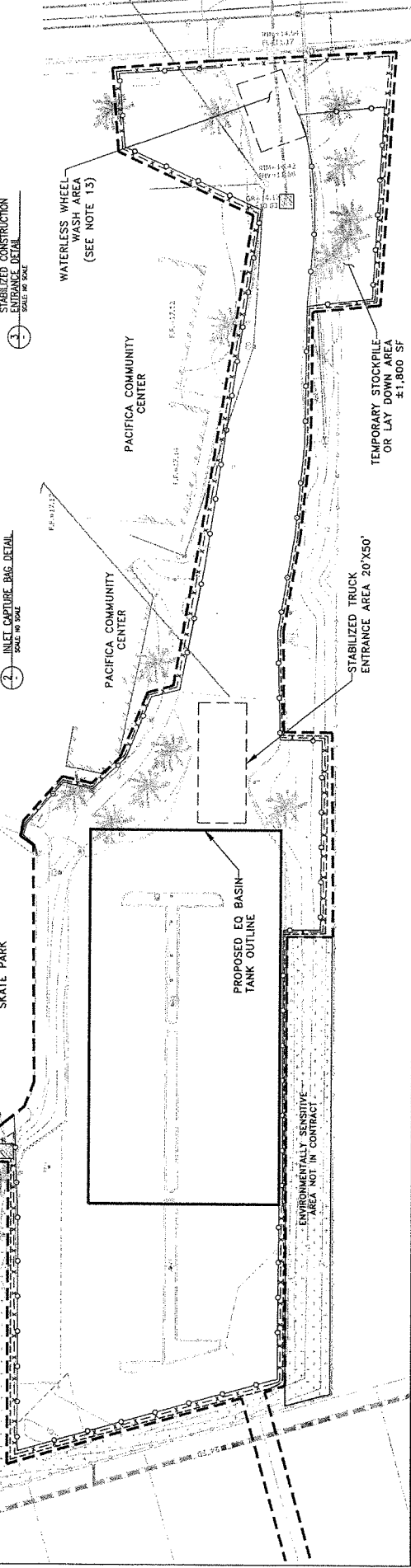
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SCALE: AS NOTED		DESIGNER: RIL	DATE: 09/09/18	REVISIONS:	DESCRIPTION OF REVISIONS
DESIGNED: AIR	DATE: 10/09/18	100% SUBMITTAL			
DRAWN: JAG	DATE: 11/29/18	90% SUBMITTAL			
CHECKED: JAG	DATE: 01/09/19	100% SUBMITTAL			
PROJ. ENGR: RIL	DATE: 01/09/19	100% SUBMITTAL			
BY:	DATE:	DESCRIPTION OF REVISIONS:	APP'D		

WET WEATHER EQUALIZATION BASIN
EROSION CONTROL PLAN
PARKING LOT
CITY OF PACIFICA, CALIFORNIA

SECRET
C-31
JOB NO. 240001

CITY OF PACIFICA
DEPARTMENT OF PUBLIC WORKS, W.P.D.
PACIFICA, CALIFORNIA 94026

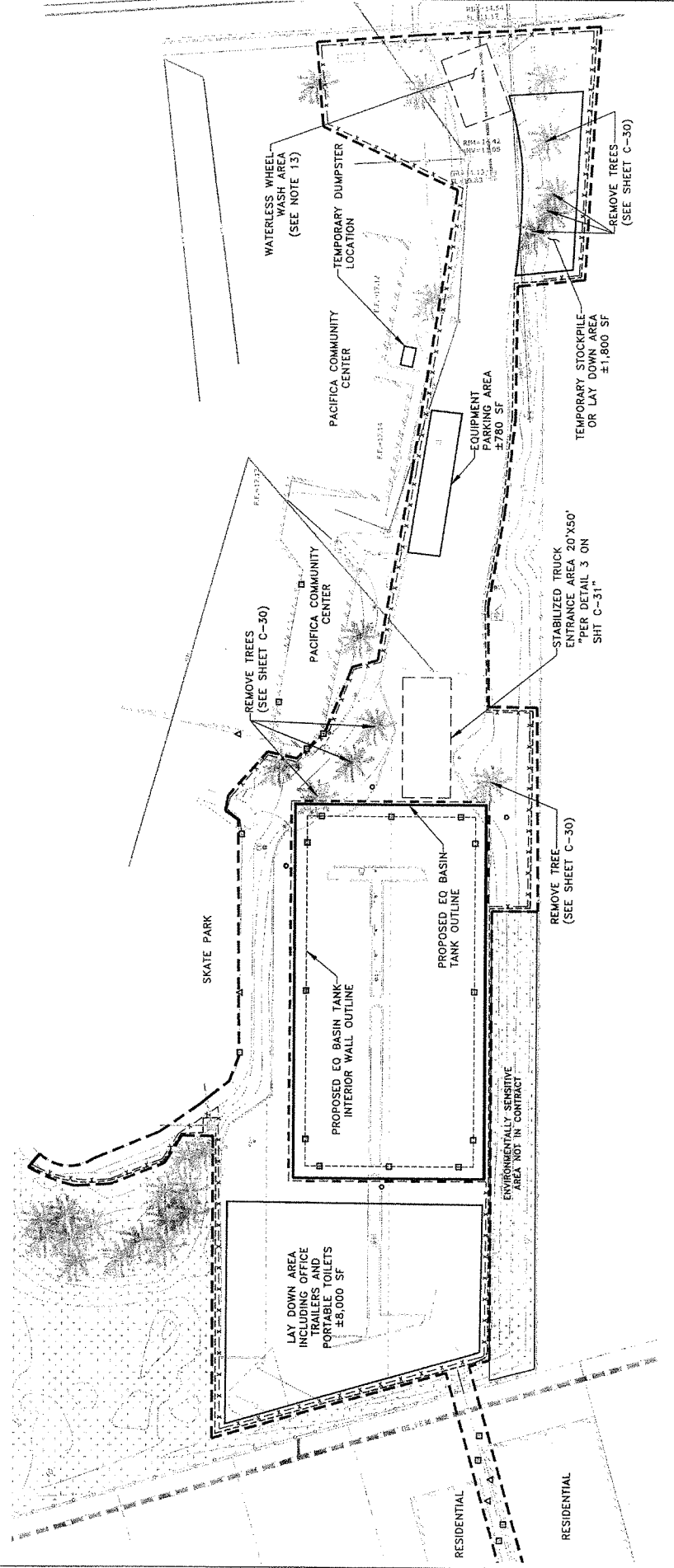
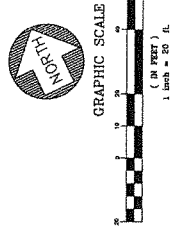
FRYER LAURETTA, INC.
1000 S. GARDEN ST. SUITE 200
PACIFICA, CALIFORNIA 94026
TEL: 415.894.0000
WWW.FRYERLAURETTA.COM

STAGING NOTES

1. LAYDOWN AREA SHALL BE A TEMPORARY LOCATION TO STORE EQUIPATED MATERIAL BEFORE OFF LOADING.
2. THE CITY OF PACIFICA WILL BE RESPONSIBLE FOR OBTAINING COVERAGE UNDER THE STATE WATER RESOURCES CONTROL BOARD'S NOISES GENERAL PERMIT FOR STORM WATER DISCHARGES ASSOCIATED WITH CONSTRUCTION AND LAND DISTURBANCE ACTIVITIES (ORDER NO. 209-0009-DWG, NOISES NO. C45000002) AS THE LEGALLY RESPONSIBLE PARTY.
3. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS AND PLANS WITH THE RELATED AGENCIES ASSOCIATED WITH THEIR WORK. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, PERMITS FOR STORAGE OF HAZARDOUS MATERIALS, BUSINESS PLANS, PERMITS FOR STORAGE OF FLAMMABLE LIQUIDS, GRADING PERMITS, OR OTHER PERMITS AS REQUIRED BY THE CITY OF PACIFICA.
4. SUBMITTING ANY BUSINESS PLANS OR PERMITS REQUIRED BY CITY, STATE OR FEDERAL AGENCIES.
5. TEMPORARY FENCING SHALL BE INSTALLED AROUND THE PERIMETER OF THE PROJECT AREA PER CALIFORNIA STANDARDS SPECIFIED IN THE PERMITS.
6. ENVIRONMENTAL EXCLUSION FENCING SHALL BE MAINTAINED THROUGHOUT THE ENTIRE DURATION OF THE PROJECT.
7. ENVIRONMENTAL EXCLUSION FENCING SHALL BE MAINTAINED THROUGHOUT THE ENTIRE DURATION OF THE PROJECT.
8. FOR DISPOSAL OF ANY ITEMS THROUGHOUT THE CONSTRUCTION ACTIVITIES AND MUST MAINTAIN ACCESS FOR THE CITY'S WASTE DISPOSAL COMPANY TO PERIODICALLY EMPTY THE TRAILERS.
9. PARKING LOT AND THE RESIDENTIAL FENCES UNDER THE SUPERVISION OF A QUALIFIED BIOLOGIST. A WAIVER EXCLUSION APPROXIMATELY 50% OF THE STATE ROUTES, SUCH AS LOT FURNISHES) SHALL BE ERECTED AROUND ACTIVE CONSTRUCTION AREAS TO PREVENT MOVEMENT OF ANIMALS INTO ACTIVE CONSTRUCTION AREAS UNDER CONSTRUCTION.
10. BREAKS, OR TAPPED WEAVERS BEFORE CONSTRUCTION ACTIVITIES COMMENCE. ANY DAMAGE TO THE FENCE SHALL BE REPAIRED WITHIN A THREE BUSINESS DAY FOR DAMAGE.
11. NEAR THE BASE OF THE WALL, CONTRACTOR SHALL POST SURVEY MARKERS. ONE SURVEY MARKER NEAR THE TOP OF THE SLURRY WALL, ONE NEAR THE MIDPOINT, AND ONE NEAR THE BASE OF THE WALL. CONTRACTOR SHALL SUBMIT LOCATIONS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL SUBMIT LOCATION PLAN FOR APPROVAL.

LEGEND

- FIBER ROLLS. SEE DETAIL 1, SHEET C-31
- INLET CAPTURE BAG. SEE DETAIL 2, SHEET C-31
- INCHOMETER
- SURVEY MARKER
- TILT METER
- TEMPORARY FENCING
- ENVIRONMENTALLY SENSITIVE AREA
- ANIMAL EXCLUSION FENCING
- EQUALIZATION BASIN TANK OUTLINE
- EQUALIZATION BASIN FENCING
- LIMIT OF WORK
- EXISTING BUILDING



SCALE: AS NOTED	05/06/18 BASIS OF DESIGN		
DESIGNED: RLL	06/06/18 60% SUBMITTAL		
DRAWN: AJR	10/02/18 90% SUBMITTAL		
CHECKED: MQ	11/23/18 95% SUBMITTAL		
PROJ. ENGR: RLL	01/19/17 100% SUBMITTAL		
REV. NO.	DATE	DESCRIPTION OF REVISIONS	APP'D



CITY OF PACIFICA
DEPARTMENT OF PUBLIC WORKS & UTILITIES
PACIFICA ENGINEERING DEPARTMENT

WET WEATHER EQUALIZATION BASIN STAGING PLAN
PARKING LOT
CITY OF PACIFICA, CALIFORNIA

SHEET C-32
JOB NO. 240001



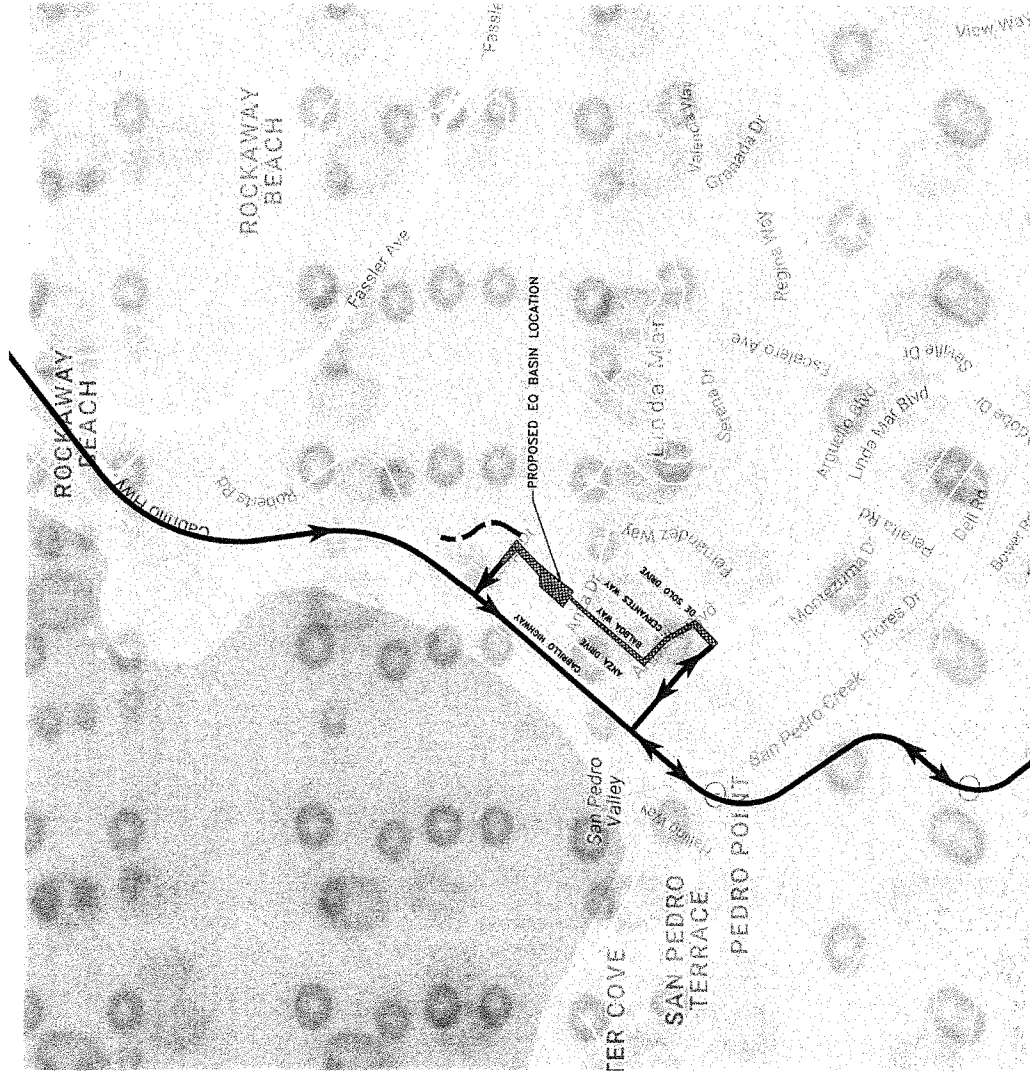
GRAPHIC SCALE
NOT TO SCALE

LEGEND

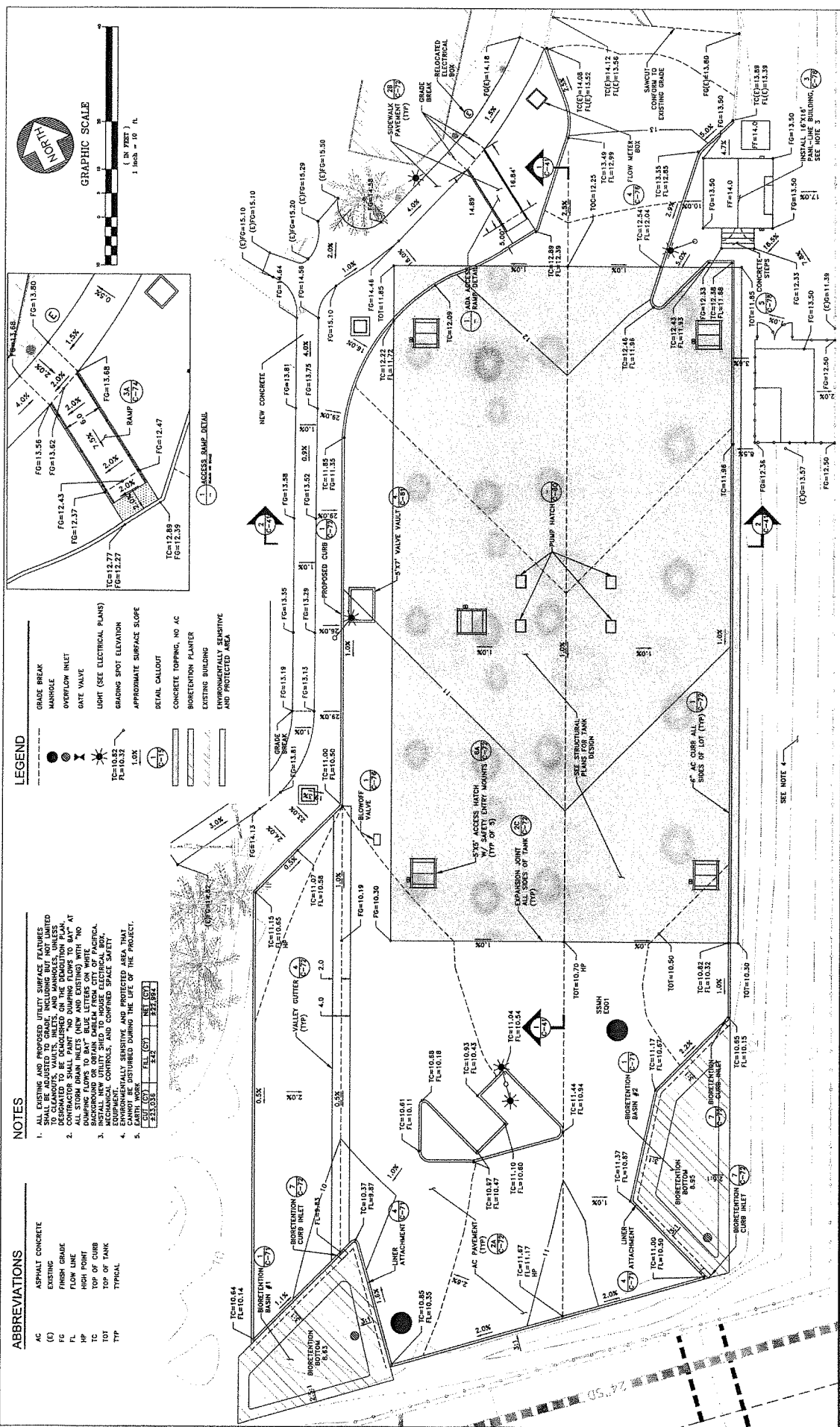
- ALLOWED TRUCK ROUTES (INBOUND AND OUTBOUND)
- ▭ PROPOSED EMPLOYEE PARKING AREA
- ▨ PROPOSED PROJECT AREA

NOTES

1. ALLOWABLE TRUCK TRAFFIC ROUTES PRESENTED ARE IN ACCORDANCE TO THE CITY'S GENERAL TRAFFIC FLOW INFORMATION PROVIDED FOR INFORMATION.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING AND SUBMITTING A TRAFFIC CONTROL PLAN TO THE CITY FOR REVIEW AND APPROVAL.
3. THE CONTRACTOR SHALL MAINTAIN AT LEAST ONE-LANE OF ACCESS ALONG ALL AFFECTED CITY STREETS DURING PIPELINE CONSTRUCTION ACTIVITIES AS WELL AS PROVIDE TEMPORARY ACCESS TO ADJACENT SIDE STREETS SUBMITTED TO THE CITY FOR REVIEW AND APPROVAL. ACCESS INTO RESIDENT DRIVEWAYS FROM CITY STREETS MUST BE RE-ESTABLISHED AT THE COMPLETION OF EACH WORK DAY.



WET WEATHER EQUALIZATION BASIN GENERALIZED TRAFFIC FLOW PLAN CITY OF PACIFICA, CALIFORNIA		SHEET C-33 JOB NO. 240001
		
CITY OF PACIFICA DEPARTMENT OF PUBLIC WORKS & UTILITIES PACIFICA, CALIFORNIA		
		
SCALE: AS NOTED	05/26/16 BASIS OF DESIGN 08/05/16 80% SUBMITTAL 10/05/16 90% SUBMITTAL 11/25/16 95% SUBMITTAL 01/29/17 100% SUBMITTAL	DESCRIPTION OF REVISIONS
DESIGNED: RPL DRAWN: ATR CHECKED: MQ PROJ. ENGR. RPL	BY DATE	APP'D



LEGEND

- GRADE BREAK
- MANHOLE
- ⊕ OVERFLOW INLET
- ⊗ GATE VALVE
- ⊙ LIGHT (SEE ELECTRICAL PLANS)
- ⊛ GRADING SPOT ELEVATION
- ⊞ APPROXIMATE SURFACE SLOPE
- ⊟ DETAIL CALLOUT
- ⊠ CONCRETE OFFRING, NO AC
- ⊡ BOREVENTION PLANTER
- ⊢ EXISTING BUILDING
- ⊣ PROPOSED BOREVENTION SENSITIVE AND PROTECTED AREA

ABBREVIATIONS

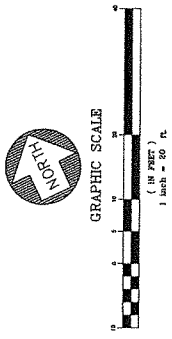
- AC ASPHALT CONCRETE
- (C) EXISTING
- FC FINISH GRADE
- FL FLOW LINE
- HP HIGH POINT
- TC TOP OF CURB
- TOT TOP OF TANK
- TYP TYPICAL

NOTES

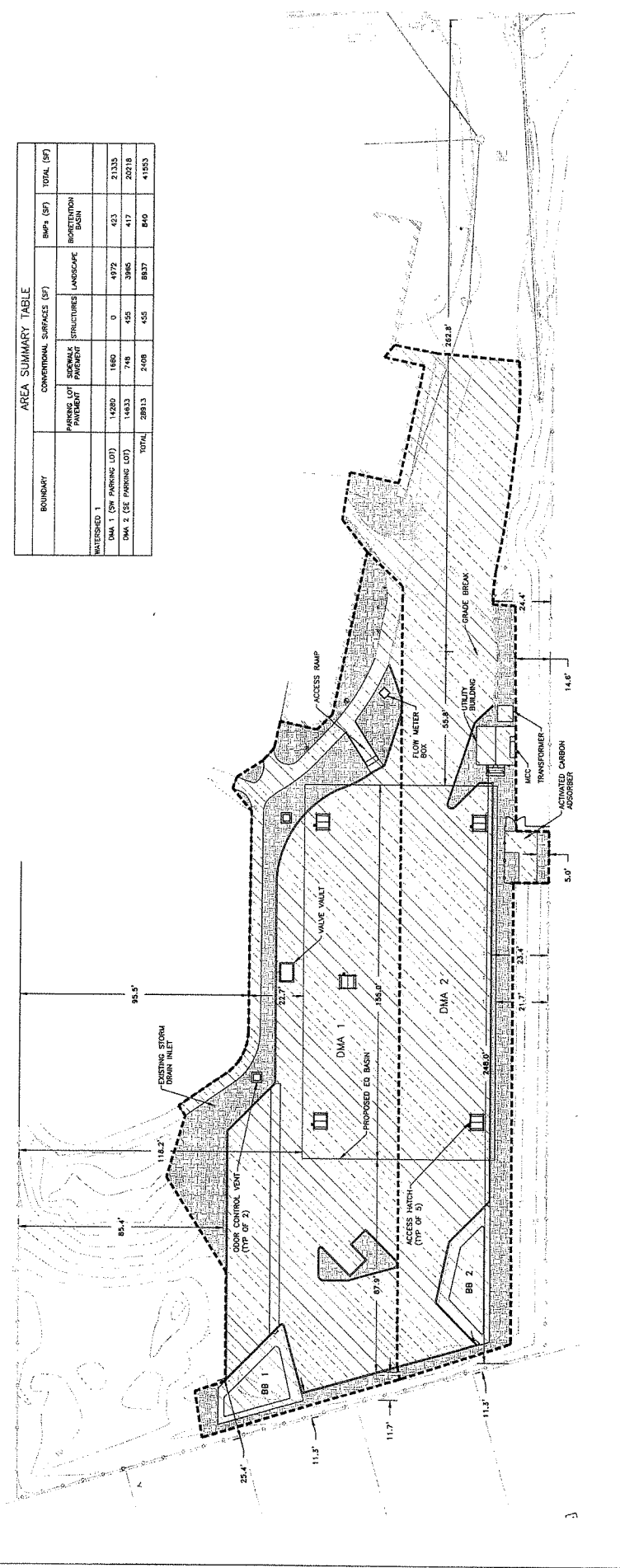
1. ALL EXISTING AND PROPOSED UTILITY SURFACE FEATURES SHALL BE ADJUSTED TO GRADE, INCLUDING BUT NOT LIMITED TO MANHOLES, VALVES, INLETS AND MANHOLES UNLESS OTHERWISE NOTED.
2. CONTRACTOR SHALL PAINT AND DUMPING FLOWS TO BAY AT ALL STORM DRAIN INLETS (NEW AND EXISTING) WITH "NO BACKFLOW" AND "NO BACKFLOW" SIGNS AND SHALL OBTAIN BACKGROUND OR OBTAIN EMBLEM FROM CITY OF PACIFICA, CALIFORNIA.
3. MECHANICAL CONTROL SHALL BE MAINTAINED THROUGHOUT EQUIPMENT INSTALLATION AND PROTECTED AREAS SHALL NOT BE DISTURBED DURING THE LIFE OF THE PROJECT.
4. EXISTING UTILITY SENSITIVE AND PROTECTED AREAS SHALL BE MAINTAINED THROUGHOUT THE LIFE OF THE PROJECT.

NO.	DATE	BY	CHKD	DESCRIPTION
1	05/06/16	RJL	RJL	ISSUED FOR PERMITS
2	05/10/16	RJL	RJL	ISSUED FOR SUBMITTAL
3	05/10/16	RJL	RJL	ISSUED FOR SUBMITTAL
4	05/10/16	RJL	RJL	ISSUED FOR SUBMITTAL

WET WEATHER EQUALIZATION BASIN GRADING PLAN PARKING LOT		SHEET C-40
CITY OF PACIFICA DEPARTMENT OF PUBLIC WORKS, WWT PACIFICA, CALIFORNIA		JOB NO. 240001
FREYER & LAURETA, INC. 1000 UNIVERSITY AVENUE, SUITE 200 PACIFICA, CALIFORNIA 94026 TEL: (415) 352-1111 FAX: (415) 352-1112		
SCALE: AS NOTED DESIGNED: RJL DRAWN: RJL CHECKED: MQ PROJ. ENGR: RJL	DATE: 05/06/16 BASIS OF DESIGN 05/10/16 60% SUBMITTAL 10/20/16 90% SUBMITTAL 11/29/16 95% SUBMITTAL 01/20/17 100% SUBMITTAL	
BY:	DESCRIPTION OF REVISIONS:	



- LEGEND**
- DMA BOUNDARY
 - BORENTRON BASIN
 - BETTER MANAGEMENT PRACTICES
 - PAVEMENT
 - LANDSCAPING
 - BORENTRON BASIN AREA
 - BORENTRON BASIN
 - BETTER MANAGEMENT PRACTICES
 - STORMWATER MANAGEMENT AREA
 - MCC
 - MUDS CONTROL CENTER
 - TYPICAL



AREA SUMMARY TABLE

BOUNDARY	CONVENTIONAL SURFACES (SF)				TOTAL (SF)
	PARKING LOT PAVEMENT	STRUCTURES	LANDSCAPE	BORENTRON BASIN	
UNTERSEISED 1					
DMA 1 (OW PARKING LOT)	14280	1880	0	4972	21335
DMA 2 (EE PARKING LOT)	14833	748	455	3985	20218
TOTAL	29113	2408	455	8937	41553

SCALE: AS NOTED

DESIGNED: R.J.L. 06/08/18 80% SUBMITTAL

DRAWN: A.J.R. 10/09/18 80% SUBMITTAL

CHECKED: M.Q. 11/29/18 85% SUBMITTAL

PROJ. ENGR: R.J.L. 9/19/17 100% SUBMITTAL

BY	DATE	DESCRIPTION OF REVISIONS	APP'D

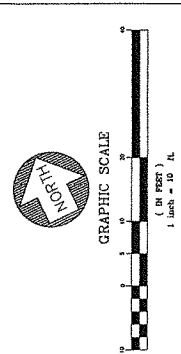
REVERO LAURETA, INC.
 2000 WEST 10TH AVENUE, SUITE 200
 DENVER, CO 80202
 (303) 733-1111
 www.reverolaureta.com

CITY OF PACIFICA
 DEPARTMENT OF PUBLIC WORKS & UTILITIES
 1000 PACIFICA AVENUE, SUITE 100
 PACIFICA, CA 94026
 (415) 485-5000

**WET WEATHER EQUALIZATION BASIN
 STORMWATER MANAGEMENT PLAN
 PARKING LOT**

CITY OF PACIFICA, CALIFORNIA

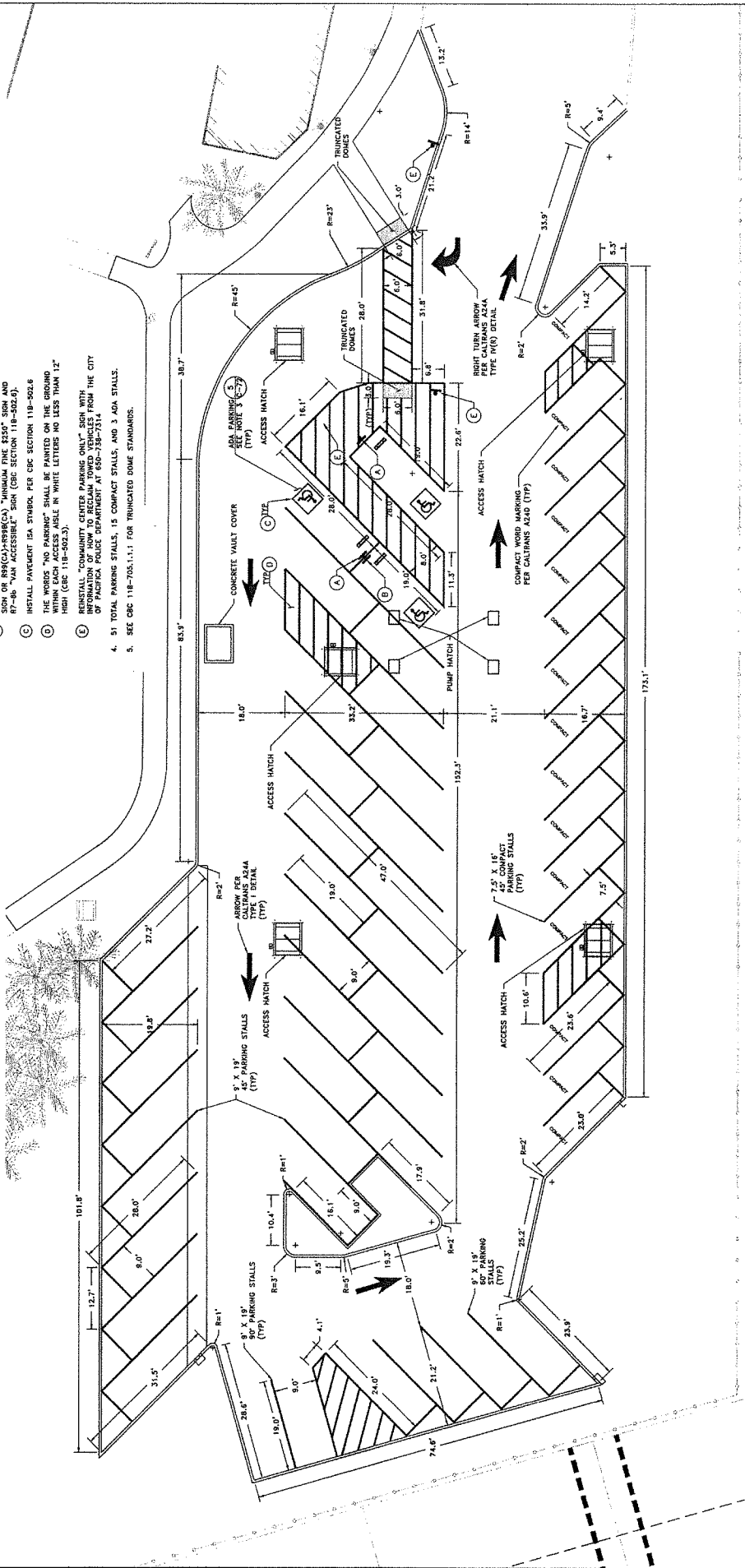
SHEET C-42
 JOB NO. 240001



- LEGEND**
- PARKING LOT CURB
 - PARKING LOT STRIPING
 - TANK
 - EXISTING BUILDING
 - DETAIL CALLOUT
 - TRAFFIC SIGN

- ABBREVIATIONS**
- ADA AMERICANS WITH DISABILITIES ACT
 - R RADIUS
=
 - SD STORM DRAIN
 - TYP TYPICAL

- NOTES**
1. ALL CURB NOT INDICATED TO DEMONSTRATE PARKING STALLS SHALL BE PAINTED RED WITH "FIRE LAKE NO PARKING" STENCILED IN WHITE. SPACED AT 30 FOOT INTERVALS.
 2. CONTRACTOR SHALL MARK ALL STORM DRAIN INLETS (NEW AND EXISTING) WITH "NO DUMPING, DRAINS TO HWY" PLACED IN ACCORDANCE TO CITY OF PACIFICA STANDARDS.
 3. ACCESSIBLE PARKING, REFER TO SHEET C22 DETAIL 5.
 - (A) INSTALL 8' X 15' X 18" CONCRETE VAULT PER (C)118-502.3.1
 - (B) INSTALL 8' X 15' X 18" CONCRETE VAULT PER (C)118-502.3.1
 - (C) INSTALL 8' X 15' X 18" CONCRETE VAULT PER (C)118-502.3.1
 - (D) INSTALL 8' X 15' X 18" CONCRETE VAULT PER (C)118-502.3.1
 - (E) INSTALL 8' X 15' X 18" CONCRETE VAULT PER (C)118-502.3.1
 4. 51 TOTAL PARKING STALLS, 15 COMPACT STALLS, AND 3 ADA STALLS.
 5. SEE CBC 118-705.1.1.1 FOR TRUNCATED DOME STANDARDS.



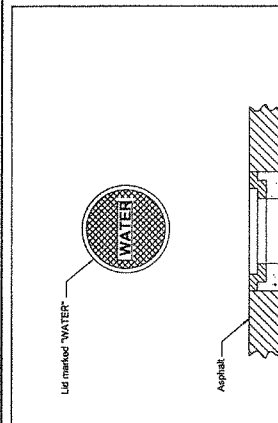
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DESIGNED:	BY:	DATE:	BY:	DATE:		
CHECKED:	BY:	DATE:	BY:	DATE:		
PROJ. ENGR:	BY:	DATE:	BY:	DATE:		

WET WEATHER EQUALIZATION BASIN STRIPING PLAN PARKING LOT

CITY OF PACIFICA, CALIFORNIA

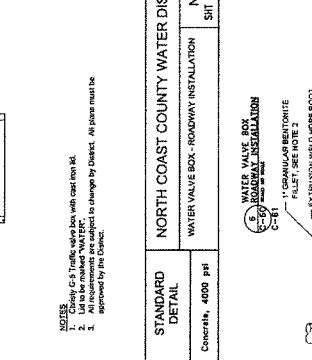
CITY OF PACIFICA
DEPARTMENT OF PUBLIC WORKS & UTILITIES
PACIFICA SANITATION DISTRICT

SHEET C-50
JOB NO. 240001

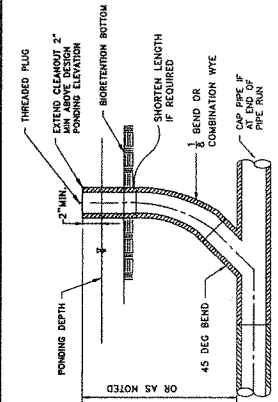


NOTES:
 1. Lid to be marked "WATER".
 2. All reinforcement is subject to change by District. All plans must be approved by the District.

STANDARD
 DETAIL
 NORTH COAST COUNTY WATER DISTRICT
 WATER VALVE BOX - ROADWAY INSTALLATION
 NC-18
 SHEET 1 OF 1
 Concrete, 4000 psi

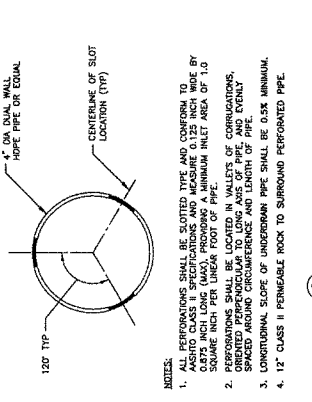


GENERAL NOTES:
 1. WATER VALVE BOXES PREPARED FOR THE USE OF FIELD-FABRICATED BOOTS, CONNECT PREPARED BOOTS TO LINER AND PIPE PER MANUFACTURER'S RECOMMENDATIONS.
 CONSTRUCTION NOTES:
 1. CUT OPENING IN LINER FOR PIPE TO WITHIN 1/2" OF PIPE OUTSIDE DIAMETER.
 2. FULL ANNUAL SPACE WITH 1" MINIMUM GRANULAR BENTONITE FILLET AS SHOWN.
 3. CONTINUOUSLY AROUND PIPE.
 4. PROVIDE CONTINUOUS EXTRUSION WELD AT PIPE BODY/LINER INTERFACE.
 5. FORM BOOT WITH SUFFICIENT MATERIAL TO PREVENT OVERSTRESSING OF LINER.
 6. CONSTRUCT BOOT FROM SAME MATERIAL AS THE LINER.
 7. SEAL CLAMP AND END OF BOOT WITH HEAT SHRINK WRAP. EXTEND HEAT SHRINK WRAP ONE PIPE DIAMETER (MINIMUM) BEYOND CLAMP.

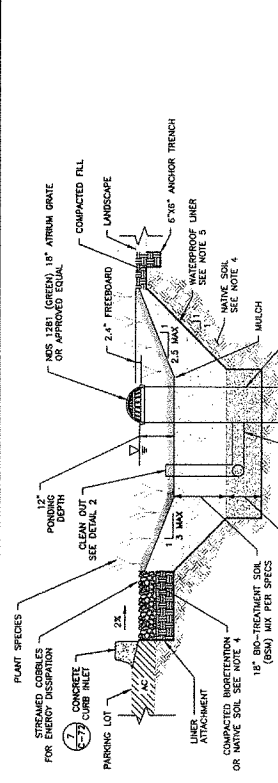


NOTES:
 1. PERFORATIONS SHALL BE SLOTTED TYPE AND CONFORM TO ASSTO CLASS II SPECIFICATIONS AND MEASURE 0.125 INCH WIDE BY 0.075 INCH LONG (MAX). PROVIDE A MINIMUM INLET AREA OF 1.0 SQ. FT. PER PERFORATION.
 2. PERFORATIONS SHALL BE LOCATED IN VALLEYS OF CORRUGATIONS, ORIENTED PERPENDICULAR TO LONG AXIS OF PIPE, AND EVENLY SPACED AROUND CIRCUMFERENCE AND LENGTH OF PIPE.
 3. LONGITUDINAL SLOPE OF UNDERDRAIN PIPE SHALL BE 0.5% MINIMUM.
 4. 12" CLASS II PERMEABLE ROCK TO SURROUND PERFORATED PIPE.

STANDARD
 DETAIL
 NORTH COAST COUNTY WATER DISTRICT
 WATER VALVE BOX - ROADWAY INSTALLATION
 NC-18
 SHEET 1 OF 1
 Concrete, 4000 psi

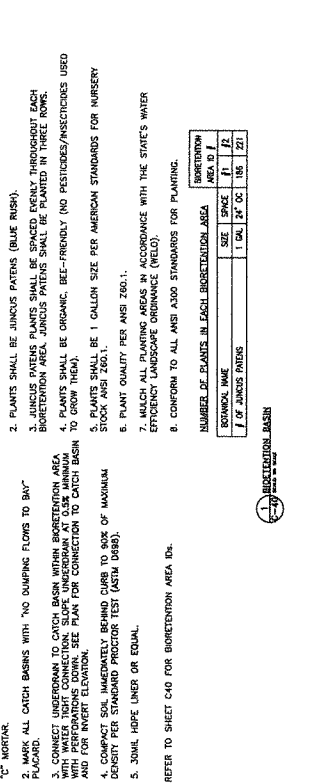


NOTES:
 1. FRAME AND GRATE FOR INGS PRODUCTS OR APPROVED EQUAL.



PLANTING NOTES:
 1. ALL PLANTS AND SOIL LAY WITHIN THE BORONIZATION AREA SHALL BE IN ACCORDANCE WITH APPENDIX A AND K, RESPECTIVELY, OF THE SAN WATED C.3 TECHNICAL GUIDELINES.
 2. PLANTS SHALL BE JUNCUS PATENS (BLUE RUSH).
 3. JUNCUS PATENS PLANTS SHALL BE SPACED EVENLY THROUGHOUT EACH BORONIZATION AREA. JUNCUS PATENS SHALL BE PLANTED IN THREE ROWS.
 4. PLANTS SHALL BE ORGANIC, BEE-FRIENDLY (NO PESTICIDES/INSECTICIDES USED TO GROW THEM).
 5. PLANTS SHALL BE 1 GALLON SIZE PER AMERICAN STANDARDS FOR NURSERY STOCK AND 200.1.
 6. PLANT QUALITY PER ANS 260.1.
 7. MULCH ALL PLANTING AREAS IN ACCORDANCE WITH THE STATE'S WATER EFFICIENCY LANDSCAPE ORDINANCE (WELD).
 8. CONFORM TO ALL ANS AND 300 STANDARDS FOR PLANTING.

NUMBER OF PLANTS IN EACH BORONIZATION AREA
 BORONIZATION AREA (SQ FT) | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 |
 SIZE | 1 GAL | 2 1/2 GAL | 5 GAL | 10 GAL | 15 GAL | 20 GAL | 25 GAL | 30 GAL | 35 GAL | 40 GAL | 45 GAL | 50 GAL |
 # OF JUNCUS PATENS | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 |



NOTES:
 1. FRAME AND GRATE FOR INGS PRODUCTS OR APPROVED EQUAL.

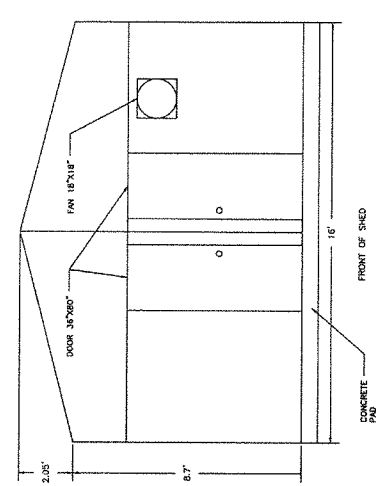
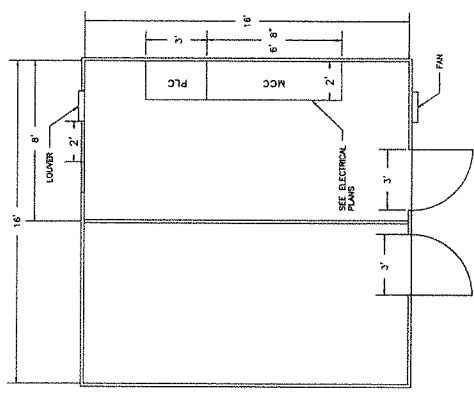


NOTES:
 1. ABOUT ALL PERFORATIONS, SEAMS, CRACKS, AND JOINTS WITH CLASS II LINER.
 2. ALL CATCH BASINS WITH "NO DUMPING FLOW TO BAY"
 3. CONNECT UNDERDRAIN TO CATCH BASIN WITHIN BORONIZATION AREA. WITH PERFORATIONS DOWN TO SLOPE UNDERDRAIN AT 0.5% MINIMUM AND FOR INVERT ELEVATION. BEING CARE TO ROCK OF MAXIMUM DENSITY PER STANDARD PROCTOR TEST (ASTM D698).
 4. 3/8 INCH HOPE UNDER OR EQUAL.
 5. REFER TO SHEET C40 FOR BORONIZATION AREA ID.

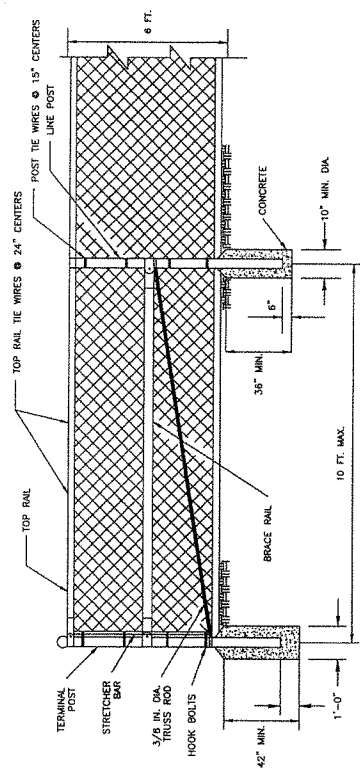
STANDARD
 DETAIL
 NORTH COAST COUNTY WATER DISTRICT
 WATER VALVE BOX - ROADWAY INSTALLATION
 NC-18
 SHEET 1 OF 1
 Concrete, 4000 psi



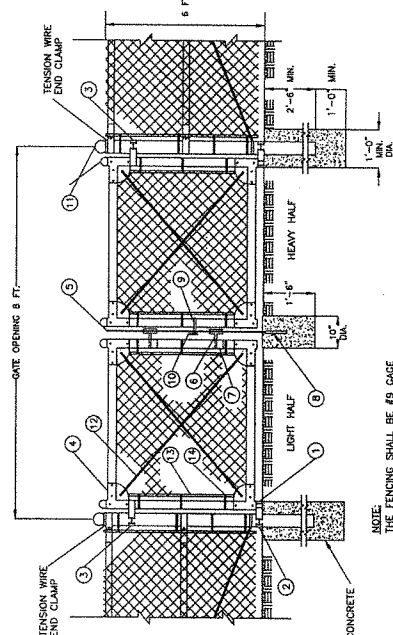
NOTES:
 1. FRAME AND GRATE FOR INGS PRODUCTS OR APPROVED EQUAL.



NOTES:
1. SEE SPECIFICATION SECTION 01 13 40 FOR DEFERRED SUBMITTAL REQUIREMENTS.



1 FENCE DETAIL

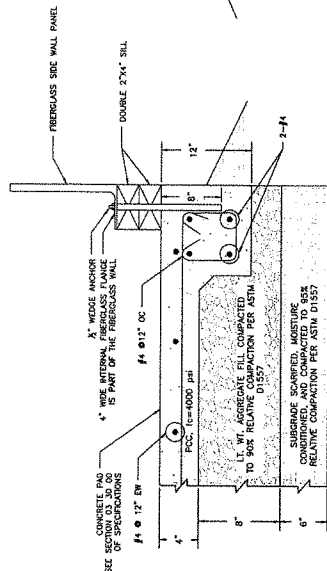


NOTE:
THE FENCING SHALL BE #9 GAGE
CHAIN LINK DIAMOND MESH.
1'-0" MIN. DIA.
2'-0" MIN.
1'-0" MIN.

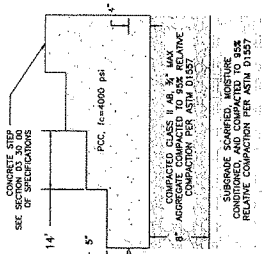
2 GATE DETAIL

PART NO.	DESCRIPTION	QUANTITY
1	STRAIGHT PLUG	2
2	BOTTOM HINGE	2
3	TOP HINGE	2
4	PLUNGER ROD	6
5	LATCH FORK	1
6	FORK CATCH	2
7	PLUNGER ROD CATCH	1
8	LOCK KEEPER	1
9	ORNAMENTAL TOPS	6
10	TRUSS RODS	4
11	STRETCHER BAR	4
12	HOOK BOLTS	4
14		12

3 GATE DETAIL



4 FIBERGLASS SHED CONCRETE SLAB MOUNT



5 CONCRETE STEPS

SCALE: AS NOTED	6/6/13 BASIS OF DESIGN	DATE: 6/6/13	BY: [Signature]	DESCRIPTION OF REVISIONS	APP'D: [Signature]
DESIGNED: R/L	5/29/13 100% SUBMITTAL				
DRAWN: A/R	5/29/13 100% SUBMITTAL				
CHECKED: B/Q	6/10/13 100% SUBMITTAL				
PROJ. ENGR: R/L	6/10/13 100% SUBMITTAL				

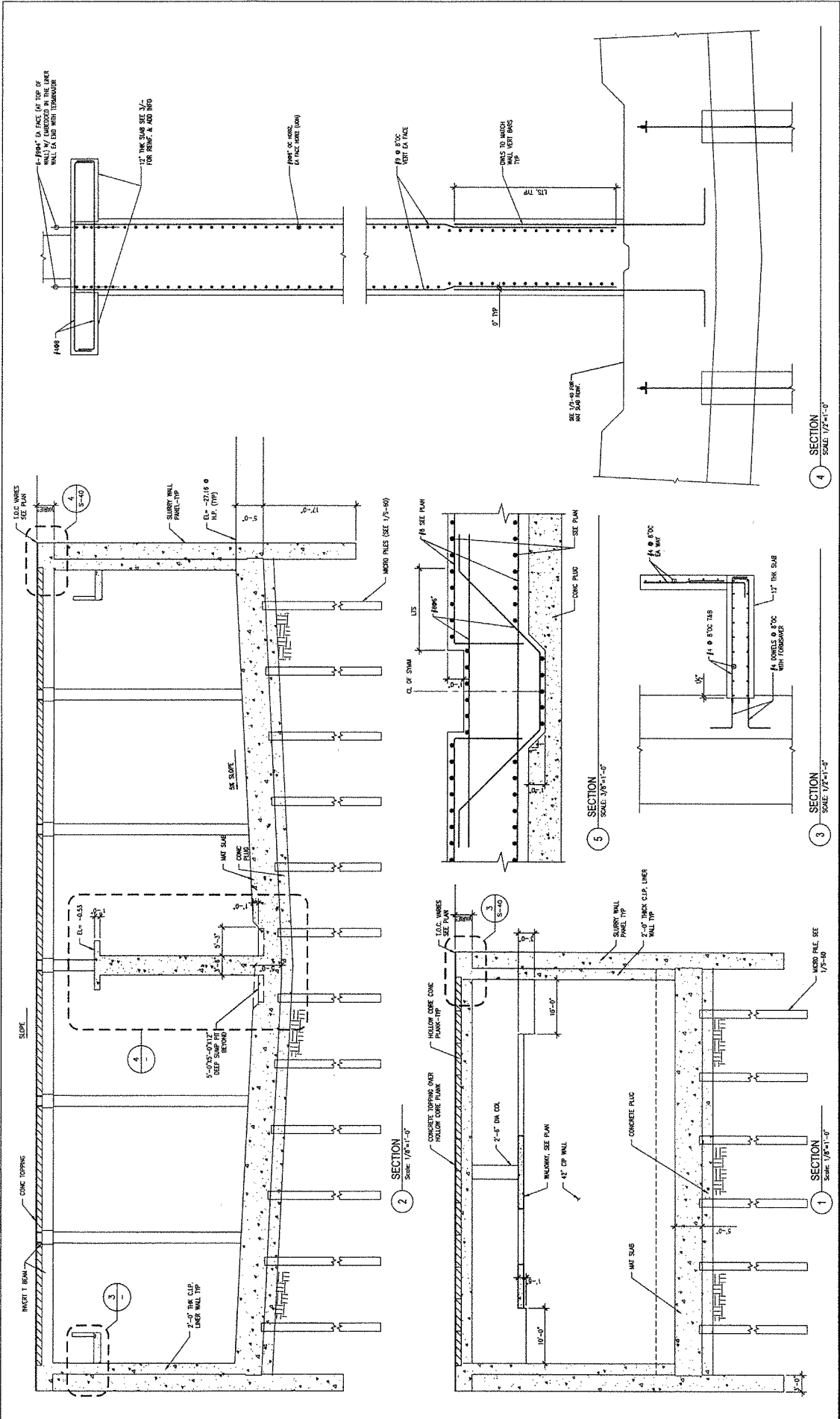
CITY OF PACIFIC
DEPARTMENT OF PUBLIC WORKS, WWT
PACIFICA, CALIFORNIA 94026

**WET WEATHER EQUALIZATION BASIN
DETAIL SHEET**

CITY OF PACIFICA, CALIFORNIA

SHEET
C-78

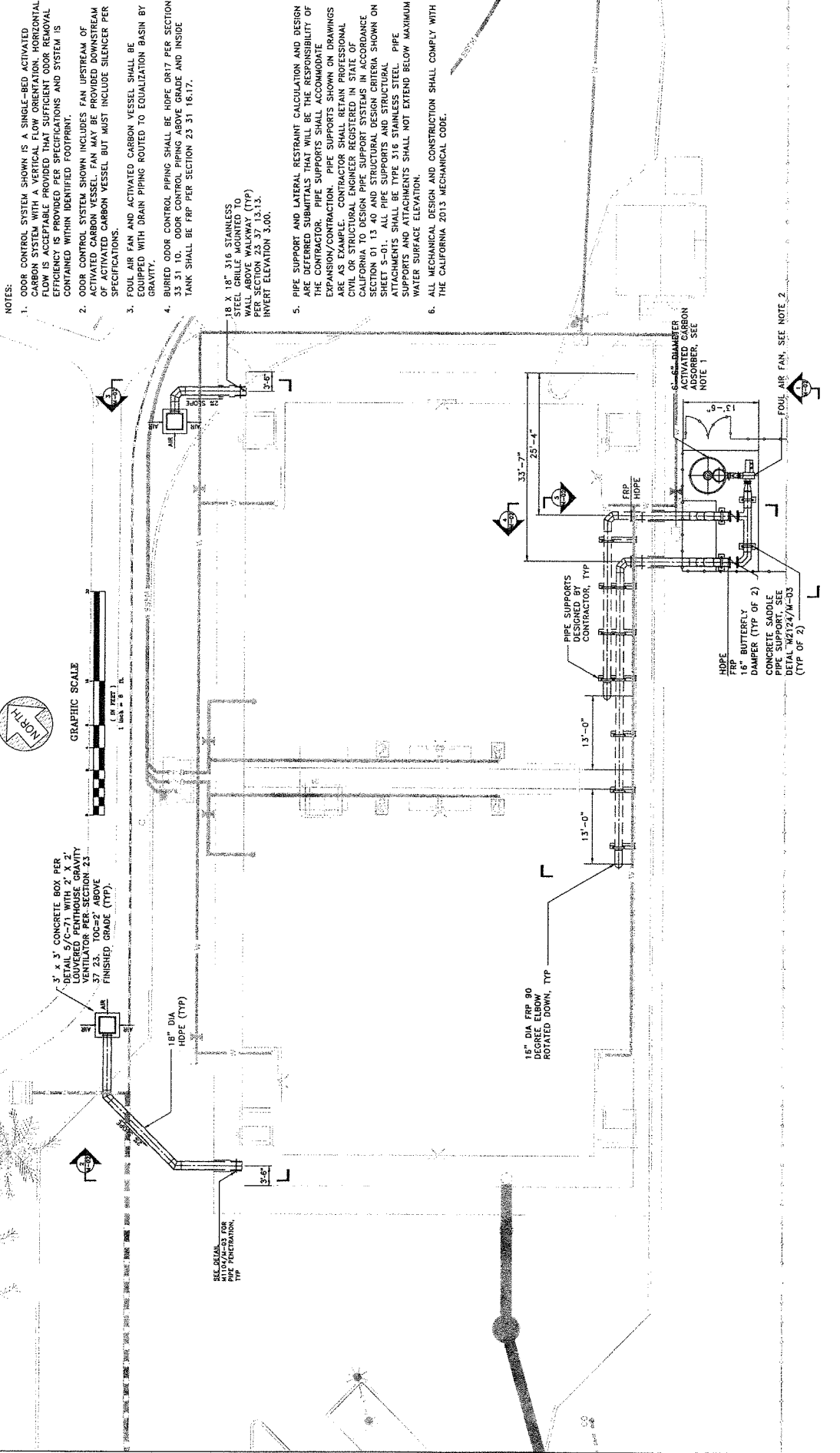
JOB NO.
240001



SCALE:	AS NOTED	DATE	BY	DATE	DESCRIPTION OF REVISIONS	APTD
DESIGNED:	DR	10/02/14	100% SUBMITTAL			
DRAWN:	AC	11/25/14	90% SUBMITTAL			
CHECKED:	DE	12/15/14	100% SUBMITTAL			
PROD. ENGR:	DE	01/15/15	100% SUBMITTAL			

Thornton Tomasetti Thornton Tomasetti, Inc. 707 Wilshire Blvd, Suite 4400 Los Angeles, CA 90017-5818 713.350.7000 / 713.350.7001	City of Pacific DEPARTMENT OF PUBLIC WORKS WET WEATHER EQUALIZATION BASIN	City of Pacific DEPARTMENT OF PUBLIC WORKS WET WEATHER EQUALIZATION BASIN
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FRYER LAURETA, INC. 10000 Wilshire Blvd, Suite 1000 Los Angeles, CA 90024 310.204.1100 / 310.204.1101	WET WEATHER EQUALIZATION BASIN TANK SECTIONS CITY OF PACIFICA, CALIFORNIA	SHEET S-30 JOB NO. 240001
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- NOTES:
1. ODOR CONTROL SYSTEM SHOWN IS A SINGLE-BED ACTIVATED CARBON SYSTEM WITH A VERTICAL FLOW ORIENTATION. HORIZONTAL FLOW IS ACCEPTABLE PROVIDED THAT SUFFICIENT ODOR REMOVAL EFFICIENCY IS PROVIDED PER SPECIFICATIONS AND SYSTEM IS CONTAINED WITHIN IDENTIFIED FOOTPRINT.
 2. ODOR CONTROL SYSTEM SHOWN INCLUDES FAN UPSTREAM OF ACTIVATED CARBON VESSEL. FAN MAY BE PROVIDED DOWNSTREAM OF ACTIVATED CARBON VESSEL BUT MUST INCLUDE SILENCER PER SPECIFICATIONS.
 3. FOUL AIR FAN AND ACTIVATED CARBON VESSEL SHALL BE EQUIPPED WITH DRAIN PIPING ROUTED TO EQUALIZATION BASIN BY GRAVITY.
 4. BURIED ODOR CONTROL PIPING SHALL BE HDPE DR17 PER SECTION 33 31 10. ODOR CONTROL PIPING ABOVE GRADE AND INSIDE TANK SHALL BE FRP PER SECTION 23 31 16.17.
 5. PIPE SUPPORT AND LATERAL RESTRAINT CALCULATION AND DESIGN ARE DEFERRED SUBMITTALS THAT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. PIPE SUPPORTS SHALL ACCOMMODATE EXPANSION/CONTRACTION. PIPE SUPPORTS SHOWN ON DRAWINGS ARE AS EXAMPLE. CONTRACTOR SHALL RETAIN PROFESSIONAL CIVIL OR STRUCTURAL ENGINEER REGISTERED IN STATE OF CALIFORNIA TO DESIGN PIPE SUPPORT SYSTEMS IN ACCORDANCE WITH SECTION 01 13 40 AND STRUCTURAL DESIGN CRITERIA SHOWN ON DRAWINGS. PIPE SUPPORTS SHALL BE TYPE 316 STAINLESS STEEL. ATTACHMENTS SHALL NOT EXTEND BELOW MAXIMUM WATER SURFACE ELEVATION.
 6. ALL MECHANICAL DESIGN AND CONSTRUCTION SHALL COMPLY WITH THE CALIFORNIA 2013 MECHANICAL CODE.

18" X 18" 316 STAINLESS STEEL SCHEDULE 40S LIMITED TYP. MAX ABOVE WALKWAY (TYP) PER SECTION 23 37 13.1.3. INVERT ELEVATION 3.00.

18" DIA FRP 90 DEGREE ELBOW ROTATED DOWN, TYP

13'-0" 13'-0" 13'-0"

33'-7" 25'-4"

PIPE SUPPORTS DESIGNED BY CONTRACTOR, TYP

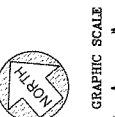
6" DIA FRP 90 DEGREE ELBOW ROTATED DOWN, TYP

6" DIA FRP 90 DEGREE ELBOW ROTATED DOWN, TYP

FOUL AIR FAN, SEE NOTE 2

16" BUTTERFLY DAMPER (TYP OF 2) CONCRETE SADDLE PIPE SUPPORT, SEE DRAWING 17-4718-03 (TYP OF 2)

6" DIA FRP 90 DEGREE ELBOW ROTATED DOWN, TYP



3' x 3' CONCRETE BOX PER DETAIL 57C-71 WITH 2" X 7" VENTILATOR PER SECTION 23 37 23. TOC=2' ABOVE FINISHED GRADE (TYP).

18" DIA HDPE (TYP)

13'-0" 13'-0"

33'-7" 25'-4"

PIPE SUPPORTS DESIGNED BY CONTRACTOR, TYP

16" BUTTERFLY DAMPER (TYP OF 2) CONCRETE SADDLE PIPE SUPPORT, SEE DRAWING 17-4718-03 (TYP OF 2)

6" DIA FRP 90 DEGREE ELBOW ROTATED DOWN, TYP

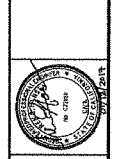
FOUL AIR FAN, SEE NOTE 2

16" BUTTERFLY DAMPER (TYP OF 2) CONCRETE SADDLE PIPE SUPPORT, SEE DRAWING 17-4718-03 (TYP OF 2)

6" DIA FRP 90 DEGREE ELBOW ROTATED DOWN, TYP

SCALE:		AS NOTED	5/29/18	BASIS OF DESIGN	APP'D
DESIGNED:	DM	09/25/18	60% SUBMITTAL		
DRAWN:	FB	11/09/18	90% SUBMITTAL		
CHECKED:	FB	11/29/18	95% SUBMITTAL		
PROJ. ENGR.:	DM	5/21/18	100% SUBMITTAL		
BY	DATE	DESCRIPTION OF REVISIONS		APP'D	

WET WEATHER EQUALIZATION BASIN
ODOR CONTROL SYSTEM PLAN
CITY OF PACIFICA, CALIFORNIA



CITY OF PACIFICA
DEPARTMENT OF PUBLIC WORKS AND UTILITIES
PACIFICA, CALIFORNIA



Brown and Caldwell

FREYER & LAURETA, INC.
10000 S. 100th Ave., Suite 100
Pacifica, CA 94041
Tel: 415.351.1000
Fax: 415.351.1001
www.freyerandlaureta.com

SHEET M-01
JOB NO. 240001

Figure 1-1: Evaluated Site Locations Overview



Figure 2-1: Potential Basin Location and Pipeline Alignment at Site Alternative 2C





CITY OF PACIFICA

WET WEATHER FLOW EQUALIZATION BASIN
PROJECT

FINAL MITIGATED NEGATIVE DECLARATION/
INITIAL STUDY

SCH # 2016122016

FEBRUARY 2017

Prepared for

City of Pacifica
1800 Francisco Boulevard
Pacifica, California 94044

Prepared by

Terraphase Engineering Inc.
1404 Franklin Street, Suite 600
Oakland, California 94612



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INTRODUCTION

Purpose of the Final MND/IS

The Final Mitigated Declaration/Initial Study (MND/IS) is an informational document prepared by the City of Pacifica (City), the Lead Agency. This document includes written public comment letters on the Draft MND/IS and presents responses to the written public comments, and as necessary, makes corrections and clarifications to the Draft MND/IS. This document, together with the Draft MND/IS, constitutes the Final MND/IS for the Project. Due to its length, the text of the Draft MND/IS is not included with this document but is included by reference as part of the Final MND/IS. The City has prepared this document pursuant to Sections 15070 – 15074 of the CEQA Guidelines which address the preparation of Negative and Mitigated Negative Declarations. Furthermore, the governing body of the City, the Pacifica City Council, along with the Pacifica Planning Commission, will consider both this document and the Draft MND/IS in their determinations on environmental certification.

Public Resources Code Section 21081.6(a) requires the Lead Agency to adopt a monitoring or reporting program for projects subject to the preparation of Environmental Impacts Reports (EIRs) or MNDs. The Draft Mitigation Monitoring and Reporting Program Checklist (MMRP) for the Project is included in Appendix E of the Draft IS.

No New Significant Information

If significant new information is added to a Draft MND/IS after notice of public review has been given, but before adoption of the Final MND/IS, the lead agency must issue a new notice and recirculate the Draft MND/IS for further comment and consultation.

Although this document contains additions and clarifications to information presented in the Draft MND/IS, none of these additions and clarifications constitute a “substantial revision” as defined under Section 15073.5 of the CEQA Guidelines, defined as:

- A new avoidable significant effect is identified and mitigation measures or project revisions must be added in order to reduce the effect to insignificance.
- The lead agency determines that the proposed mitigation measures or project revisions will not reduce potential effects to less than significance and new measures or revisions must be required.

Information presented in this document support the City’s determination that recirculation of the Draft MND/IS is not required because:

- Revisions to mitigation measures are more effective as revised pursuant to Section 15074.1 of the CEQA Guidelines.

- Revisions to the Project do not represent new avoidable significant effects.
- New information is added to clarify the Project based on community input.

Organization of this Final MND/IS

This Final MND/IS contains information about the proposed Project, supplemental environmental information, and responses to comments raised during the public review and comment period on the Draft MND/IS. Following this Introduction, the document is organized as described below.

Project Summary: summarizes the proposed Project, potential environmental impacts, and recommended mitigation measures.

List of Commenters on Draft MND/IS: lists public agencies and individuals that submitted written comments on the Draft MND/IS during the public review and comment period.

Written Comments on the Draft MND/IS and Responses to these Comments: contains the comment letters received on the Draft MND/IS and presents individual responses to the specific CEQA-related comments raised.

Revisions to the Draft MND/IS: contains text changes and corrections to the Draft MND/IS initiated by the City (as the Lead Agency) or resulting from comments received on the Draft MND/IS.

PROJECT SUMMARY

Summary of the Project

Project Description and Location

The City intends to construct and utilize the proposed wet weather flow equalization basin (“the EQ basin”) and associated pipelines (together referred to as “the Project”) as a key element to mitigate storm-related sanitary sewer overflows (SSOs) in the City’s wastewater collection system and reduce peak wet weather flows to the City’s Calera Creek Water Recycling Plant. The City’s sanitary sewer collection system is subject to infiltration and inflow (I/I) of extraneous groundwater and stormwater into the system, resulting in high wet weather flows during storm events. As a result, SSOs have occurred at several locations in the system during large storms.

The Project would include the installation of a 2.1-million-gallon capacity EQ basin, two diversion structures to passively divert excess flows from the existing Linda Mar Boulevard and Arguello Boulevard sanitary sewer lines and transport the flow via a conveyance pipeline to the EQ basin during storm events, an effluent conveyance pipeline routing flows to the existing Crespi Drive sanitary sewer line and the Linda Mar Pump Station, a 10-foot-tall motor control center (MCC) building equipped with a 4-foot-high metal antenna that would allow radio communication with the City’s Calera Waste Water Treatment Plant, a ventilation and odor-control system, and a potable-water-supplied cleaning system within the EQ basin.

The EQ basin would be located within the parking lot for the Skatepark and immediately southwest of the Community Center (located at 540 Crespi Drive) on the east side of SR-1. The property for the proposed EQ basin is owned by the City, zoned for Controlled Manufacturing, and is identified as Public Facilities in the City’s General Plan.

The proposed EQ basin construction site is bounded by open space areas to the east and west, residential parcels approximately 80 feet to the southwest, and the Skatepark and Community Center complexes to the northwest and northeast, respectively. There is an open, vegetated drainage swale between the southwestern end of the parking lot and the nearby residences.

The proposed two diversion structures and conveyance pipelines to and from the EQ basin would be constructed in City-owned rights-of-way or public utility easements.

During construction of the EQ Basin, the Skatepark parking lot would be closed to the public, and normal parking in this area would be diverted to the Crespi Parking Lot west of the Community Center. Following completion of EQ basin construction, the Skatepark parking lot would be reconstructed above the EQ basin structure to provide, at minimum, the same number of parking spots as in the existing parking lot.

Project construction is anticipated to occur during a 17-month period starting in May 2017 and ending in late September 2018. Construction hours would be limited to the following:

- Within the EQ basin area (Skatepark parking lot area including the excavation for the jacking and receiving pits): 8:00 a.m. to 5:00 p.m. Monday through Friday
- Along City streets (except at the intersection of Linda Mar Boulevard and De Solo Drive): 9:00 a.m. to 4:00 p.m.
- At the intersection of Linda Mar Boulevard and De Solo Drive: 9:00 a.m. to 3:00 p.m.

Weekend and night work is not anticipated.

CEQA Process and Schedule

The City has conducted several public meetings in order to involve the community in the EQ basin site evaluation and selection process. The following provides a summary of the public meetings conducted, and notices transmitted, to date:

- Presentation of the EQ Basin Project and potential site locations at the City Council Chambers (August 14, 2013). Notices for the meeting were distributed on July 29, 2013, to the residences within a 300-foot radius of each of the four potential EQ basin sites. The purposes of the meeting were to: (1) provide an overview of the Project, (2) discuss the possible locations of the EQ basin, and (3) obtain public input on the Project and the possible locations. Approximately 20 community members attended the meeting.
- Presentation of the Draft Feasibility Study findings to the City Council (March 23, 2015). Notices for the meeting were distributed on March 18, 2015, to the residences within a 300-foot radius of each of the four potential EQ basin sites. The meeting was also posted on the City's website under the City Council and Wastewater categories at least seven days prior to the meeting. The purpose of the meeting was to present the findings of the Feasibility Study to the City Council, and to allow the community an opportunity to provide comments.
- Presentation of the Final Feasibility Study findings to the City Council (September 28, 2015). The meeting was posted on the City's website under the City Council and Wastewater categories at least seven days prior to the meeting. The purposes of the meeting were to present staff's final preferred site alternative for the EQ Basin Project and for the City Council to approve and direct staff to move forward with the necessary processes for a successful completion of the Project. The meeting also provided the community with an opportunity to provide comments.
- Presentation of the Wet-Weather Equalization Basin Project to the City Council (December 12, 2016). Notices for the meeting were distributed on November 28, 2016, to the residences within a 300-foot radius of the proposed EQ basin site. On November 30, 2016, Mr. Louis Sun and Ms. Maria Aguilar of the City of Pacifica Public Works Department visited the two residences on Anza Boulevard which border the utility easement to notify them of the presentation to the City Council. Informational letters were left at each residence. The

purposes of the meeting were to present a status update on the EQ basin and for the City Council to request any additional information on the Project.

- Public Meeting on the Draft MND/IS (December 15, 2016). An information sheet was posted to the City's website, NextDoor, and at Project site, and multiple times on the Connect with Pacifica e-newsletter. The Notice of Intent, which included the meeting information, was distributed on December 6, 2016, to the residences within a 300-foot radius of the proposed EQ basin site and the pipeline alignments. The Notice of Intent was published in the San Mateo County Times and at the San Mateo County Clerk's office on December 8, 2016. The purpose of the meeting was to present the findings of the Draft MND/IS and provide the community with an opportunity to ask question and provide written comments. During the public meeting, participants were notified that comments would not recorded as formal comments, and participants were invited to fill out comment forms during the meeting to ensure that any comments were fully addressed. The general themes of questions and comments raised during the meeting were noted as summarized below.
 - a. Questions related to the practicability of the solution to meet the requirements of the requirements of the Cease and Desist Order (CDO), and discussion of alternative solutions that had been evaluated by the City. The City summarized the findings of the feasibility study which evaluated multiple options to address the CDO.
 - b. Clarification regarding construction methods and operation of the basin and pipelines. The City provided additional detail on the construction methods for the Project, and how the Project would operate (i.e., during storm events).
 - c. Comments regarding flood zone impact on neighborhoods (written comment provided). See Response to Comment Letter 4.
 - d. Concerns regarding noise and odor and noise associated with the operation of the Project. The City clarified that operational noise and odor would occur only when the EQ basin was in use (estimated to be up to 5 times per year). The City described the odor control system included in the Project design. Construction-related noise would be below significance thresholds with mitigation.
 - e. Traffic impacts to the pick-up/drop-off schedule at Cabrillo School to the north of the Project area. Cabrillo Elementary School has a start time of 8:20 am and end times of 1:20 (Kindergarten), 2:20 (first through third grades), and 2:45 (fourth through eighth grades) on Monday, Tuesday, Thursday, and Friday (depending on grade level), and end times of 1:10 pm (Kindergarten through third grades) and 1:20 pm (fourth through eighth grades) on Wednesday and minimum days.¹ Mitigation Measure TRANS-1 would limit the arrival of trucks onsite to after 8:00 am and would prevent trucks from leaving the site prior to 9:00 am. The Project site would only be able to accommodate an estimated two to three vehicles at a time; therefore, no more than three trucks would

¹ Pacifica School District. 2016. Cabrillo School Family Handbook 2016-2017.
http://pacificasd.org/files/user/6/file/2016-17%20Cabrillo%20Student%20Handbook%2010_16.pdf

be anticipated to be traveling along Crespi Drive between 8:00 am and 9:00 am, when morning school traffic would be present.

Additionally, Mitigation Measure TRANS-1 would prevent trucks from leaving the site after 3:00 pm. It is anticipated that trucks would be filled one at a time and would leave immediately upon filling, if within the allowed timing of Mitigation Measure TRANS-1. The timing associated with filling a truck, covering the load, and signing waste manifests would be approximately 30 minutes. Therefore, afternoon school traffic would likely experience one to two trucks leaving the site during a 30-minute period. In addition to the limitation on truck traffic timing, MM TRANS-1 includes requirements for traffic control personnel such as flaggers during disruptions to the City rights-of-way. The presence of two to three trucks between 8:00 and 9:00 am and two trucks during a half-hour period in the afternoon may cause a slight delay to school traffic; however, the impact would be short-term, temporary, and would not be considered significant with the implementation of Mitigation Measure TRANS-1.

- f. Concern regarding the potential wetland area to the west of the EQ basin site. The Project would not encroach into this area.

The purpose of the City's community engagement effort was to inform the public about the Project and its environmental review process and to receive comments from the community regarding their concerns about the environmental effects of the Project.

On December 8, 2016, the City issued a Notice of Intent to Adopt a Mitigated Negative Declaration for the Wet Weather Flow Equalization Basin Project. The public review and comment period on that Draft MND/IS ended on January 13, 2017. The Final MND/IS will be presented to the City of Pacifica Planning Commission for adoption on February 6, 2017.

Summary of Impacts and Mitigation Measures

The summary table presented below identify the potentially significant impacts and recommended mitigation measures that would reduce the potentially significant impacts to less than significant. The City agreed to incorporate the recommended mitigation measures identified in the Draft Initial Study. Thus, a Mitigated Negative Declaration was prepared for the proposed Project in conformance with Public Resources Code Section 21080.

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
Mitigation Measures			
Initial Study Section 3 - Air Quality			
During Project construction, heritage trees would be removed.	Potentially Significant	<p>AES-1: The Tree Protection Plan prepared by the City (or designee) shall include a schedule and replacement ratio for heritage tree removal. The Tree Protection Plan shall require that replacement trees shall be placed as closely as feasible to the removal sites in order to return aesthetics to pre-project conditions.</p> <p>AQ-1: The Project's general contractor and their subcontractors shall implement basic measures to control dust and exhaust during construction, as recommended by the Bay Area Air Quality Management District (BAAQMD). During any construction period with ground disturbance, the Project's general contractor and their subcontractors shall implement the following BAAQMD-recommended best management practices (BMPs):</p> <ol style="list-style-type: none"> All exposed non-hardscaped surfaces (e.g., temporary parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered appropriately to maintain a damp condition. All haul trucks transporting soil, sand, or other loose material offsite shall be covered. All visible mud or dirt tracked onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping or washing down paved streets using potable water is prohibited. All vehicle speeds on unpaved roads shall be limited to 15 miles per hour. All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used. Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage with these requirements shall be provided for construction workers at all access points. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation. A publicly visible sign shall be posted with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. The BAAQMD's phone number shall also be visible to ensure compliance with applicable regulations. 	Less than Significant
During Project construction, grading and soil disturbing activities would generate fugitive dust.	Potentially Significant		Less than Significant

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
<p>Initial Study Section 4 - Biological Resources</p> <p>Project construction could impact biological resources including candidate, sensitive, or special status species, riparian habitat or sensitive natural community, and wetlands.</p>	<p>Potentially Significant</p>	<p>BIO-1: The City shall implement the following measures or designate implementation of these measures to the Project's general contractor prior to construction:</p> <ol style="list-style-type: none"> 1. Post signage indicating that travel and parking of vehicles and equipment must be limited to pavement, existing roads, and previously disturbed areas. 2. Include provisions in the Project Plans detailing the areas that have been found to be acceptable for disturbance (i.e., previously disturbed areas and those within the Biological Study Area [BSA]). The contractor shall not disturb or remove vegetation outside of these areas. Work areas that would be temporarily impacted by construction would be restored with respect to pre-existing contours and conditions upon completion of work. Restoration work including re-vegetation and soil stabilization shall be evaluated upon completion of work and performed as needed. 3. The City shall ensure that the Stormwater Pollution Prevention Plan (SWPPP) prepared and implemented for the project shall include measures that: <ul style="list-style-type: none"> • minimize erosion and/or prevent water-borne silt from being deposited in adjacent undeveloped areas; • prevent waste and/or construction materials from getting into the adjacent undeveloped areas; and • control and prevent the discharge of all potential pollutants, including hazardous materials, solid wastes, paints, concrete, petroleum products, chemicals, wash water or sediment, and non-stormwater discharges to adjacent undeveloped areas via storm drains, water courses, or sheet flow. <p>BIO-2: The City shall implement the following measures or designate implementation of these measures to the Project's general contractor:</p> <ol style="list-style-type: none"> 1. A qualified biologist shall assist with the placement of wildlife exclusion fencing, and verify that stormwater protection measures to protect adjacent undeveloped areas are in place prior to construction. The biologist shall be provided the contact information of the Project's general contractor Qualified SWPPP Developer (QSD), and vice versa. 	<p>Less than Significant</p>

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
		<p>2. Before the contractors, their employees, or any persons start any work onsite, each worker shall participate in an employee education program, consisting of a brief presentation to explain biological resources on the Project site, which shall be conducted by a qualified biologist. The program shall include the following:</p> <ul style="list-style-type: none"> a. a description of relevant special-status species, nesting birds, and bats along with their habitat needs as they pertain to the BSA; b. a report of the occurrence of these species in the Project vicinity, as applicable; c. an explanation of the status of these species and their protection under the federal and state regulations; d. a list of measures being taken to reduce potential impacts to natural resources during project construction and implementation; e. instructions if a special-status species is found onsite; and f. a summary of the personal consequences of violating state and federal law related to these species. <p>A fact sheet conveying this information shall be prepared and distributed to the above-mentioned people and anyone else who may enter the work areas within the BSA. Upon completion of training, employees shall sign a form stating that they attended the training and agree to all the conservation and protection measures. The training logs shall be provided to the City on a monthly basis.</p> <p>3. A qualified biologist shall conduct a pre-construction survey within the Project area for the presence of the California red-legged frog (CRLF) and/or the San Francisco garter snake (SFGS). The survey shall be conducted immediately prior to the initial onset of Project activities. If any special-status species are found, work shall not commence until the appropriate state and/or federal resource agencies are contacted and avoidance and mitigation measures are in place.</p> <p>4. Within the disturbed areas of the BSA, all burrows that can be occupied by CRLF and SFGS shall be hand-excavated with extreme caution by a qualified biologist in possession of a scientific collecting permit. At the first indication of CRLF or SFGS present, excavation shall immediately cease, and the United States Fish and Wildlife Service (USFWS) and California Department of Fish and Wildlife (CDFW) shall be consulted before any further actions are taken. Because handling and other take of SFGS, a Fully Protected Species, cannot be authorized under California Fish and Game Code, full avoidance must be achieved through measures that would allow the species to passively vacate the site.</p>	

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
		<p>5. A wildlife exclusion/environmental fence (with escape routes, such as exit funnels) shall be erected around active construction areas to prevent the movement of animals into active construction areas under oversight by a qualified biologist. During construction, the fence shall be checked every day before construction activities commence for damage, breaks, or trapped wildlife. Any damage to the fence shall be repaired in a timely manner. The qualified biologist overseeing the placement of wildlife exclusion fencing shall ensure placement of the fence so that a minimum 3-foot-wide wildlife corridor remains open between the south end of the Skatepark parking lot and the residential fences.</p> <p>6. A qualified biologist shall inspect the area inside of the fence for CRLF and SFGS every day before construction activities commence. If any special-status species are found, construction activities shall not be allowed to start and the USFWS and CDFW shall be consulted on an appropriate course of action. Such action could include leaving the animal alone to move away on its own.</p> <p>7. When construction and construction-related activities (including, but not limited to, mobilization and staging, clearing, grubbing, vegetation removal, fence installation, demolition, and grading noise) occur during the avian nesting season (from February 1 to August 31 for passerines and January 1 to September 15 for raptors), all suitable habitats located within the Project's area of disturbance, including staging and storage areas plus a 250-foot (passerines) and 1,000-foot (raptor nests) buffer around these areas, shall be thoroughly surveyed, as feasible, for the presence of active nests by a qualified biologist no more than five days before commencement of any site disturbance activities and equipment mobilization. If project activities are delayed by more than five days, an additional nesting bird survey shall be performed. Active nesting is present if a bird is building a nest, sitting in a nest, a nest has eggs or chicks in it, or adults are observed carrying food to the nest. The results of the surveys shall be documented and provided to the City's Planning Department.</p> <p>If pre-construction nesting bird surveys identify potential impacts to active nests, no site disturbance and mobilization of heavy equipment shall take place within a buffer determined by the biologist in consultation with a CDFW biologist. During that consultation, it can also be determined what low-impact construction activities are allowed within the buffer. The buffer shall be in place until the chicks have fledged. Monitoring shall be required to ensure compliance with the Migratory Bird Treaty Act (MBTA) and California Fish and Game Code. Monitoring dates and findings shall be documented and provided to the City's Planning Department.</p>	

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
		<p>8. A preconstruction survey of trees within the developed and disturbed areas of the BSA shall be conducted by a qualified biologist for colony bat roosts within 14 days prior to the onset of project activity, and the survey shall be documented and provided to the City's Planning Department. If an occupied maternity or colony roost is detected, the CDFW shall be consulted to determine appropriate measures, such as bat exclusion methods, if the roost cannot be avoided.</p> <p>9. Food items may attract wild animals onto the construction site, which would expose them to construction-related hazards. The construction site shall be maintained in a clean condition. All trash (e.g., food scraps, cans, bottles, containers, wrappers, cigarette butts, and other discarded items) shall be placed in closed containers and properly disposed of.</p> <p>10. If an animal is found at the work site and is believed to be a protected species, work must be halted and the project biologist contacted for guidance. Care must be taken not to harm or harass the species. No wildlife species shall be handled and/or removed from the Project area by anyone except qualified biologists in consultation with CDFW/USFWS as appropriate and in accordance with applicable laws and regulations.</p> <p>11. The Project specifications, the contractor's work plan, and/or SWPPP shall include provisions to ensure that the use of monofilament netting, including its use in temporary and permanent erosion control materials, is avoided altogether. All holes greater than 1 foot deep must be sealed overnight to prevent the entrapment of wildlife. Where holes or trenches cannot be sealed, escape ramps that are no greater than 30% slope shall be positioned such that entrapped wildlife would be able to escape. The escape ramps should be at least 1 foot wide and covered/fitted with a material that provides traction.</p>	
<p>Initial Study Section 5 - Cultural Resources</p> <p>During Project construction, it is possible that unknown prehistoric, historic, or paleontological resources and human remains could be disturbed.</p>	<p>Potentially Significant</p>	<p>CUL-1: In the event that any prehistoric, historic, or paleontological materials are discovered during ground-disturbing activities, ground work shall cease at the Project site and discovered materials and their surroundings shall not be altered or collected. A qualified archaeologist or paleontologist shall be consulted as to the significance of the find, and avoidance measures or appropriate mitigation shall be completed according to CEQA guidelines. Significant cultural materials recovered shall be subject to scientific analysis and/or professional museum curation, and a report shall be prepared by the qualified archaeologist according to current professional standards. Ground disturbance activities shall continue upon direction from the qualified archaeologist according to current professional standards.</p> <p>CUL-2: Prior to performing ground disturbance activities, all onsite workers shall be trained by a City-approved archaeologist in what cultural resources are, identifying cultural resources, the procedure if a cultural resource is found, and their legal responsibility to protect cultural resources. Training logs shall be provided to the City regularly.</p>	<p>Less than Significant</p>

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
<p>Initial Study Section 8 - Hazards and Hazardous Materials</p> <p>During Project construction, it is possible that contaminated soil or groundwater could be encountered.</p>	<p>Potentially Significant</p>	<p>HAZ-1: The City shall ensure that:</p> <p>(a) The construction contractor (or designee) shall conduct a private utility survey for the presence of underground utilities, fill pipes, and underground storage tanks (USTs) prior to excavation within the Skatepark parking lot area, areas of the proposed jacking and receiving pits, and along the proposed locations of the influent and effluent conveyance pipelines. In the event that a UST is identified within the excavation boundaries, the City shall contact an environmental consultant, who shall perform and/or coordinate the investigation for the presence of contamination in accordance with applicable regulations. A report of the findings of the geophysical survey shall be submitted to the City's Construction Manager and City's Public Works Department liaison.</p> <p>(b) The Project's general contractor shall require that all site workers (including subcontractors) be trained in identifying contaminated soil and/or groundwater. In the event that contaminated soil or groundwater is encountered (either visually or through odor detection) during excavation activities, the construction contractor shall follow the procedures below:</p> <ul style="list-style-type: none"> • Stop work in areas with suspected contamination; • Immediately report observations to the City's Construction Manager; • Contact an environmental consultant, who shall perform and/or coordinate the investigation of suspected contamination in accordance with applicable regulations. <p>(c) If necessary, based on the findings of the environmental consultant, the City's Public Works Department shall notify San Mateo County Environmental Health Department, San Francisco Bay RWQCB, and/or the California Department of Toxic Substances Control (DTSC).</p> <p>(d) If investigation confirms presence of contamination, the environmental consultant shall perform and/or coordinate appropriate site investigation and cleanup procedures in accordance with regulatory requirements, including the appropriate segregation and disposal of contaminated soil and groundwater. Once the extents of the contamination have been delineated and the contaminated materials (i.e., soil and/or groundwater) have been excavated or otherwise remediated (e.g., in-situ treatment), ground-disturbing activities shall continue.</p>	<p>Less than Significant</p>

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
<p>During Project construction, hazardous materials would be stored and used on the Project site.</p>	<p>Potentially Significant</p>	<p>HAZ-2: The Project's general contractor and their subcontractors shall be required to use BMPs to minimize the potential for releases of hazardous materials to groundwater, surface water, and soil. The BMPs shall be outlined in the general contractor's SWPPP document that will be prepared by their QSD submitted to the City's Construction Manager and uploaded to the State Water Quality Resource Board's (SWRCB) SMART database as required under the SWCRB's General Permit for Construction Activities. The SWPPP shall include BMPs that accomplish the following:</p> <ul style="list-style-type: none"> • Discussion of methodology and available technology for waste management and materials pollution control, as well as other construction-related activities. • Provisions for training the site workers on the proper storage and handling of hazardous substances, such as fuels, lubricants, paints, and solvents. Training logs shall be provided to the City regularly. • A process for responding to, and tracking, complaints pertaining to construction activity, including identification of the City's Construction Manager. The Construction Manager shall determine the cause of the complaints and shall take prompt action to correct the problem. The City's Planning and Zoning Department shall be informed who the City's Construction Manager is prior to the issuance of the first permit issued by Building Services. • Provision for accommodation of pedestrian flow and prevention of any unauthorized personnel from entering the construction zone or material and equipment storage areas. • Prior to construction, a portable toilet facility and a debris box shall be installed on the site and properly maintained through project completion. 	<p>Less than Significant</p>

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
<p>Initial Study Section 9 - Hydrology and Water Quality</p> <p>During Project construction, the Project could violate water quality standards or waste discharge requirements if dewatering or stormwater management is not properly implemented.</p>	<p>Potentially Significant</p>	<p>HYDRO-1: The general contractor shall develop a dewatering plan and obtain any necessary permits for performing dewatering. The dewatering plan shall include methods to manage the potential environmental impacts that dewatering activities might have. The dewatering plan shall include the following at a minimum:</p> <ul style="list-style-type: none"> • a pre-dewatering topographic survey with a minimum vertical accuracy of 0.01 foot (if the existing site topographic survey already prepared for design purposes provides a minimum vertical accuracy of 0.01 foot, this survey can be utilized and an additional topographic survey would not be required); • a photographic survey of structures and flatwork in the surrounding area documenting any pre-dewatering damage to the structures or flatwork, including measurements of the widths and lengths of any significant cracks in the structures or flatwork; • pre-construction evaluation of required groundwater extraction rates and volumes, calculation of the radius of influence of the dewatering wells/sumps and anticipated settlements as a function of distance from the excavation; • measures to address situations where water resource impacts or excessive settlements are occurring. <p>Monitoring of groundwater levels in the piezometer PZ-1 that is located in the vicinity of the proposed excavation zone shall be required to verify the assumptions used to calculate potential settlements.</p>	<p>Less than Significant</p>

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
<p>Initial Study Section 12 - Noise</p> <p>During Project construction and operation of the blower, noise levels from the Project site would increase the ambient noise levels in the project vicinity above current levels, or thresholds identified in the City's Draft General Plan.</p>	<p>Potentially Significant</p>	<p>NOISE-1: No construction activities shall be permitted on the weekends or at night. To reduce construction noise levels emanating from the site and minimize disruption and annoyance of existing noise-sensitive receptors in the Project vicinity, the City shall require the selected contractor to develop a Noise Control Plan. This Noise Control Plan shall include, but not be limited to, the following construction BMPs:</p> <ul style="list-style-type: none"> • All equipment driven by internal combustion engines shall be equipped with mufflers that are in good condition and appropriate for the equipment. • The construction contractor shall utilize "quiet" models of air compressors and other stationary noise sources where technology exists. • Unnecessary idling of internal combustion engines shall be prohibited. • Construction staging areas shall be established at locations that would create the greatest distance between the construction-related noise sources and receptors nearest the Project site during all Project construction, as feasible. • Locate stationary noise sources as far from receptors as feasible. If they must be located near receptors, adequate muffling (with enclosures, where feasible and appropriate) would be used as necessary to comply with local noise ordinance and general plan limits. Any enclosure openings or venting would face away from receptors. • Locate material stockpiles, as well as maintenance/equipment staging and parking areas, as far as feasible from residential receptors. • Neighbors located adjacent to the construction site shall be notified of the construction schedule in writing. • Designate a project liaison who shall be responsible for responding to noise complaints during construction. The name and phone number of the liaison shall be conspicuously posted at construction areas and on all advance notifications. This person shall take steps to resolve complaints, including periodic noise monitoring, if necessary. Results of noise monitoring shall be presented at regular project meetings with the Project contractor, and the liaison shall coordinate with the contractor to modify, to the extent feasible, any construction activities that generated excessive noise levels. • Require a reporting program that documents complaints received, actions taken to resolve problems, and effectiveness of these actions. • Hold a preconstruction meeting with the job inspectors and the general contractor's onsite project manager to confirm that noise mitigation and practices (including construction hours, construction schedule, and noise coordinator) are completed. 	<p>Less than Significant</p>

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
<p>Initial Study Section 14 - Public Services</p> <p>During Project construction, road closures could impact emergency vehicle routes.</p>	<p>Potentially Significant</p>	<p>PUB-1: At least 48 hours prior to road closures, the City's Public Works Department (or designee, such as the City's Consultant Construction Manager) shall notify local emergency service providers (Pacifica Police Department at 650-738-7314 and North County Fire Authority at 650-991-8138) of road closures and length of closure. The construction contractor shall obtain encroachment permits from the City's Engineering Department prior to construction-related lane or street parking closures.</p>	<p>Less than Significant</p>
<p>Initial Study Section 16 - Transportation and Circulation</p> <p>During Project construction, construction trucks would impact local roadways and highways.</p>	<p>Potentially Significant</p>	<p>TRANS-1: The selected contractor shall be required to prepare a Traffic Control Plan, which shall include, at minimum:</p> <ul style="list-style-type: none"> • A set of comprehensive traffic control measures to maintain safety and Level of Service (LOS), including: <ul style="list-style-type: none"> - scheduling of major truck trips and deliveries to avoid peak traffic hours – deliveries and soil off-haul trucks shall not arrive onsite prior to 8 a.m. and shall not leave the site outside of the hours of 9 a.m. to 3 p.m., - requirements for posting of detour signs, - requirements for traffic control personnel such as flaggers during disruptions in the City rights-of-way, - lane closure procedures and signage requirements, - placement requirements for signs and cones for drivers, and - designated construction access routes; • Methods for maintaining the condition and LOS of city and state roadways; • Notification procedures for adjacent properties and public safety personnel regarding when major deliveries, detours, and lane closures would occur; • Location of construction staging areas for materials, equipment, and vehicles at an approved location; • Any heavy equipment brought to the construction site shall be transported by truck, where feasible. 	<p>Less than Significant</p>

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
Applicant Proposed Measures			
Initial Study Section 3 - Air Quality			
Project construction emissions would cause excess cancer risk to sensitive receptors (where residences in the area have infants present) to exceed the BAAQMD CEQA community risk thresholds for project-only impacts.	Potentially Significant	<p>APM-1: During construction, the Project's general contractor shall use off-road equipment that would meet, at minimum, the following criteria:</p> <ol style="list-style-type: none"> 1. All mobile diesel-powered off-road equipment larger than 25 horsepower (hp) and operating on the site for more than 20 hours shall meet, at a minimum, one of the following: <ol style="list-style-type: none"> a. Equipped with engines meeting U.S. Environmental Protection Agency (EPA) particulate matter emissions standards for Tier 2 engines or equivalent; b. All diesel-powered portable equipment (i.e., generators, concrete saws, and pumps) operating on the site for more than 20 hours shall be equipped with CARB-certified Level 3 Diesel Particulate Filters or meet U.S. EPA particulate matter emissions standards for Tier 4 engines or equivalent; c. Use alternatively fueled (i.e., non-diesel) equipment; or d. Other measures may be the use of added exhaust devices, or a combination of measures, provided that these measures are approved by the City and demonstrated to reduce community risk impacts to less than significant. 2. Line power shall be utilized as soon as feasible, limiting the use of all diesel-fueled generators, including any diesel-powered welders, used for construction activities exceeding 20 days for each piece of equipment. <p>Alternatively, prior to construction, the Project's general contractor may develop a plan, verified by a qualified air specialist and approved by the City, which ensures that the off-road equipment used onsite to construct the Project would achieve a fleet-wide average 75 percent reduction in PM10 exhaust emissions, compared to uncontrolled aggregate statewide emission rates for similar equipment.</p>	Less than Significant
Initial Study Section 12 - Noise			
During Project construction and operation of the blower, noise levels from the Project site would increase the ambient noise levels in the Project vicinity above current levels, or thresholds identified in the City's Draft General Plan.	Potentially Significant	<p>APM-2: The Project's general contractor shall be required to construct temporary noise barriers to shield stationary noise sources (e.g., tunneling equipment) from nearby receptors. The barrier shall be a minimum of 16 feet in height and would provide approximately 8 to 10 A-weighted decibels (dBA) of attenuation at the first floor, and approximately 5 dBA of attenuation at second and third floors, where the line-of-sight to construction activities is interrupted by the barrier.</p>	Less than Significant

Impact	Significance Before Mitigation	Mitigation Measures and Applicant Proposed Measures	Significance After Mitigation
Applicant Proposed Measures			
Initial Study Section 18 - Utilities and Service Systems			
During cleaning of the EQ basin following use for wet weather events, the Project would require 130 pounds per square inch (psi) of water pressure off the existing water main on Crespi Drive.	Potentially Significant	APM-3: As noted in the Project Description, prior to the flushing and cleaning of the EQ basin, the City's Wastewater Treatment Plant Superintendent, or deputy thereof, shall contact both the North County Fire Department and the North Coast County Water District to alert them of anticipated water usage. If the water usage would impede water service for either of the agencies, the flushing and cleaning of the tank would be delayed until the demand could be met.	Less than Significant

LIST OF COMMENTERS ON DRAFT MND/IS

Public Agencies Commenting in Writing

The following public agencies provided written comments on the Wet Weather Flow Equalization Basin Project Draft MND/IS:

- Scott Wilson, California Department of Fish and Wildlife, Regional Manager, Bay Delta Region – Letter dated December 29, 2016.
- Department of Transportation (Caltrans) – Letter from Patricia Maurice, District Branch Chief; dated January 13, 2017.

Individuals Commenting in Writing

The following individuals provided written comments on the Wet Weather Flow Equalization Basin Project Draft MND/IS:

- Mike O’Connell – Email dated December 14, 2016.
- Lori Bowie – Comment Form dated December 15, 2016.
- Eamon Murphy – Email dated December 16, 2016.

WRITTEN COMMENTS ON THE DRAFT MND/IS AND RESPONSES TO THESE COMMENTS

This section includes copies of the written comments received during the public review and comment period on the Draft MND/IS. Specific responses to the individual comments in each correspondence follow each letter.

Each correspondence is identified by a numeric designator (e.g., "1"). Specific comments within each correspondence also are identified by a numeric designator reflecting the numeric sequence of the specific comment within the correspondence (e.g., "1-2" for the second comment in Comment Letter 1).

Responses focus on comments that pertain to the adequacy of the analysis in the MND/IS or to other aspects pertinent to the potential effects of the Project on the environment, pursuant to CEQA. Comments that address topics beyond the purview of the MND/IS or CEQA are noted as such for the public record. Where comments have triggered changes to the Draft MND/IS, these changes appear as part of the specific response and are consolidated in Revisions to the Draft MND/IS section of this document where they are generally listed in the order the revision would appear in the Draft MND/IS document.

Comment Letter 1: Scott Wilson, California Department of Fish and Wildlife, Regional Manager, Bay Delta Region



State of California – The Natural Resources Agency
DEPARTMENT OF FISH AND WILDLIFE
Bay Delta Region
7329 Silverado Trail
Napa, CA 94558
(707) 944-5500
www.wildlife.ca.gov

EDMUND G. BROWN JR., Governor
CHARLTON H. BONHAM, Director



December 29, 2016

RECEIVED

JAN 06 2017

City of Pacifica

Ms. Bonny O'Connor
City of Pacifica Planning Department
1800 Francisco Boulevard
Pacifica, CA 94044

Dear Ms. O'Connor:

Subject: Wet Weather Flow Equalization Basin Project, Mitigated Negative Declaration, SCH #2016122016, City of Pacifica, San Mateo County

The California Department of Fish and Wildlife (CDFW) reviewed the Mitigated Negative Declaration (MND) provided for the Wet Weather Flow Equalization Basin Project (Project) located at 540 Crespi Drive, Pacifica, San Mateo County. The MND was received in our office on December 19, 2016.

CDFW is a Trustee Agency with responsibility under the California Environmental Quality Act (CEQA) §15386 for commenting on projects that could impact fish, plant and wildlife resources. CDFW is also considered a Responsible Agency if a project would require discretionary approval, such as the California Endangered Species Act (CESA) Permit, the Native Plant Protection Act, the Lake and Streambed Alteration Agreement (LSAA) and other provisions of the Fish and Game Code that afford protection to the State's fish and wildlife trust resources. Pursuant to our jurisdiction, CDFW has the following concerns, comments, and recommendations regarding the Project.

1-1

The Project would consist of the construction of a 2.1-million gallon capacity equalization basin; two diversion structures to passively divert excess flows from the existing Linda Mar and Arguello sanitary sewer lines and transport the flow via a conveyance pipeline to the equalization basin during storm events; an effluent conveyance pipeline routing flows to the existing Crespi Drive sanitary sewer line and Linda Mar Boulevard pump station; a 10-foot-tall motor control center building that is equipped with a 4-foot-high metal antenna to allow radio communication with the City's Calera Waste Water Treatment Plant and the Linda Mar pump station; a ventilation and odor-control system; and a potable-water-supplied cleaning system with the equalization basin. The equalization basin would be located with the parking lot for the City of Pacifica Skatepark.

San Francisco Garter Snake Protection Measures

The MND indicates that San Francisco garter snake (*Thamnophis sirtalis tetrataenia*) may be present in the Biological Study Area (BSA) and could be impacted as a result of Project activities. The San Francisco garter snake is fully protected under Fish and Game Code §5050, and as such, may not be taken or possessed at any time, except for scientific purposes.

1-2

Conserving California's Wildlife Since 1870

Ms. Bonny O'Connor
December 29, 2016
Page 2 of 2

Mitigation Measure BIO-1, subsection 4, attempts to address potential impacts to this species as follows:

Within the disturbed areas of the BSA, all burrows that can be potentially occupied by CRLF and SFGS shall be hand-excavated by a qualified biologist who holds current permits to handle these species, prior to the start of construction activities. If CRLF or SFGS are found, the United States Fish and Wildlife Service (USFWS) and California Department of Fish and Wildlife shall be consulted to determine an appropriate course of action. Actions could include the relocation of the animal to nearby habitat, and compensatory mitigation for removal of occupied habitat.

Please note that authorized take under a Scientific Collecting Permit is limited to scientific research approved by CDFW. While possession of a Scientific Collecting Permit for a particular species may be an indication that a biologist has particular expertise with that species, the Scientific Collecting Permit allows handling only pursuant to the approved research project(s) indicated in the permit. The Permittee cannot legally handle the species for other purposes, such as relocation activities. Please refer to our website for further information on the Scientific Collecting Permit: <https://www.wildlife.ca.gov/Licensing/Scientific-Collecting>.

Excavation of burrows that may contain San Francisco garter snake should be done with extreme caution. At the first indication that San Francisco garter snakes are present, excavation should immediately cease, and CDFW should be consulted before any further actions are taken. Because handling and other take of fully protected species as part of this Project cannot be authorized under Fish and Game Code, full avoidance must be achieved through measures that would allow the species to passively vacate the site.

Filing Fees

CDFW anticipates that the Project will have an impact on fish and/or wildlife, and assessment of filing fees is necessary (Fish and Game Code, § 711.4; Pub. Resources Code, § 21089). Fees are payable upon filing of the Notice of Determination by the Lead Agency and serve to help defray the cost of environmental review by CDFW.

If you have any questions, please contact Ms. Randi Adair, Senior Environmental Scientist (Supervisory), at (707) 576-2786 or Randi.Adair@wildlife.ca.gov.

Sincerely,



Scott Wilson
Regional Manager
Bay Delta Region

cc: State Clearinghouse #2016122016

1-2
(Cont.)

1-3

Response to Comment Letter 1: Scott Wilson, California Department of Fish and Wildlife, Regional Manager, Bay Delta Region

- 1-1 This comment identifies the California Department of Fish and Wildlife's (CDFW's) role as a Trustee Agency, and provides a summary of the Project. No response required.
- 1-2 CDFW's recommendations regarding mitigation for CRLF and SFGS are well-taken. Based on a camera survey of the burrows in question, it was determined that most burrows were shallow, and that none contained amphibians or reptiles. The assessment performed by the City's biological consultants from TRA|MIG determined that the chance of finding a CRLF or SFGS during construction is very low; however, the City included mitigation measures to ensure that impacts are avoided. The Mitigation Measure BIO-2, subsection 4, text in the MND/IS is revised as follows:

Within the disturbed areas of the BSA, all burrows that can be ~~potentially~~ occupied by CRLF and SFGS shall be hand-excavated with extreme caution by a qualified biologist ~~who holds in possession of a scientific collecting permit. current permits to handle these species, prior to the start of construction activities~~ At the first indication of CRLF or SFGS present, excavation shall immediately cease, and if CRLF or SFGS are found, the United States Fish and Wildlife Service (USFWS) and California Department of Fish and Wildlife (CDFW) shall be consulted before any further actions are taken to determine an appropriate course of action. Actions could include the relocation of the animal to nearby habitat, and compensatory mitigation for removal of occupied habitat. Because handling and other take of SFGS, a Fully Protected Species, cannot be authorized under California Fish and Game Code, full avoidance must be achieved through measures that would allow the species to passively vacate the site.

The Mitigation Measure BIO-2, subsection 6, text has been modified as follows:

A qualified biologist shall inspect the area inside of the fence for CRLF and SFGS every day before construction activities commence. If any special-status species are found, construction activities shall not be allowed to start and the USFWS and CDFW shall be consulted on an appropriate course of action. Such action could include leaving the animal alone to move away on its own ~~or the relocation of the animal to an area outside of the BSA.~~

The Mitigation Measure BIO-2, subsection 10, text has been modified as follows:

If an animal is found at the work site and is believed to be a protected species, work must be halted and the project biologist contacted for guidance. Care must be taken not to harm or harass the species. No wildlife species shall be handled and/or removed from the Project area by anyone except qualified biologists in consultation with CDFW/USFWS as appropriate and in accordance with applicable laws and regulations.

- 1-3 The City shall submit the CDFW filing fee at the time of filing the Notice of Determination with the County of San Mateo County Clerk pursuant to California Public Resources Code Section 21089.

Comment Letter 2: Patricia Maurice, California Department of Transportation District
Branch Chief, Local Development - Intergovernmental Review

STATE OF CALIFORNIA—CALIFORNIA STATE TRANSPORTATION AGENCY

EDMUND G. BROWN Jr., Governor

DEPARTMENT OF TRANSPORTATION
DISTRICT 4
OFFICE OF TRANSIT AND COMMUNITY PLANNING
P.O. BOX 23660, MS-10D
OAKLAND, CA 94623-0660
PHONE (510) 286-5528
FAX (510) 286-5559
TTY 711
www.dot.ca.gov



Serious Drought.
Help save water!

January 13, 2017

SCH # 2016122016
GTS # 04-SM-2016-00068
SM-01- 8.411- 40.957

Ms. O'Connor
Planning Department
City of Pacifica
1800 Francisco Blvd.
Pacifica, CA 94044

**City of Pacifica Wet Weather Flow Equalization Basin Project— Initial Study/Mitigated
Negative Declaration**

Dear Ms. O'Connor:

Thank you for including the California Department of Transportation (Caltrans) in the environmental review process for the City of Pacifica Wet Weather Flow Equalization Basin Project. In tandem with the Metropolitan Transportation Commission's (MTC) Sustainable Communities Strategy (SCS), the Caltrans' mission signals a modernization of our approach to evaluate and mitigate impacts to the State Transportation Network (STN). Caltrans' *Strategic Management Plan 2015-2020* aims to reduce Vehicle Miles Travelled (VMT) by tripling bicycle and doubling both pedestrian and transit travel by 2020. Our comments are based on the Draft Mitigated Negative Declaration (MND)/Initial Study (IS) dated December 2016. The Notice of Intent to adopt the above reference MND states that the public review period ends January 13, 2017.

Project Understanding

The City of Pacifica intends to construct and utilize the proposed wet weather flow equalization basin ("the EQ basin") and associated pipelines (together referred to as "the Project") as a key element to mitigate storm-related sanitary sewer overflows (SSOs) in the City's wastewater collection system and reduce peak wet weather flows to the City's Calera Creek Water Recycling Plant. The Project would include:

- The installation of a 2.1-million-gallon capacity EQ basin.
- Two diversion structures to passively divert excess flows from the existing Linda Mar Boulevard and Arguello Boulevard sanitary sewer lines and transport the flow via a

*"Provide a safe, sustainable, integrated and efficient transportation
system to enhance California's economy and livability."*

2-1

Ms. O'Connor, City of Pacifica
January 13, 2017
Page 2

- conveyance pipeline to the EQ basin during storm events.
- An effluent conveyance pipeline routing flows to the existing Crespi Drive sanitary sewer line and the Linda Mar Pump Station.
- A 10-foot tall motor control center building equipped with a 4-foot-high metal antenna that would allow radio communication with the City's Calera Waste Water Treatment Plant and the Linda Mar Pump Station.
- A ventilation and odor-control system, and
- A potable-water-supplied cleaning system within the EQ basin.

2-1
(Cont.)

The proposed two diversion structures and conveyance pipelines to and from the EQ basin would be constructed in City-owned right-of-way (ROW) or public utility easements.

The project location is about 400 feet from State Route (SR) 1 via Crespi Drive and about 900 feet from SR 1 via Linda Mar Boulevard. Project construction is anticipated to occur during a 17-month period starting in May 2017 and ending in late September 2018. Construction would occur on weekdays and would avoid peak traffic for coastal visitors, which is concentrated on weekends in the spring and fall. A maximum of approximately 120 trips per day would be generated during construction. Construction hours would be limited to the following:

- Within the EQ basin area (Skatepark parking lot area including the excavation for the jacking and receiving pits): 8:00 a.m. to 5:00 p.m. Monday through Friday
- Along City streets (except at the intersection of Linda Mar Boulevard and De Solo Drive): 9:00 a.m. to 4:00 p.m.
- At the intersection of Linda Mar Boulevard and De Solo Drive: 9:00 a.m. to 3:00 p.m. Weekend and night work is not anticipated.

Project Description

Further clarification is requested on whether the construction staging areas referenced in Mitigation Measure TRANS-1 will require crossing SR 1.

2-2

Lead Agency

As the lead agency, the City of Pacifica is responsible for all project mitigation, including any needed improvements to the STN. The project's fair share contribution, financing, scheduling, implementation responsibilities and lead agency monitoring should be fully discussed for all proposed mitigation measures. This includes any required improvements to the STN or reductions in VMT. Required improvements should be completed prior to issuance of the Building Permit. Since the Department will not issue an Encroachment Permit until our concerns are adequately addressed, we strongly recommend that the City of Pacifica work with both the applicant and the Department to ensure that our concerns are resolved during the California Environmental Quality Act (CEQA) process, and in any case prior to submittal of a permit application. See the end of this letter for more information on the Encroachment Permit process.

2-3

Ms. O'Connor, City of Pacifica
January 13, 2017
Page 3

Transportation Management Plan

Where vehicular, bicycle, and pedestrian traffic may be impacted during the construction of the proposed project requiring traffic restrictions and detours, a Caltrans-approved Transportation Management Plan (TMP) is required. Pedestrian and bicycle access through the construction zone must be maintained at all times and comply with the Americans with Disabilities Act (ADA) regulations (see Caltrans' *Temporary Pedestrian Facilities Handbook* for maintaining pedestrian access and meeting ADA requirements during construction at:

http://www.dot.ca.gov/hq/construction/safety/Temporary_Pedestrian_Facilities_Handbook.pdf
(See also Caltrans' Traffic Operations Policy Directive 11-01 "Accommodating Bicyclists in Temporary Traffic Control Zones" at: www.dot.ca.gov/trafficops/policy/11-01.pdf).

2-4

All curb ramps and pedestrian facilities located within the limits of the project are required to be brought up to current ADA standards as part of this project. The TMP must also comply with the requirements of corresponding jurisdictions. For further TMP assistance, please contact the Caltrans District 4 Office of Traffic Management Operations at (510) 286-4579. Further traffic management information is available at the following website:

www.dot.ca.gov/hq/traffops/traffgmt/tmp_ics/index.htm.

Cultural Resources

No cultural resources studies were conducted in support of this IS-MND. Although the City of Pacifica reviewed their General Plan, aerial photos, and various registers for cultural resources in the project location vicinity (pages 103 and 104), this does not constitute a cultural resources study. The project area has a very high potential for surface archaeological resources. We recommend that the City conduct a cultural resources study, including a records search at the Northwest Information Center of the California Historical Resources Information System (CHRIS) at Sonoma State University, a survey of the project location by a qualified archaeologist, and possible subsurface testing appropriate for the scope of the project.

2-5

On page 23 of the IS-MND, the text states that the City has not received any requests for tribes to be placed on the agency's consultation notification list for CEQA projects, per Assembly Bill 52, and that tribal consultation was not conducted. However, Native Americans had a year (July 1, 2015 to July 1, 2016) to submit such letters to agencies, and during the interim it was recommended that agencies refer to the list of Native American contacts generated as a result of Senate Bill 18 or to contact the Native American Heritage Commission (NAHC) to identify tribes, groups, and individuals who may have knowledge of the project area. We recommend that the City of Pacifica conduct Native American consultation in support of the project.

Transportation Permit

Project work that requires movement of oversized or excessive load vehicles on State roadways requires a Transportation Permit that is issued by Caltrans. To apply, a completed Transportation

2-6

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Ms. O'Connor, City of Pacifica
January 12, 2017
Page 4

Permit application with the determined specific route(s) for the shipper to follow from origin to destination must be submitted to:

Caltrans Transportation Permits Office
1823 14th Street
Sacramento, CA 95811-7119.

2-6
(Cont)

See the following website for more information about Transportation Permits:

<http://www.dot.ca.gov/trafficops/permits/index.html>

Encroachment Permit

Please be advised that any work or traffic control that encroaches onto the State ROW requires an Encroachment Permit that is issued by Caltrans. Traffic-related mitigation measures should be incorporated into the construction plans prior to the encroachment permit process. To apply, a completed Encroachment Permit application, the adopted environmental document, and five (5) sets of plans clearly indicating State ROW must be submitted to the address below. Traffic-related mitigation measures should be incorporated into the construction plans prior to the encroachment permit process.

2-7

David Salladay, District Office Chief
Office of Permits, MS 5E
California Department of Transportation, District 4
P.O. Box 23660
Oakland, CA 94623-0660

See the following website for more information:

<http://www.dot.ca.gov/trafficops/ep/index.html>

Thank you again for including Caltrans in the environmental review process. Should you have any questions regarding this letter, please contact Jannette Ramirez at 510-286-5535 or jannette.ramirez@dot.ca.gov.

Sincerely,



PATRICIA MAURICE
District Branch Chief
Local Development - Intergovernmental Review

c: State Clearinghouse

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Response to Comment Letter 2: Patricia Maurice, California Department of Transportation District Branch Chief, Local Development - Intergovernmental Review

- 2-1 This comment identifies the California Department of Transportation's (Caltrans') understanding of the Project. No response required.
- 2-2 Staging areas for equipment and materials for the EQ basin and pipeline construction would be located within the areas identified in Figure 7, Proposed Staging Areas. Materials for use in the pipeline construction in the area south of the EQ basin would be transported along City streets so as to reduce the number of trips from the staging area to the construction area. SR-1 would not be used by trucks to transport materials from the staging areas to the pipeline construction.
- 2-3 The Project would not include work in the State Transportation Network (STN), and would use the STN only for the transportation of workers, equipment, and materials to the Project.
- 2-4 The Project would not require restrictions or detours to the STN.
- 2-5 The Project would primarily occur within a previously disturbed and paved area. In 2009, a CHRIS search covering the City of Pacifica was performed and did not identify the potential for significant environmental impacts to cultural resources on the Project site.² Additionally, construction in 2004/2005 for the Skatepark, which is located on the same lot and immediately adjacent to the EQ basin, did not result in the discovery of significant cultural resources. Lastly, a consultant did complete a surface survey of all unpaved areas as part of the biological resources analysis and did not find any cultural artifacts at that time.

CEQA documents prepared by Caltrans for the Highway 1 San Pedro Bridge Replacement Project determined that there would be no impacts on cultural resources. While the City understands that the Highway 1 San Pedro Bridge Replacement Project does not overlap with the proposed Project area, the sites are only approximately one-quarter mile apart and augering performed at the Highway 1 San Pedro Bridge Replacement Project in 2011 did not identify a significant potential to impact cultural resources.³

The existing disturbed conditions of the site, surface surveying of site, results of nearby cultural testing, findings from a 2009 CHRIS search, a review of aerial photographs of the site, and review of the General Plan, as well as the implementation of the mitigation measures identified in the MND/IS, all support the City's determination that the proposed Project would have a less than significant impact on cultural resources.

² City of Pacifica. 2014. Pacifica General Plan Draft Environmental Impact Report. SCH No. 2012022046. Prepared by Dyett & Bhatia. March.

³ City of Pacifica. 2012. Highway 1 San Pedro Creek Bridge Replacement IS/MND Addendum. SCH No. 2005012126. Prepared by TRA Environmental Sciences, Inc. November

As stated in the Draft MND/IS, to date, the City of Pacifica has not received any applicable tribal consultation requests per Assembly Bill 52. The release of this Draft MND was well after the end of the Native American notification "grace period" identified in your comment. No Native American consultation was conducted in response to your comment.

2-6 Comment noted.

2-7 Comment noted. The Project would not encroach into the STN.

Comment Letter 3: Mike O'Connell

From: mike@roundhouseindustries.com [mailto:mike@roundhouseindustries.com]
Sent: Wednesday, December 14, 2016 9:03 AM
To: O'Connor, Bonny
Cc: Wehrmeister, Tina
Subject: EQ Basin

Hi Bonny

I'm not able to attend the meeting about the EQ basin tomorrow but I had one question I would like to submit. Does the project incorporate any features that make it resilient to sea level rise? I'm working on a lot of projects on the bay in SF where we are required to design for 16" of sea level rise initially and we also need to provide an adaptive management strategy for how to deal with sea level rise beyond 16" if that were to occur. There is more info on BCDC's website about the amount of predicted sea level rise. I understand that the bottom of this structure is indefinitely below sea level so my concern is that it doesn't get submerged in the future.

3-1

Thanks,

Mike

This message has been scanned for malware by Websense. www.websense.com

Response to Comment Letter 3: Mike O'Connell

- 1-1 Consideration of sea level rise was made during the site selection process for the Project. The proposed location was found to be less vulnerable to sea level rise due to its location east of State Route 1. See RMC's *City of Pacifica Wet Weather Equalization Basin Site Feasibility Evaluation Final Report* and *Site Alternative 2C and Prioritization Addendum to Site Feasibility Evaluation Report* available online http://www.cityofpacifica.org/depts/wwt/waste_water_collection/default.asp for more information.

The CEQA requirement for an environmental document to analyze the "environmental effects" of a project does not require agencies to analyze the environment's effects on a project. In *California Bldg. Industry Assn. v. Bay Area Air Quality Management Dist.*, 62 Cal.4th 369 (2015), the California Supreme Court held that in light of CEQA's text, structure, and purpose, a general requirement for an analysis of how existing environmental conditions will affect a project's future users or residents would improperly expand the scope of the statute and add significantly to the burdens of compliance. As the court put it: "Given the sometimes costly nature of the analysis required under CEQA when an EIR is required, such an expansion would tend to complicate a variety of residential, commercial, and other projects beyond what a fair reading of the statute would support."

In keeping with this California Supreme Court decision, an analysis of sea level rise on the Project is not mandated as part of the CEQA process. However, as explained above, such a review was indeed undertaken as part of the site selection process.

Comment Letter 4: Lori Bowie



City of Pacifica
Draft Mitigated Negative Declaration/Initial Study of the
Proposed Wet Weather Equalization Basin Project

Public Comment Period December 8, 2016 - January 13, 2017

Name (please print): Lori Bowie

Affiliation (if applicable): _____

Address: 209 Marilla Ave

Email: _____ Phone: 650 455 0639

LORETTA BOWIE @ SBEGLOBAL.NET

COMMENT

1) Some people are in flood zone pay more for
Hous Inc. Will this get us out of 915
Inc. Will The Pacifica City help us with
this. 4-1

2) How is the pumps working at this time?
Thank you. 4-2

3) Called Pub Works today drain was fixed with
Leaves they came out in second time
9x Service
Job well done
☺ 4-3

(Attach additional sheets, if necessary)

Comments must be received by **January 13, 2017 by 5:00pm**
Mail: Bonny O'Connor, City of Pacifica Planning Department
1800 Francisco Blvd., Pacifica, CA 94044
Fax: (650) 359-5807 Email: o'connorb@ci.pacifica.ca.us

Response to Comment Letter 4: Lori Bowie

- 4-1 The objective of the Project is to address insufficient capacity-caused SSOs. The Project would not remove local residences from the flood zone as defined by the Federal Emergency Management Agency. Insurance payments are not a CEQA-related issue and therefore are not addressed in the MND/IS.

- 4-2 This question is assumed to address the Linda Mar Pump Station at the time of the public meeting (December 15, 2016) when the comment form was received. The proposed Project would not change the existing function of the Linda Mar Pump Station; therefore, the status of the Linda Mar Pump Station is beyond the scope of this CEQA document.

- 4-3 Comment noted, and the feedback is appreciated.

Comment Letter 5: Eamon Murphy

From: Eamon Murphy [<mailto:emurphy@homesiteservices.net>]
Sent: Friday, December 16, 2016 9:49 AM
To: O'Connor, Bonny
Cc: Engineering Division; Brendan Murphy
Subject: RE: Wet Weather Equalization Basin Project Public Meeting - Follow-up

Hi Bonny,

Great job last night on the presentation. As you may know, our development at 570 Crespi Dr. will be in need of approximately 1500 square yards of soil to bring our property out of the flood zone for development. We have reached out to Rob Smith and Van Ocampo regarding our willingness to work with the city to make the transfer of the soil from your property to ours seamless.

It is our belief that this partnership is in the best interest of everyone. Bringing the city's 40' portion of property, adjacent to our lot, up and out of the flood zone in conjunction with our 80' section simply makes sense. By working together we can mitigate traffic issues, costs of off-haul, and most importantly the timing of this project. We also believe it will generate goodwill within the community.

We would appreciate a meeting with Planning and Engineering to discuss this in further detail. Please let me know if you have any questions.

Regards,
Eamon Murphy
60 Driftwood Cir.

5-1

Response to Comment Letter 5: Eamon Murphy

- 3-1 The amount of soil requested by the commenter is an insignificant amount of the approximately 20,000 cubic yards of soil that would be off-hauled from the site as a part of the Project. Therefore, transfer of the requested amount of soil on the commenter's property would have little beneficial effect on the overall cost of the Project, project timing, and the already less-than-significant impact on traffic. Currently, there are several unknown factors associated with transferring the soil to the Commenter's property, which could result in a delay to the City's schedule. Therefore, the City respectfully declines the commenter's offer. No change to the Project Description is proposed.

REVISIONS TO THE DRAFT MND/IS

The changes to the Draft MND/IS presented in this section of the Final MND/IS are either initiated by the City (Lead Agency) staff or made in response to public comments received on the Draft MND/IS. Changes consisted of additions, revisions, or clarifications to descriptive information presented in the Draft MND/IS. None of the changes affected the original findings or determinations of the Draft MND/IS. Throughout this section, newly added text is shown in single underline format and deleted text is shown in ~~striketrough~~ format. For changes specifically initiated by comments received on the Draft MND/IS, the numeric designator for the comment is indicated in [brackets] prior to its description.

Changes are listed generally in the order in which they would appear in the Draft MND/IS document. As indicated in the Introduction section, the entirety of the Final MND/IS consists of the Draft MND/IS and this document. Thus, the changes to the Draft MND/IS presented in this section incorporate and supersede the text of the Draft MND/IS.

MITIGATED NEGATIVE DECLARATION: Mitigation Measures and Applicant Proposed Measures

The City has revised the text of Mitigation Measure AQ-1 to clarify that dust control requirements apply to both the general contractor and subcontractors (page 6):

AQ-1: The Project's selected general contractor and their subcontractors shall implement ~~include~~ basic measures to control dust and exhaust during construction, as recommended by the Bay Area Air Quality Management District (BAAQMD). During any construction period with ground disturbance, the Project's general contractor and their subcontractors shall implement the following BAAQMD-recommended best management practices (BMPs):

The City has revised the text of Mitigation Measure AQ-1, subsection 3, for clarification of meaning (page 6):

3. All visible mud or dirt tracked ~~out~~ onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping or washing down paved streets using potable water is prohibited.

The City has revised the text of Applicant Proposed Measure APM-1 for clarification of meaning (page 7):

APM-1: During construction, the ~~selected~~ Project's general contractor shall use off-road equipment that would meet, at minimum, the following criteria:

and

Alternatively, prior to construction, the ~~selected~~ Project's general contractor may develop a plan, verified by a qualified air specialist and approved by the City, which ensures that the off-road equipment used onsite to construct the Project would achieve a fleet-wide average 75 percent reduction in PM10 exhaust emissions, compared to uncontrolled aggregate statewide emission rates for similar equipment.

The City has revised the text of Mitigation Measure BIO-1 for clarification of meaning (page 8):

BIO-1: The City shall implement the following measures or designate implementation of these measures to the Project's general contractor prior to construction:

The City has revised the text of Mitigation Measure BIO-2 for clarification of meaning (page 8):

BIO-2: The City shall implement the following measures or designate implementation of these measures to the Project's general contractor:

1. A qualified biologist shall assist with the placement of wildlife exclusion fencing, and verify that stormwater protection measures to protect adjacent undeveloped areas are in place prior to construction. The biologist shall be provided the contact information of the ~~SWPPP~~ Project's general contractor Qualified SWPPP Developer (QSD), and vice versa.

The City has revised the text of Mitigation Measure BIO-2, subsection 4, for clarification of the allowed activities associated with biological monitoring (page 9):

Within the disturbed areas of the BSA, all burrows that can be ~~potentially~~ occupied by CRLF and SFGS shall be hand-excavated with extreme caution by a qualified biologist ~~who holds in possession of a scientific collecting permit. current permits to handle these species, prior to the start of construction activities~~ At the first indication of CRLF or SFGS present, excavation shall immediately cease, and if CRLF or SFGS are found, the United States Fish and Wildlife Service (USFWS) and California Department of Fish and Wildlife (CDFW) shall be consulted before any further actions are taken to determine an appropriate course of action. Actions could include the relocation of the animal to nearby habitat, and compensatory mitigation for removal of occupied habitat. Because handling and other take of SFGS, a Fully Protected Species, cannot be authorized under California Fish and Game Code, full avoidance must be achieved through measures that would allow the species to passively vacate the site.

The City has revised the text of Mitigation Measure BIO-2, subsection 6, for clarification of the allowed activities associated with biological monitoring (page 10):

A qualified biologist shall inspect the area inside of the fence for CRLF and SFGS every day before construction activities commence. If any special-status species are found, construction activities shall not be allowed to start and the USFWS and CDFW shall be

consulted on an appropriate course of action. Such action could include leaving the animal alone to move away on its own ~~or the relocation of the animal to an area outside of the BSA.~~

The City has revised the text of Mitigation Measure BIO-2, subsection 10, for clarification of the allowed activities associated with biological monitoring (page 11):

If an animal is found at the work site and is believed to be a protected species, work must be halted and the project biologist contacted for guidance. Care must be taken not to harm or harass the species. No wildlife species shall be handled and/or removed from the Project area by anyone except qualified biologists in consultation with CDFW/USFWS as appropriate and in accordance with applicable laws and regulations.

The City has revised the text of Mitigation Measure HAZ-1, subsection b, for clarification of meaning (page 12):

(b) ~~The construction~~ Project's general contractor shall require that all site workers (including subcontractors) be trained in identifying contaminated soil and/or groundwater.

The City has revised the text of Mitigation Measure HAZ-2 to clarify that the Project's general contractor shall incorporate all BMPs in the QSD developed SWPPP (page 13):

HAZ-2: ~~The selected~~ Project's general contractor and their subcontractors shall be required to use BMPs to minimize the potential for releases of hazardous materials to groundwater, surface water, and soil. The BMPs shall be outlined in the general contractor's ~~Work Plan~~ SWPPP document that will be prepared by their QSD and provided-submitted to the City's Construction Manager, ~~as well as the SWPPP, and uploaded to the State Water Quality Resource Board's (SWRCB) SMART database as required under the SWRCB's SWRCB Construction General Permit for Construction Activities.~~ The ~~Work Plan~~ SWPPP shall include BMPs that accomplish the following:

The City has revised the text of Mitigation Measure HYDRO-1 for clarification of location of groundwater monitoring (page 14):

Monitoring of groundwater levels ~~perpendicular~~ in the piezometer PZ-1 that is located in the vicinity of ~~to~~ the proposed excavation zone shall be required to verify the assumptions used to calculate potential settlements.

The City has revised the text of Applicant Proposed Measure APM-2 for clarification of meaning (page 16):

APM-2: The Project's general contractor shall be required to construct temporary noise barriers to shield stationary noise sources (e.g., tunneling equipment) from nearby receptors.

INITIAL STUDY: Project Description

The text identified that a total of four heritage trees had been identified for removal in the area east of the driveway entrance to the Skatepark parking lot, and referenced Figure 8 showing the location of the trees. Figure 8 showed a total of seven trees in this area, and accurately reflects the tree removal plan. Additionally, during the storm event on January 8, 2017, one of the trees that was planned for removal as part of the Project fell down. The text has been modified to reflect that the Project would result in the removal of six rather than four trees east of the driveway entrance (page 56):

Additionally, the Project includes the use of the area to the east of the intersection of Crespi Drive and the driveway entrance to the Skatepark parking lot for staging and laydown, which would require the removal of ~~four~~ six heritage trees as shown on **Figure 8**.

and (page 57):

Following completion of the construction of the Project, the removed trees would be replaced at a minimum ratio of 1:1, meaning that a total of at least ~~eight~~ ten trees would be planted to replace the ~~eight~~ ten removed trees.

Figure 8 has been modified to reflect that the removal of the tree that fell on January 8, 2017, is no longer included in the Project (see next page).



Source: NAIP 6/13/2014

Figure 8
Proposed Tree Removal

INITIAL STUDY

The City has revised the text of the Initial Study to reflect the changes to the following mitigation measures and applicant proposed measures as identified in the MND discussion above: AQ-1, APM-1, BIO-1, BIO-2 (including subsections 4, 6, and 10), HAZ-1, HAZ-2, HYDRO-1, and APM-2.

APPENDIX E: Mitigation Monitoring Plan and Reporting Program Checklist

The City has revised the text of the Mitigation Monitoring Plan and Reporting Program Checklist to reflect the changes to the following mitigation measures and applicant proposed measures as identified in the MND discussion above: AQ-1, APM-1, BIO-1, BIO-2 (including subsections 4, 6, and 10), HAZ-1, HAZ-2, HYDRO-1, and APM-2.

The City has revised the Mitigation Monitoring Plan and Reporting Program Checklist to include the following information. The revised Mitigation Monitoring Plan and Reporting Program Checklist is provided as Appendix A to this document.

- Parties responsible for implementation
- Parties responsible for verification of implementation
- Form of verification required
- Additional comments / instructions for implementation

APPENDIX A
MITIGATION MONITORING AND REPORTING PROGRAM CHECKLIST

Mitigation Monitoring Plan and Reporting Program Checklist
Wet Weather Flow Equalization Basin Project
Final Mitigated Negative Declaration/Initial Study
City of Pacifica, California

Mitigation Measures and Applicant Proposed Measures	Timing	Responsible for Implementation	Responsible for Verification	Form of Verification	Comments/ Special Instructions	Initials	Date
<p>AES-1: The Tree Protection Plan prepared by the City (or designee) shall include a schedule and replacement ratio for heritage tree removal. The Tree Protection Plan shall require that replacement trees shall be placed as closely as feasible to the removal sites in order to return aesthetics to pre-project conditions.</p>	One time	Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans)	Project Construction Manager	City Arborist's confirmation of planting completion	The Tree Protection Plan is a requirement of the City's Heritage Tree Permit.		
<p>AQ-1: The Project's general contractor and their subcontractors shall implement basic measures to control dust and exhaust during construction, as recommended by the Bay Area Air Quality Management District (BAAQMD). During any construction period with ground disturbance, the Project's general contractor and their subcontractors shall implement the following BAAQMD-recommended best management practices (BMPs):</p> <ol style="list-style-type: none"> All exposed non-hardscaped surfaces (e.g., temporary parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered appropriately to maintain a damp condition. All haul trucks transporting soil, sand, or other loose material offsite shall be covered. All visible mud or dirt tracked onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping or washing down paved streets using potable water is prohibited. All vehicle speeds on unpaved roads shall be limited to 15 miles per hour. All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used. Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California Airborne Toxics Control Measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage with these requirements shall be provided for construction workers at all access points. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation. A publicly visible sign shall be posted with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. The BAAQMD's phone number shall also be visible to ensure compliance with applicable regulations. 	Ongoing during project construction	Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans)	Project Construction Manager	Project Construction Manager, or designee, shall document at least on a weekly basis that BMPs are being implemented.			

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Wet Weather Flow Equalization Basin Project
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<p>BIO-1: The City shall implement the following measures or designate implementation of these measures to the Project's general contractor prior to construction:</p> <ol style="list-style-type: none"> Post signage indicating that travel and parking of vehicles and equipment must be limited to pavement, existing roads, and previously disturbed areas. Include provisions in the Project Plans detailing the areas that have been found to be acceptable for disturbance (i.e., previously disturbed areas and those within the Biological Study Area [BSA]). The contractor shall not disturb or remove vegetation outside of these areas. Work areas that would be temporarily impacted by construction would be restored with respect to pre-existing contours and conditions upon completion of work. Restoration work including re-vegetation and soil stabilization shall be evaluated upon completion of work and performed as needed. The City shall ensure that the Stormwater Pollution Prevention Plan (SWPPP) prepared and implemented for the project shall include measures that: <ul style="list-style-type: none"> minimize erosion and/or prevent water-borne silt from being deposited in adjacent undeveloped areas; prevent waste and/or construction materials from getting into the adjacent undeveloped areas; and control and prevent the discharge of all potential pollutants, including hazardous materials, solid wastes, paints, concrete, petroleum products, chemicals, wash water or sediment, and non-stormwater discharges to adjacent undeveloped areas via storm drains, water courses, or sheet flow. 	<p>Post prior to start of construction and maintain during project construction</p> <p>One time during post-construction</p> <p>Prepare prior to construction and implement during construction</p>	<p>Project General Contractor</p> <p>(City shall include these measures on all appropriate bid, contract, and engineering / site plans)</p>	<p>Project Construction Manager</p>	<p>Project Construction Manager, or designee, shall document at least on a weekly basis that BMPs are being implemented.</p>			
<p>BIO-2: The City shall implement the following measures or designate implementation of these measures to the Project's general contractor:</p> <ol style="list-style-type: none"> A qualified biologist shall assist with the placement of wildlife exclusion fencing, and verify that stormwater protection measures to protect adjacent undeveloped areas are in place prior to construction. The biologist shall be provided the contact information of the Project's general contractor Qualified SWPPP Developer (QSD), and vice versa. Before the contractors, their employees, or any persons start any work onsite, each worker shall participate in an employee education program, consisting of a brief presentation to explain biological resources on the Project site, which shall be conducted by a qualified biologist. The program shall include the following: <ol style="list-style-type: none"> a description of relevant special-status species, nesting birds, and bats along with their habitat needs as they pertain to the BSA; a report of the occurrence of these species in the Project vicinity, as applicable; an explanation of the status of these species and their protection under the federal and state regulations; a list of measures being taken to reduce potential impacts to natural resources during project construction and implementation; instructions if a special-status species is found onsite; and a summary of the personal consequences of violating state and federal law related to these species. A fact sheet conveying this information shall be prepared and distributed to the above-mentioned people and anyone else who may enter the work areas within the BSA. Upon completion of training, employees shall sign a form stating that they attended the training and agree to all the conservation and protection measures. The training logs shall be provided to the City on a monthly basis. A qualified biologist shall conduct a pre-construction survey within the Project area for the presence of the California red-legged frog (CRLF) and/or the San Francisco garter snake (SFGS). The survey shall be conducted immediately prior to the initial onset of Project activities. If any special-status species are found, work shall not commence until the appropriate state and/or federal resource agencies are contacted and avoidance and mitigation measures are in place. 	<p>Install prior to construction and monitor and maintain during construction</p> <p>Prior to workers starting on work site</p> <p>Prior to construction</p>	<p>City and City's Contractor (Biologist)</p> <p>City and City's Contractor (Biologist)</p>	<p>Project Construction Manager</p>	<p>Written record of placement of wildlife exclusion fencing and SWPPP BMPs.</p> <p>Review training logs on a monthly basis to ensure training of all employees</p> <p>Written record of survey results and consultation with USFWS or CDFW, if applicable</p>			

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<p>4. Within the disturbed areas of the BSA, all burrows that can be occupied by CRLF and SFGS shall be hand-excavated with extreme caution by a qualified biologist in possession of a scientific collecting permit. At the first indication of CRLF or SFGS presence, excavation shall immediately cease, and the United States Fish and Wildlife Service (USFWS) and California Department of Fish and Wildlife (CDFW) shall be consulted before any further actions are taken. Because handling and other take of SFGS, a Fully Protected Species, cannot be authorized under California Fish and Game Code, full avoidance must be achieved through measures that would allow the species to passively vacate the site.</p> <p>5. A wildlife exclusion/environmental fence (with escape routes, such as exit funnels) shall be erected around active construction areas to prevent the movement of animals into active construction areas under oversight by a qualified biologist. During construction, the fence shall be checked every day before construction activities commence for damage, breaks, or trapped wildlife. Any damage to the fence shall be repaired in a timely manner. The qualified biologist overseeing the placement of wildlife exclusion fencing shall ensure placement of the fence so that a minimum 3-foot-wide wildlife corridor remains open between the south end of the Skatepark parking lot and the residential fences.</p> <p>6. A qualified biologist shall inspect the area inside of the fence for CRLF and SFGS every day before construction activities commence. If any special-status species are found, construction activities shall not be allowed to start and the USFWS and CDFW shall be consulted on an appropriate course of action. Such action could include leaving the animal alone to move away on its own.</p>	<p>Prior to construction in BSA</p> <p>Install prior to construction and monitor and maintain during construction</p> <p>Ongoing during project construction</p>	<p>City and City's Contractor (Biologist)</p> <p>City and City's Contractor (Biologist)</p> <p>City and City's Contractor (Biologist)</p>	<p>City's Contractor (Biologist) to provide documentation of hand-excavation to Project Construction Manager</p> <p>City's Contractor (Biologist) to provide record of daily check to Project Construction Manager</p> <p>City's Contractor (Biologist) to provide record of daily check to Project Construction Manager</p>	<p>Written record of hand-excavation activities and observations, and consultation with USFWS or CDFW, if applicable</p> <p>Written record of daily checks and repairs</p> <p>Written record of daily checks and consultation with USFWS or CDFW, if applicable</p>			
<p>7. When construction and construction-related activities (including, but not limited to, mobilization and staging, clearing, grubbing, vegetation removal, fence installation, demolition, and grading noise) occur during the avian nesting season (from February 1 to August 31 for passerines and January 1 to September 15 for raptors), all suitable habitats located within the Project's area of disturbance, including staging and storage areas plus a 250-foot (passerines) and 1,000-foot (raptor nests) buffer around these areas, shall be thoroughly surveyed, as feasible, for the presence of active nests by a qualified biologist no more than five days before commencement of any site disturbance activities and equipment mobilization. If project activities are delayed by more than five days, an additional nesting bird survey shall be performed. Active nesting is present if a bird is building a nest, sitting in a nest, a nest has eggs or chicks in it, or adults are observed carrying food to the nest. The results of the surveys shall be documented and provided to the City's Planning Department. If pre-construction nesting bird surveys identify potential impacts to active nests, no site disturbance and mobilization of heavy equipment shall take place within a buffer determined by the biologist in consultation with a CDFW biologist. During that consultation, it can also be determined what low-impact construction activities are allowed within the buffer. The buffer shall be in place until the chicks have fledged. Monitoring shall be required to ensure compliance with the Migratory Bird Treaty Act (MBTA) and California Fish and Game Code. Monitoring dates and findings shall be documented and provided to the City's Planning Department.</p>	<p>One time, 5 days prior to construction during nesting season</p>	<p>City and City's Contractor (Biologist)</p>	<p>City's Contractor (Biologist) to provide record of survey and monitoring of active nests to Project Construction Manager and Planning Department.</p>	<p>Written record of survey and monitoring results.</p>			
<p>8. A preconstruction survey of trees within the developed and disturbed areas of the BSA shall be conducted by a qualified biologist for colony bat roosts within 14 days prior to the onset of project activity, and the survey shall be documented and provided to the City's Planning Department. If an occupied maternity or colony roost is detected, the CDFW shall be consulted to determine appropriate measures, such as bat exclusion methods, if the roost cannot be avoided.</p>	<p>One time, no more than 14 days prior to construction</p>	<p>City and City's Contractor (Biologist)</p>	<p>City's Contractor (Biologist) to provide record of survey to Project Construction Manager and Planning Department.</p>	<p>Written record of survey and consultation with CDFW, if applicable</p>			

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<p>9. Food items may attract wild animals onto the construction site, which would expose them to construction-related hazards. The construction site shall be maintained in a clean condition. All trash (e.g., food scraps, cans, bottles, containers, wrappers, cigarette butts, and other discarded items) shall be placed in closed containers and properly disposed of.</p> <p>10. If an animal is found at the work site and is believed to be a protected species, work must be halted and the project biologist contacted for guidance. Care must be taken not to harm or harass the species. No wildlife species shall be handled and/or removed from the Project area by anyone except qualified biologists in consultation with CDFW/USFWS as appropriate and in accordance with applicable laws and regulations.</p> <p>11. The Project specifications, the contractor's work plan, and/or SWPPP shall include provisions to ensure that the use of monofilament netting, including its use in temporary and permanent erosion control materials, is avoided altogether. All holes greater than 1 foot deep must be sealed overnight to prevent the entrapment of wildlife. Where holes or trenches cannot be sealed, escape ramps that are no greater than 30% slope shall be positioned such that entrapped wildlife would be able to escape. The escape ramps should be at least 1 foot wide and covered/fitted with a material that provides traction.</p> <p>CUI-1: In the event that any prehistoric, historic, or paleontological materials are discovered during ground-disturbing activities, ground work shall cease at the Project site and discovered materials and their surroundings shall not be altered or collected. A qualified archaeologist or paleontologist shall be consulted as to the significance of the find, and avoidance measures or appropriate mitigation shall be completed according to CEOA guidelines. Significant cultural materials recovered shall be subject to scientific analysis and/or professional museum curation, and a report shall be prepared by the qualified archaeologist according to current professional standards. Ground disturbance activities shall continue upon direction from the qualified archaeologist according to current professional standards.</p> <p>CUI-2: Prior to performing ground disturbance activities, all on-site workers shall be trained by a City-approved archaeologist in what cultural resources are, identifying cultural resources, the procedure if a cultural resource is found, and their legal responsibility to protect cultural resources. Training logs shall be provided to the City regularly.</p>	<p>Prepare prior to construction and implement during construction</p> <p>Ongoing during project construction</p> <p>Install prior to construction and monitor and maintain during construction</p> <p>Ongoing during project construction</p> <p>Prior to workers starting on work site</p>	<p>Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans)</p> <p>City and City's Contractor (Biologist)</p> <p>City shall include these measures on all appropriate bid, contract, and engineering / site plans</p> <p>City to implement if cultural resources are encountered.</p> <p>City and City's Contractor (Qualified Professional Archaeologist)</p>	<p>Project Construction Manager</p> <p>City's Contractor (Biologist) to provide employee training logs to Project Construction Manager</p> <p>City's Contractor (Biologist) to provide documentation of daily checks of work area to Project Construction Manager</p> <p>City's Contractor (Qualified Professional Archaeologist) to provide documentation to Project Construction Manager, CHRIS, and the City Planning Department</p> <p>City's Contractor (Qualified Professional Archaeologist) to provide documentation of training to Project Construction Manager</p>	<p>Project Construction Manager, or designee, shall document at least on a weekly basis that BMPs are being implemented</p> <p>Review training logs on a monthly basis to ensure training of all employees</p> <p>Written record of daily checks</p> <p>Written archaeological report of findings</p> <p>Review training logs on a monthly basis to ensure training of all employees</p>			

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<p>HAZ-1: The City shall ensure that:</p> <p>(a) The construction contractor (or designee) shall conduct a private utility survey for the presence of underground utilities, fill pipes, and underground storage tanks (USTs) prior to excavation within the Skatepark parking lot area, areas of the proposed parking and receiving pits, and along the proposed locations of the influent and effluent conveyance pipelines. In the event that a UST is identified within the excavation boundaries, the City shall contact an environmental consultant, who shall perform and/or coordinate the investigation for the presence of contamination in accordance with applicable regulations. A report of the findings of the geophysical survey shall be submitted to the City's Construction Manager and City's Public Works Department liaison.</p> <p>(b) The Project's general contractor shall require that all site workers (including subcontractors) be trained in identifying contaminated soil and/or groundwater. In the event that contaminated soil or groundwater is encountered (either visually or through odor detection) during excavation activities, the construction contractor shall follow the procedures below:</p> <ul style="list-style-type: none"> • Stop work in areas with suspected contamination; • Immediately report observations to the City's Construction Manager; • Contact an environmental consultant, who shall perform and/or coordinate the investigation of suspected contamination in accordance with applicable regulations. <p>(c) If necessary, based on the findings of the environmental consultant, the City's Public Works Department shall notify San Mateo County Environmental Health Department, San Francisco Bay RWQCB, and/or the California Department of Toxic Substances Control (DTSC).</p> <p>(d) If investigation confirms presence of contamination, the environmental consultant shall perform and/or coordinate appropriate site investigation and cleanup procedures in accordance with regulatory requirements, including the appropriate segregation and disposal of contaminated soil and groundwater. Once the extent of the contamination have been delineated and the contaminated materials (i.e., soil and/or groundwater) have been excavated or otherwise remediated (e.g., in-situ treatment), ground-disturbing activities shall continue.</p>	<p>One time prior to excavation</p> <p>Prior to workers starting on work site</p> <p>One time, if contaminated soils are discovered</p> <p>One time, if contaminated soils are discovered</p>	<p>Project General Contractor</p> <p>(City shall include these measures on all appropriate bid, contract, and engineering / site plans)</p> <p>Project General Contractor</p> <p>(City shall include these measures on all appropriate bid, contract, and engineering / site plans)</p> <p>City's Public Works Department liaison</p> <p>City and City's Contractor (Environmental Consultant)</p>	<p>Project General Contractor to provide documentation to Project Construction Manager and Public Works Department liaison</p> <p>Project General Contractor to provide training logs to Project Construction Manager</p> <p>City's Public Works Department liaison to provide documentation of report to Project Construction Manager</p> <p>City's Public Works Department liaison to provide documentation of investigation and cleanup, as applicable, to Project Construction Manager</p>	<p>Report of findings of the geophysical survey</p> <p>Review training logs on a monthly basis to ensure training of all employees</p> <p>Written documentation summarizing notifications made</p> <p>Written documentation of investigation and cleanup</p>			

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<p>HAZ-2: The Project's general contractor and their subcontractors shall be required to use BMPs to minimize the potential for releases of hazardous materials to groundwater, surface water, and soil. The BMPs shall be outlined in the general contractor's SWPPP document that will be prepared by their OSD submitted to the City's Construction Manager and uploaded to the State Water Quality Resource Board's (SWRCB) SMART database as required under the SWCRB's General Permit for Construction Activities. The SWPPP shall include BMPs that accomplish the following:</p> <ul style="list-style-type: none"> • Discussion of methodology and available technology for waste management and materials pollution control, as well as other construction-related activities. • Provisions for training the site workers on the proper storage and handling of hazardous substances, such as fuels, lubricants, paints, and solvents. Training logs shall be provided to the City regularly. • A process for responding to, and tracking, complaints pertaining to construction activity, including identification of the City's Construction Manager. The Construction Manager shall determine the cause of the complaints and shall take prompt action to correct the problem. The City's Planning and Zoning Department shall be informed who the City's Construction Manager is prior to the issuance of the first permit issued by Building Services. • Provision for accommodation of pedestrian flow and prevention of any unauthorized personnel from entering the construction zone or material and equipment storage areas. • Prior to construction, a portable toilet facility and a debris box shall be installed on the site and properly maintained through project completion. 	<p>Prepare prior to construction and implement during construction</p>	<p>Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans)</p>	<p>Project General Contractor to provide Work Plan and SWPPP to Project Construction Manager</p>	<p>Review of Work Plan and SWPPP for BMPs that accomplish the requirements identified in the measure</p>			
<p>HYDRO-1: The general contractor shall develop a dewatering plan and obtain any necessary permits for performing dewatering. The dewatering plan shall include methods to manage the potential environmental impacts that dewatering activities might have. The dewatering plan shall include the following at a minimum:</p> <ul style="list-style-type: none"> • a pre-dewatering topographic survey with a minimum vertical accuracy of 0.01 foot (if the existing site topographic survey already prepared for design purposes provides a minimum vertical accuracy of 0.01 foot, this survey can be utilized and an additional topographic survey would not be required); • a photographic survey of structures and flatwork in the surrounding area documenting any pre-dewatering damage to the structures or flatwork, including measurements of the widths and lengths of any significant cracks in the structures or flatwork; • pre-construction evaluation of required groundwater extraction rates and volumes, calculation of the radius of influence of the dewatering wells/sumps and anticipated settlements as a function of distance from the excavation; • measures to address situations where water resource impacts or excessive settlements are occurring. <p>Monitoring of groundwater levels in the piezometer PZ-1 that is located in the vicinity of the proposed excavation zone shall be required to verify the assumptions used to calculate potential settlements.</p>	<p>Prepare prior to dewatering and implement during dewatering</p>	<p>Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans)</p>	<p>Project General Contractor to provide dewatering plan to Project Construction Manager</p>	<p>Review of dewatering plan to ensure the plan includes the elements identified in the measure</p>			

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<p>NOISE-1: No construction activities shall be permitted on the weekends or at night. To reduce construction noise levels emanating from the site and minimize disruption and annoyance of existing noise-sensitive receptors in the Project vicinity, the City shall require the selected contractor to develop a Noise Control Plan. This Noise Control Plan shall include, but not be limited to, the following construction BMPs:</p> <ul style="list-style-type: none"> All equipment driven by internal combustion engines shall be equipped with mufflers that are in good condition and appropriate for the equipment. The construction contractor shall utilize "quiet" models of air compressors and other stationary noise sources where technology exists. Unnecessary idling of internal combustion engines shall be prohibited. Construction staging areas shall be established at locations that would create the greatest distance between the construction-related noise sources and receptors nearest the Project site during all Project construction, as feasible. Locate stationary noise sources as far from receptors as feasible. If they must be located near receptors, adequate muffling (with enclosures, where feasible and appropriate) would be used as necessary to comply with local noise ordinance and general plan limits. Any enclosure openings or venting would face away from receptors. Locate material stockpiles, as well as maintenance/equipment staging and parking areas, as far as feasible from residential receptors. Neighbors located adjacent to the construction site shall be notified of the construction schedule in writing. Designate a project liaison who shall be responsible for responding to noise complaints during construction. The name and phone number of the liaison shall be conspicuously posted at construction areas and on all advance notifications. This person shall take steps to resolve complaints, including periodic noise monitoring, if necessary. Results of noise monitoring shall be presented at regular project meetings with the Project contractor, and the liaison shall coordinate with the contractor to modify, to the extent feasible, any construction activities that generated excessive noise levels. Require a reporting program that documents complaints received, actions taken to resolve problems, and effectiveness of these actions. Hold a preconstruction meeting with the job inspectors and the general contractor's onsite project manager to confirm that noise mitigation and practices (including construction hours, construction schedule, and noise coordinator) are completed. 	<p>Prepare prior to construction and implement during construction</p>	<p>City and Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans) City to ensure that stationary operational equipment complies with local noise ordinance / general plan limits.</p>	<p>Project General Contractor to provide Noise Control Plan to Project Construction Manager City to provide documentation of compliance of stationary operational equipment with local noise ordinance / general plan limits to Project Construction Manager</p>	<p>Review of Noise Control Plan and documentation of stationary operational equipment noise levels to ensure they address the elements identified in the measure. Weekly inspection of equipment to confirm it meets specifications of Noise Control Plan.</p>			
<p>PUB-1: At least 48 hours prior to road closures, the City's Public Works Department (or designee, such as the City's Consultant Construction Manager) shall notify local emergency service providers (Pacifica Police Department at 650-738-7314 and North County Fire Authority at 650-991-8138) of road closures and length of closure. The construction contractor shall obtain encroachment permits from the City's Engineering Department prior to construction-related lane or street parking closures.</p>	<p>48 hours prior to road closures</p>	<p>Project Construction Manager</p>	<p>City Project Construction Management to provide documentation of notification to City Public Works Department Designee</p>	<p>Written documentation of notifications</p>			

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<p>TRANS-1: The selected contractor shall be required to prepare a Traffic Control Plan, which shall include, at minimum:</p> <ul style="list-style-type: none"> A set of comprehensive traffic control measures to maintain safety and Level of Service (LOS), including: <ul style="list-style-type: none"> - scheduling of major truck trips and deliveries to avoid peak traffic hours – deliveries and soil off-haul trucks shall not arrive onsite prior to 8 a.m. and shall not leave the site outside of the hours of 9 a.m. to 3 p.m., - requirements for posting of detour signs, - requirements for traffic control personnel such as flaggers during disruptions in the City rights-of-way, - lane closure procedures and signage requirements, - placement requirements for signs and cones for drivers, and - designated construction access routes; Methods for maintaining the condition and LOS of city and state roadways; Notification procedures for adjacent properties and public safety personnel regarding when major deliveries, detours, and lane closures would occur; Location of construction staging areas for materials, equipment, and vehicles at an approved location; Any heavy equipment brought to the construction site shall be transported by truck, where feasible. 	Prepare prior to construction and implement during construction	Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans)	Project General Contractor to provide Traffic Control Plan to Project Construction Manager	Review of Traffic Control Plan to ensure elements identified in measure are addressed			
<p>APM-1: During construction, the Project's general contractor shall use off-road equipment that would meet, at minimum, the following criteria:</p> <ol style="list-style-type: none"> All mobile diesel-powered off-road equipment larger than 25 horsepower (hp) and operating on the site for more than 20 hours shall meet, at a minimum, one of the following: <ol style="list-style-type: none"> Equipped with engines meeting U.S. Environmental Protection Agency (EPA) particulate matter emissions standards for Tier 2 engines or equivalent; All diesel-powered portable equipment (i.e., generators, concrete saws, and pumps) operating on the site for more than 20 hours shall be equipped with CARB-certified Level 3 Diesel Particulate Filters or meet U.S. EPA particulate matter emissions standards for Tier 4 engines or equivalent; Use alternatively fueled (i.e., non-diesel) equipment; or Other measures may be the use of added exhaust devices, or a combination of measures, provided that these measures are approved by the City and demonstrated to reduce community risk impacts to less than significant. Line power shall be utilized as soon as feasible, limiting the use of all diesel-fueled generators, including any diesel-powered welders, used for construction activities exceeding 20 days for each piece of equipment. <p>Alternatively, prior to construction, the Project's general contractor may develop a plan, verified by a qualified air specialist and approved by the City, which ensures that the off-road equipment used onsite to construct the Project would achieve a fleet-wide average 75 percent reduction in PM10 exhaust emissions, compared to uncontrolled aggregate statewide emission rates for similar equipment.</p>	Ongoing during project construction	Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans)	Project Construction Manager	Monthly inspection of equipment to confirm it is certified in accordance with criteria			
<p>APM-2: The Project's general contractor shall be required to construct temporary noise barriers to shield stationary noise sources (e.g., tunneling equipment) from nearby receptors. The barrier shall be a minimum of 16 feet in height and would provide approximately 8 to 10 A-weighted decibels (dBA) of attenuation at the first floor, and approximately 5 dBA of attenuation at second and third floors, where the line-of-sight to construction activities is interrupted by the barrier.</p>	Install prior to construction and monitor and maintain during construction	Project General Contractor (City shall include these measures on all appropriate bid, contract, and engineering / site plans)	Project Construction Manager	Written / Photographic documentation of noise barrier installation, and specifications in accordance with the measure			
<p>APM-3: As noted in the Project Description, prior to the flushing and cleaning of the EQ basin, the City's Wastewater Treatment Plant Superintendent, or deputy thereof, shall contact both the North County Fire Department and the North Coast County Water District to alert them of anticipated water usage. If the water usage would impede water service for either of the agencies, the flushing and cleaning of the tank would be delayed until the demand could be met.</p>	Prior to cleaning of EQ basin	City's Wastewater Treatment Plant Superintendent, or deputy	City Public Works Director, or deputy	Written documentation of notifications			

O'Connor, Bonny

From: William Booth-Dunbar <william@gardensystems.com>
Sent: Monday, January 30, 2017 7:35 PM
To: O'Connor, Bonny; aguilarm@ci.pacifica.ca.us
Subject: Fwd: Comments on Proposed Wet Weather Equalization Basin

To Whom It May Concern:

I am reaching out as a concerned homeowner at 1003 Anza Dr.

NOTE: I understand that we need a solution to the current sanitary overflows and illegal waste discharges that the city is under court order to stop. However, I feel this location endangers wetlands, wildlife, and could cause nuisance to hundreds of people and homes.

To clarify my personal position and find where we can work together I will outline the issues I personally see with this proposal-

1. Environmental Impact Report and Pacificas intent to adopt a Mitigated Negative Declaration:

I do not see where this project would qualify for the MND/IS exemption under CEQA, maybe you can be of some help in determining if the following factors would be valid objections:

- Complete change of use from parking lot to sewage storage facility with parking lot
- Dewatering and lowering of water table
- Impacts of dewatering and lowering of water table within wetland to the west
- Impacts on deer, and other wildlife that inhabit the open space to both the east and the west
- Air quality, pollution, and in particular dust
- Noise and odor
- No emergency overflow or flood plan in place

2. Negative Impact on Public Goods

The residents and taxpayers of Pacifica have invested millions into the community center and skatepark which both see very heavy traffic and usage. Locating the basin near these community investments may negatively impact and possibly destroy the enjoyment of these spaces with foul odors. It seems shortsighted to put the enjoyment of these areas at risk.

5. Negative Impact on Neighbors

- Construction noise
- Odor
- Loss of business for adjacent businesses
- Air quality and methane
- Negatively impacts one of the most used walking routes in Lindamar

3. Lack of sufficient documentation that this will actually fix the problem-

Nowhere have I seen the calculations published that were used to estimate the correct size of the basin for the amount of inflow and infiltration of stormwater. Until I see proof that 2.1 million gallons is of adequate size I will be opposed to this project. Also, with the "passive diversion structures" there is no way to stop inflow into the equalization basin if something were to go wrong. If the internal pumps fail, where will all the sewage go?

4. No Flood Simulation or Flood Response Plan-

- What happens during a 30yr or 100yr flood? Where does that 2.1 million gallons of sewage go? What will the city do in response?

5. No Legal Guarantee That This Will Not Be Used To Store Waste Long Term

- In the last meeting the city promised the basin would **ONLY** be used in wet weather events a few times a year, and we have no legal guarantee or recourse if they change their mind and want to increase capacity by utilizing the 2.1 million gallon tank for general sewer storage.

6. Previous History of Poor Decision Making and Lack of Maintenance/Management

- The city of Pacifica has a reputation that speaks for itself concerning past projects and maintenance. Furthermore, I have not seen a maintenance cost estimate to go along with the proposal which as a taxpayer I would like to know.

During a two inch rain event over 95 million gallons drains through the valley, I can't imagine how angry all the taxpayers will be if this 20 million dollar project doesn't actually work.

Thanks again for taking the time,

William at 1003 Anza Dr

William Booth-Dunbar

[\(415\)-857-4435](tel:(415)857-4435)

william@gardensystems.com

gardensystems.com



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O'Connor, Bonny

From: Erin Macias <necesse@comcast.net>
Sent: Wednesday, February 01, 2017 2:15 PM
To: Aguilar, Maria; O'Connor, Bonny
Subject: objection to basin

Follow Up Flag: Follow up
Flag Status: Completed

I want to express my objection in writing to the 2.1 million gallon equalization basin at the Community Center. I would like a link to the environmental impact report with regards to air quality. When the Sewage Treatment plant was constructed, the residents were told we would not know it's there. The stench from that plant has a direct environmental impact to the surrounding neighborhood and to the students at Vallemar School. My children attended there from 2006-2015 and the air pollution was an ongoing issue. I am advocating on behalf of the children in this community who should not be subjected to the smell at the Pacifica Skate Park, the Community Center, nor at Cabrillo Elementary.

The wind and topography determine how plumes travel. There are children in the immediate vicinity who will be directly impacted by this basin.

Please provide me a copy of either the EIR and sufficient evidence that 10' vents will maintain the odor plume above the skatepark and that plume will be directed over the ocean 100% of the time.

Sincerely,
Erin Macias, Linda Mar

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Scenic Pacifica
Incorporated Nov. 22, 1957

PLANNING COMMISSION Staff Report

DATE: February 6, 2017

ITEM: 4

SUBJECT: Annual Report to the City Council

BACKGROUND/DISCUSSION:

The "Resolution Providing for Appointments, Terms, Reporting and Reorganization Requirements for Commission and Committees" adopted on May 13, 2013 stated that annual reports from committees and commissions shall be required. As described in the "Pacifica City Council Subcommittee Report on Committees and Commissions", the annual report should be a summary of the Commission's work for the year and provide an opportunity for Council questions and feedback on Commission activities.

Staff has provided a draft summary of the Commission's work during the 2016 calendar year, which would comprise the written report to City Council (Attachment A).

COMMISSION ACTION REQUESTED:

Discuss Annual Report and presentation to City Council scheduled for February 27, 2017.

Attachments:

A. Draft Annual Report



CITY OF PACIFICA
PLANNING COMMISSION
2016 ANNUAL REPORT TO THE CITY COUNCIL

The Planning Commission is submitting the following summary of work for 2016.

Commission Members	Term Expires
Josh Gordon, Chair	2019
John Nibbelin, Vice Chair	2017
Jeffery Cooper	2018
Thomas Clifford	2018
Charles (Chuck) Evans	2019
Jack Baringer	2020
Richard Campbell	2020

Attendance Record

<u>Commissioner</u>	Absences					
	2014	Percent of Meetings Held	2015	Percent of Meetings Held	2016	Percent of Meetings Held
Gordon	5	50%	4	24%	4	17%
Nibbelin	2	20%	8	47%	10	43%
Cooper	1	10%	5	29%	5	22%
Clifford	--	--	--	--	2	13%*
Evans	3	30%	2	12%	3	13%
Baringer	--	--	--	--	1	6%*
Campbell	2	20%	3	18%	3	13%

* Commissioners Clifford and Baringer were appointed mid-year. Percentage based on number of meetings held since appointment.

Permit and Commission Activity Report

- The Planning Commission heard 8 applications in 2014 (excluding study sessions and permit extensions), 28 applications in 2015, and 29 applications in 2016. These figures do not include appeals to City Council.
- The Planning Division received 30 new entitlement applications in 2014, 90 in 2015, and 50 in 2016. These figures include all types of entitlement applications including new uses in existing structures or modifications to existing structures.

➤ Building Permits Issued:

Year	Permits Issued	Inspections	Residential Valuation	Commercial Valuation
2014	1,500	3,686	\$9,642,770	\$813,395
2015	1,763	4,600	\$15,378,120	\$2,317,468
2016	1,599	4,241	\$13,933,100	\$8,998,495

Summary of Significant Items Heard by the Commission in 2016

- Coastal Development Permits for unsafe structure(s) on Esplanade Avenue
- Study Session on property designated Very Low Density Residential in the General Plan
- Study Session on Redevelopment of the Quarry Property
- Recommended amendments to the Zoning Code related to Massage Businesses
- Pacifica Resource Center Expansion
- Coastal Development Permit for portion of Pedro Point Trail Restoration Project
- Mixed-Use Infill Development at Francisco and Carmel
- Apartment complex at 4000-block of Palmetto
- Coastal Development Permit for Anchor Inn (Pedro Point)
- Study Session for 570 Crespi Drive Mixed-Use Development

Public Transparency and Participation Initiatives in 2016

- Encourage community outreach prior to Commission meetings as discussed at Council budget sessions.
- On-line access to current project information and dedicated webpages for significant projects such as the Quarry proposal.
- Use online agenda management tool for ease of public access to staff reports (same as City Council agendas). This initiative is in progress.

Major Projects & City Initiatives in the Pipeline

- Condominium Project on Fassler
- Quarry Reclamation Plan
- Short-term Rental Regulation*
- Beach Boulevard Development*
- New Library Project*
- General Plan Update*
- Sea Level Rise Adaptation Planning and Local Coastal Plan Update*
- Housing Element Implementation & Affordable Housing Initiatives*
- Residential Commercial Linkage Nexus Study*
- Establish Priority Development Area*
- Accessory Dwelling Unit ordinance update*
- Land Use regulation recommendation for non-medical marijuana uses

* Items are on the City Council work plan. The City Council is holding a goal setting session on February 3-4th. Staff will provide an update on changes, if any, impacting the items listed above at the February 6th Planning Commission meeting.